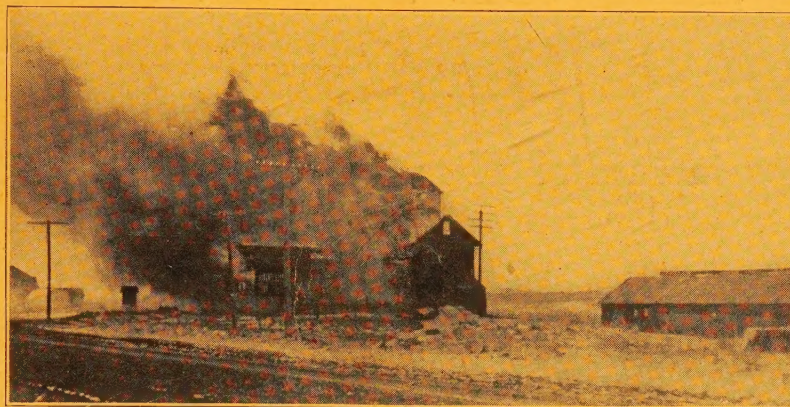


GRAIN DEALERS JOURNAL

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Preventing

Fires and the Suspension of Business is of far more importance to Grain Elevator Owners than just insuring them against loss and doing nothing to save their plants from the flames.

Our earnest aim through all the years we have been insuring the property of Grain Dealers has been to assist and encourage our policy holders to correct the fire hazards of their plants, and thereby prevent the loss of property and the suspension of business. Our self inspection reports have eliminated many hazardous features, reduced fire losses and the cost of insurance to policy holders.

We specialize not only in correcting the known fire hazards of Country Grain Elevators, but we insure them against fire and wind at a minimum cost and see to it that grain salvage is promptly disposed of in the interests of all concerned. Many progressive grain dealers are the proud possessors of our improved policies.

Whether or not your elevator is an acceptable risk, our inspectors will be glad to point out how you can reduce its fire hazards and maybe prevent its burning. Shall we instruct them to look it over?

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Omaha, Nebr.

C. A. McCOTTER,
Secretary
Indianapolis, Ind.

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THE grain men of Memphis want no credit for what they have done to put Memphis second to none as a feed manufacturing point. However, they are quick to point out the excellent location of their market for both receiving grain from the large producing sections and for reaching that large feed consuming territory in the great Southeast. Memphis' success was logical. You will be pleased to deal with any of the Merchants Exchange Members mentioned here—and they will be pleased to handle your business. Write them all today.

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Delta Flour & Feed Co.
Flour, Feed, Meal, Grains

Get the Memphis Habit

Directory of the Grain Trade

In Organized Markets Only Members of the Local Grain Exchange Will Be Listed

HAVING YOUR name in this directory will introduce you to many old and new firms during the year, whom you do not know or could not meet in any other way. Many new concerns are looking for connections, seeking an outlet or an inlet, possibly in your territory. It is certain that they turn to this recognized Directory, and act upon the suggestions it gives them.

AMARILLO, TEXAS.

Early Grain & Elevtr. Co., wholesale grain.*
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Gill & Fisher, receivers and shippers.*
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Steen & Bro., E., grain receivers and exporters.*

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Birmingham Grain Co., grain, feed, flour.*
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Globe Grain Co., receivers & shippers.*
Harold, A. W., grain, barley a specialty.
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McConnell Grain Corporation, commission merchants.*
McKillen, Inc., J. G., receivers and shippers.*
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Pratt & Co., receivers, shippers of grain.*
Ratliffe, S. M., commission merchant.*
Seymour-Wood Grain Co., consignments.*
Sunset Grain & Feed Co., grain & feed.
Taylor & Rounique Co., grain merchants.
Traders & Producers Supply Co., millfeeds a specialty.
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Urmston Elevator Co., grain commission.*
Watkins Grain Co., consignments.*
Whitney & Gibson, consignments.*

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Bailey & Co., E. W., grain commission merchants.*
Bartlett-Frazier Co., grain merchants.*
Brennan & Co., John E., grain commission merchants.*
Carhart Code Harwood Co., grain commission.*

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Freeman & Co., Henry H., grain, hay, straw.
Gerstenberg & Co., commission merchants.*
Hales & Hunter Co., grain merchants.*
Harris, Winthrop & Co., grain commission.*
Hitch & Carder, commission merchants.*
Holt & Co., Lowell, commission, grain and seeds.
Lamsen Bros. & Co., consignments solicited.*
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McKenna & Dickey, commission merchants.*
Mumford & Co., W. R., grain, hay, millstuffs.*
Norris Grain Co., grain merchants.*
Northern Grain & Warehouse Co., grain receivers.
Paynter, H. M., grain and field seeds.
Pope & Eckhardt Co., grain and seeds.
Press & Co., W. G., grain, provisions, stocks, etc.
Requa Bros., wheat a specialty.*
Rumsey & Co., grain commission.*
Schiffin & Co., P. H., commission.*
Shaffer Grain Co., J. C., grain merchants.*
Simons, Day & Co., grain merchants.*
Somers, Jones & Co., grain and field seeds.*
Uddike Grain Co., consignments.*
Leland & Co., E. F., grain and seeds.*

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Gale Grain Co., The A. C., receivers & shippers.

CLEVELAND, O.

Grain & Hay Exchange Members.

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Cleveland Grain & Milling Co., The, revrs. & shprs.*
Gates Elevator Co., The, receivers and shippers.*
Sheets Elevator Co., The, grain, hay, straw.*
Shepard, Clark & Co., grain merchants.*
Strauss & Co., H. M., receivers, shippers hay & grain.*

COLBY, KANSAS.

Harris & Haynes, wholesale—brokers—grain.

CLOVIS, N. MEX.

Western Elevtr. Co., The, revrs-shprs.. hdqtrs kafir-milo.*

COLORADO SPRINGS, COLO.

Robinson-Hunt Grain Co., grain, hay, seeds, beans.*

DAVENPORT, IOWA.

Davenport Elevator Co., receivers and shippers.*

DALHART, TEXAS.

Kinard Grain Co., J. C., wholesale grain & hay.*

DECATUR, ALA.

Decatur Coal & Mfg. Co., grain and feedstuffs.
Lyle-Taylor Grain Co., whole grain, hay, feeds.*

DECATUR, ILL.

Baldwin & Co., H. I., grain dealers.*

DENVER, COLO.

Grain Exchange Members.

Ady & Crowe Merc. Co., The, grain and hay.
Ashcraft Grain Co., S. B., wholesale grain.
Conley-Ross Grain Co., The, grain and beans.
Crescent Flour Mills Co., The, merchant millers.*
Denver Elevator, wholesale grain, flour, millfeed.*
Farmers Elevator Co., The, H. F. Rover, Mgr.
Gallagher Grain Co., grain merchants.*
Gilmore-Livingston Grain Co., grain and hay.
Houlton Grain Co., wholesale grain.
Kellogg Grain Co., O. M., receivers and shippers.*
McCaull-Dinsmore Co., wholesaler and commission.*
Moore-Lawless Grain Co., private wires to all markets.*
Phelps Grain Co., T. D., wholesale grain.*
Rocky Mountain Grain Co., export and domestic grain.
Scott-George Grain Co., The, receivers and shippers.*
Summit Grain Co., wheat, corn, oats, rye, barley.*

DES MOINES, IOWA.

Board of Trade Members.

Anderson Co., D. L., grain merchants.
Iowa Grain Co., receivers and shippers.
Lockwood, Lee, grain, millfeed broker.
Marshall Hall Grain Co., grain commission.
Taylor & Patton Co., corn and oats.

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Board of Trade Members.

Dumont, Roberts & Co., receivers, shippers.*
Huston, C. R., grain and hay.
Lapham & Co., J. S., receivers & shippers.
Lichtenberg & Son, oats, corn, hay, straw.
Simmons & Co., F. J., grain and hay.*
Swift Grain Co., consign or ask for bid.*

DODGE CITY, KANSAS.

Hillyer Grain Co., grain.

DUBLIN, GA.

Smith Brothers, brokers, mdse., grain, feedstuffs.*

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Board of Trade Members.

Mitchell Co., W. C., grain commission.*
White Grain Co., shippers all grains.*

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Barkemeyer Grain & Seed Co., grain dealers.*

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Moon-Taylor Co., grain and hay brokers.*

GREENVILLE, O.

Grubbs Grain Co., E. A., track buyers.*

HAGERSTOWN, MD.

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HASTINGS, NEBR.

Elder, Fred W., wholesale grain, hay and mill pdts.*
Koehler-Twisdale Elevator Co., grain dealers.*
Moritz Grain Co., C., wholesale grain.*
Sexson, C. R., grain.

HATTIESBURG, MISS.

Merchants Grocery Co., whlse. grocers, grain, fd., dr.

HILLSBORO, KANSAS.

Hillsboro Roller Mills, gr. dlrs., dr., chick feed.

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Kothschild Co., S., grain, c/s products, rice b/p.*

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Central Grain Co., The, buyers for mills.
Farmers Co-op. Com. Co., commission merchants.
Goffe & Carkner, private wire.*
Hayes Grain Co., John, grain merchants.
Hutchinson Grain Co., grain merchants.
Logan Bros. Grain Co., receivers & shippers.*
McClure Grain Co., J. B., buyers and sellers.*
Midwest Grain Co., The, shippers.
Moore Grain Co., consignments—buyers of grain.
Producers Grain Co., The, milling wheat.
Southwest Grain Co., receivers and shippers.
Union Grain Co., grain merchants.
Vanderslice-Lynds Co., grain commission merchants.

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Boyd Grain Co., Bert A., strictly brokerage & com.
Cleveland Grain & Milling Co., grain commission.
Evans Grain Co., W. R., commission and brokerage.*
Hayward-Rich Grain Co., grain commission.*
Hoosier Grain Co., consignments only.
Kendrick & Sloan Co., Inc., grain and hay.
Menzie Grain & Bkg. Co., Carl D., grain commission.*
Montgomery & Tompkins, receivers and shippers.*
Pro'at & Kassebaum, Inc., hay, grain, feed.
Steinhart Grain Co., commission and brokerage.*
Urmston Elevator Co., receivers and shippers.*
Witt, Frank A., grain commission and brokerage.*

JACKSON, MICH.

Bartlett & Co., J. E., feed, grain, salvage.
Wagner-White Co., track buyers-sellers, grain-feed.

JACKSON, MISS.

Field Co., Robt., succ. to P. L. Brittain Co.
Royal Feed & Mfg. Co., mixed feed mfrs.*

KANSAS CITY, MO.

Board of Trade Members.

Alfalfa Grain Pds. Co., everything in the feed line.
Beyer Grain Co., consignments.
Christopher & Co., B. O., kafir, feterita, milo.*
Claiborne Commission Co., commission merchants.*
Croysdale Grain Co., commission merchants.
Davis Grain Co., A. C., grain commission.
Denton Kuhn Grain Co., consignments.*
Ernst Davis Grain Co., commissions.*
Federal Grain Co., receivers, shippers.*

(Continued on next page.)

*Member Grain Dealers National Association.

Directory of the Grain Trade

In Organized Markets Only Members of the Local Grain Exchange Will Be Listed

KANSAS CITY, MO., (Continued)

Friscove Elevators Co., grain merchants.*
Goffe & Carkever, grain commission.*
Hah-Baker Grain Co., consignments.*
Hipple Grain Co., feterita, kafir, milo.
Larabee Flour Mills Corp., The, mfrs. "Larabee Best."
Lichtig & Co., H., kafir, milo, screenings.*
Logan Bros. Grain Co., receivers and shippers.*
Mensendieck Grain Co., consignments.*
Miller Grain Co., S. H., consignments.*
Moore-Lawless Grain Co., grain receivers.*
Moore-Seaver Grain Co., receivers and shippers.*
Nicholson Grain Co., W. S., strictly commission.*
Norris Grain Co., grain merchants and exporters.*
Roehen Grain Co., E. E., consignments.*
Root Grain Co., consignments and futures.*
Scouler Bishop Grain Co., receivers and shippers.*
Shannon Grain Co., consignments.*
Simonds, Shields, Lonsdale Grain Co., grain.*
Stevenson Grain Co., buyers and sellers.*
Terminal Elevators, receivers, shippers.*
Twidale-Wright Grain Co., consignment futures.*
Thresher Fuller Grain Co., grain commission.*
Western Grain Co., receivers and shippers.*
Wilser Grain Co., consignments.*

LAWRENCE, KANS.

Underwood & Sons, J., grain, feed, seeds.

LEAVENWORTH, KANS.

Wilson-Legier Hay & Grain Co., branch at Kansas City.

LIBERAL, KANS.

Light Grain & Mfg. Co., mill pdts., kafir, milo.
Vickers Grain & Seed Co., grain and field seeds.

LINCOLN, NEBR.

Grain Exchange Members.

Ewart Grain Co., wheat, corn, oats, rye, barley.*
Foster, E. D., feeders supplies.*
Lincoln Grain Co., grain merchants.*
Wright-Leet Grain Co., receivers and shippers.*

LITTLE ROCK, ARK.

Grain Exchange Members.

Caple & Stockton, hay, grain, feed.
Davis, S. P., Est. 1893, grain, flour, cottonseed meal.
Farmer Wilson Co., brokers, hay, grain, mill feed.*
Gordy Co., C. L., grain brok., hay, grain, mill feed.*
Wilson Co., John R., grain brokers.*

LOUISVILLE, KY.

Board of Trade Members.

Bingham-Hewett Gr. Co., receivers-shippers of grain.*
Callahan & Sons, receivers and shippers of grain.*
Edinger & Co., grain, hay, flour.*
Farmer & Sons, Oscar, hay, grain and feeds.*
Fruechtenicht, Henry, grain, feed, hay.*
Kentucky Public Elevator Co., storers and shippers.*
Verhoeff & Co., H., receivers and shippers.*
Zorn & Co., S., receivers and shippers.*

LYNCHBURG, VA.

Moon-Taylor Co., grain and hay brokers.*

M'KINNEY, TEXAS.

Reinhart & Company, wheat, corn, oats, maize.

MARSHALL, MO.

Claiborne Commission Co., commission merchants.*

MEMPHIS, TENN.

Merchants Exchange Members.

Browne, Walter M., broker & com., consignments.*
Buxton, E. E., broker and commission merchant.*
Clark-Burke & Co., grain and hay.*
Cook, L. P., receiver and shipper.*
Davis & Andrews Co., grain, mixed feed.*
Delta Flour & Feed Co., flour, feed, meal, grains.*
Denyven & Co., brokers and commission.*
Edgar-Morgan Co., mixed feed manufacturers.*
Hasenwinkle, H. J., consignments.*
International Sugar Feed Co., feed mfrs. and grain.*
Lovitt & Co., L. B., cotton seed and peanut products.*
Mississippi Elevator Co., grain dealers, feed mfrs.*
Pease & Dwyer, grain, mixed feed.*
Quaker Oats Co., feed & cereal mfrs.*
Royal Feed & Milling Co., mixed feed mfrs.*
Sessum Grain Co., grain, mixed feed.*
U. S. Feed Co., grain, hay, mill feed.*
Wade & Sons, Inc., John, grain, feed, flour.*

MERIDIAN, MISS.

Board of Trade Members.

Lyon & Co., A. J., whole, gro., grain, feed.
Meyer Bros., wholesale groc., grain, feed.*
Sturgis Co., grain dealers, mixed feed mfrs.*
Threefoot Bros. & Co., whole grain, feed, fl., gro.*

MIDDLEPOINT, OHIO.

Pollock Grain Co., grain, hay, straw, earl corn.

MIDDLETOWN, CONN.

Meech & Stoddard, Inc., grain, feed, hay, flour.*

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Bell Co., W. M., commission merchants.*
Blanchard Grain Co., "Always Dependable."*
Buerger Commission Co., grain and seed.*
Cargill Grain Co., grain and seeds.*

MILWAUKEE, WIS., (Continued)

Donahue-Stratton Co., dhrs. grain and feed.*
Flanley Grain Co., consignments solicited.*
Franke-La Budde Grain Co., feeds, grain, hay.*
Fraser-Smith Co., commission merchants.*
Kamm Company, P. C., grain shippers.*
Lauer & Co., J. V., grain merchants.*
Merriam Commission Co., consignments.*
Milwaukee Grain Commission Co., grain commission.*
Mitchell Co., W. C., commission merchants.*
Mohr-Holstein Commission Co., grain merchants.*
Rang & Co., Henry, commission merchants.*
Rankin, M. G., & Co., grain and feed.*
Rialto Elvtr. Co., grain receivers and shippers.*
Runkel & Dadmun, grain commission merchants.*
Taylor & Bournique Co., grain merchants.*
Updike Grain Co., consignments solicited.*

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Cargill Commission Co., grain commission.*
Carter-Sammis & Co., grain commission.*
Cereal Grading Co., grain merchants.*
Chambers-Mackay Co., screenings & mill feed.*
Geo. Grain Co., G. W., receivers and shippers.*
Getchell-Tanton Co., grain commission.*
Gould Grain Co., receivers and shippers.*
Hankinson & Co., H. L., grain commission.*
Malmquist & Co., C. A., receivers & shippers.*
Marfield Grain Co., grain commission.*
McCarthy Bros. & Co., receivers & shippers.*
McCaull Dinsmore Co., consignments solicited.*
Mitchell Co., W. C., grain commission.*
Poehler Co., H., grain commission.*
Seldi, Frank J., all grains and feeds.*
Sheffield Elevator Co., shippers of grain.*
Sterling Grain Co., receivers and shippers all grains.*
Van Dusen-Harrington Co., grain merchants.*
Welch Co., E. L., mill oats and screenings.*
Wernli-Anderson Co., grain commission, screenings.*

NEWARK, N. J.

Smith & Wallace Co., J. C., receivers shippers.*

NEW CASTLE, PA.

Hamilton Co., grain, feed, flour, hay, potatoes.*

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Board of Trade Members.

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Barr, R. J., grain exporter.*
Fox Co., C. B., exporters.*
Gibbons, J. T., gr. dealers, mixed fd. mfrs., exptas.*
Matthews Sons, Geo. B., mill feed manufacturers.*
Nathan & Pettis, fwdg. agt. & expt. fght. broker.*
Neumond, Inc., K. & E., dhrs. & exptas in feed articles.*
Richeson Co., Inc., W. L., expt. shpg., fgt. bkg. & fg.*
Rodd & Co., Chas. M., gr. brokers & fwdg. agents.*
Waterman & Co., J. S., gr. flour & fd. bkrs., flr. jobsrs.*

MOULTRIE, GA.

Delay, A. J., flour and grist mill.

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Allen Grain Co., receivers & shippers.*
Allfeed Milling Co., feed manufacturers.*
Kerr, S. S., receiver and shipper.*
Nashville Grain & Feed Co., receivers & shippers.*
Tennessee Grain Co., receivers and shippers.*

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Produce Exchange Members.

Jones & Co., M. B., buyers—quote us.*
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Therrien, A. F., broker.*

NORFOLK, VA.

Moon-Taylor Co., grain and hay brokers.*

OKLAHOMA CITY, OKLA.

Grain Exchange Members.

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Conyers Grain Co., grain merchants.*
Lang Grain Co., J. H., prompt and efficient service.*
Langenberg Bros. Grain Co., grain merchants.*
Marshall Grain Co., grain, feed, seeds.*
Moore, George L., grain merchant.*
Okla. City M. & E. Co., grain merchants, millers.*
Perkins Grain Co., W. L., brokers.*
Scannell-Winters Grain Co., grain and feed.*
Stinnett Grain Co., grain merchants.*
Stowers Grain Co., W. B., commission merchants.*
Strader & Co., J. Edgar, grain, hay, feed.*
White Grain Co.*

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Grain Exchange Members.

Butler Welsh Grain Co., grain merchants.*
Crowell Elevator Co., receivers, shippers.*
Holmquist Elevator Co., receivers and shippers.*
Maney Grain Co., The, consignments.*
Miller Wilson Grain Co., receivers and shippers.*
Roberts Grain Co., Geo. A., grain merchants.*
Stockham Grain Co., E., commission merchants.*
Trans-Mississippi Grain Co., receivers and shippers.*
United Grain Co., grain commission.*
Updike Grain Co., consignments.*

OTTAWA, KANS.

Ross Milling Co., The, millers, hard wheat flour.

PAMPA, TEXAS.

McMurtry Grain Co., L. C., wheat, kafir, milo.*

PEORIA, ILL.

Board of Trade Members.

Bartlett Co., S. C., grain commission.*
Bowen Grain Co., H. D., receivers & shippers.*
Cole Grain Co., Geo. W., receivers and shippers.*
Dewey & Sons, W. W., grain commission.*
Feltman Grain Co., C. H., grain commission.*
Harrison, Ward & Co., receivers & shippers.*
Luke Grain Co., grain commission.*
McFadden & Co., G. O., consignments.*
McCreery & Son, J. A., wheat, corn, oats.*
Miles, P. B. & C. O., grain commission.*
Mueller Grain Co., J. C., receivers and shippers.*
Shaffer Grain Co., J. C., receivers & shippers.*
Turner Hudnut Co., receivers and shippers.*
Tyng Grain Company, receivers and shippers.*

PHILADELPHIA, PA.

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Delp Grain Co., E. E., grain and millfeeds.*
Dunwoody Co., Edl., flour, grain, feed.*
Lemont & Son, E. K., wheat, corn, oats, flour, feed.*
McKay, Donald, grain and millfeeds.*
Miller & Sons, L. F., grain, seeds, hay.*
Richardson Bros., grain, flour, millfeeds.*
Richardson, Geo. M., grain and feeds.*
Rogers & Co., E. L., hay, straw, grain, feed.*
Standard Hay & Grain Co., grain and hay.*
Stites, A. Judson, grain & millfeed.*
Young & Co., S. H., wheat, corn, oats.*

PITTSBURGH, PA.

Members Grain and Hay Exchange.

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Burson Grain Co., C. G., recvrs., shprs-commission.*
Elwood & Co., R. D., hay and grain.*
Foster Co., C. A., grain merchants.*
Geidel & Leubin, grain and hay.*
Hardman & Heck, grain, hay, millfeed.*
Harper Grain Co., corn a specialty.*
Heck & Co., W. F., grain, hay, millfeed.*
Herb Bros. & Martin, grain and hay.*
McCague, R. S., grain, hay.*
Rogers & Co., Geo. E., grain & hay.*
Smith & Co., J. W., grain merchants.*
Stewart & Co., Jesse C., grain and mill feed.*
Walton Co., Samuel, grain and hay.*

PONTIAC, ILL.

Balbach, Paul A., grain buyer, all markets.

PORTLAND, MAINE.

Casco Grain Co., wholesale, grain & feed.*
Doten Grain Co., The, grain, feed, flour.*
Maine Grain Co., grain, feed and flour.*
Paris Flouring Co., flour, grain and feed.*

PORTLAND, ORE.

Pacific Coast Elevator Co., grain.*
Pacific Grain Co., grain exporters.*

PRINCETON, MO.

Alley Grain Co., oats, corn, wheat, seeds.*

PUEBLO, COLO.

McClelland Met'l I. & R. Co., grain hay, and feed.*

QUINTER, KANSAS.

Jones-Rogers Grain Co., brokers.*

RICHMOND, VA.

Grain Exchange Members.

Beveridge & Co., S. T., grain, hay, feeds, seeds.*

ROCHESTER, N. Y.

Dailey Bros., Inc., receivers and shippers.*

SAGINAW, MICH.

Saginaw Milling Co., flour, feed, hay, grain.*
Smith-Connor Hay & Grain Co., hay and grain.*

SALINA, KANS.

Board of Trade Members.

Baber Grain, Feed & Seed Co., grains, feed, seeds.*
Bever Grain Co., The, consignments and mill orders.*
Bossemeyer Grain Co., The Paul, grain merchants.*
Geo. E. Gano Grain Co., The, receivers & shippers.*
Richter Grain Co., wheat, coarse grains & millfeed.*
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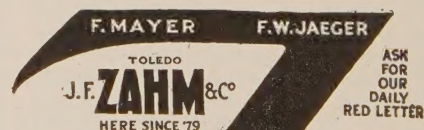
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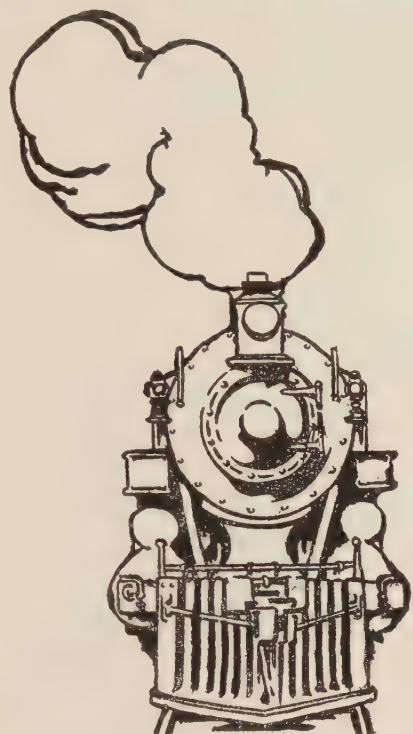
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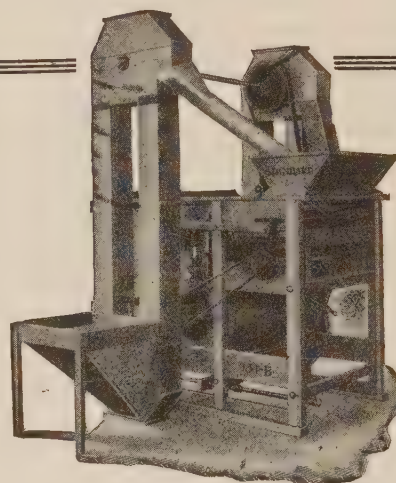
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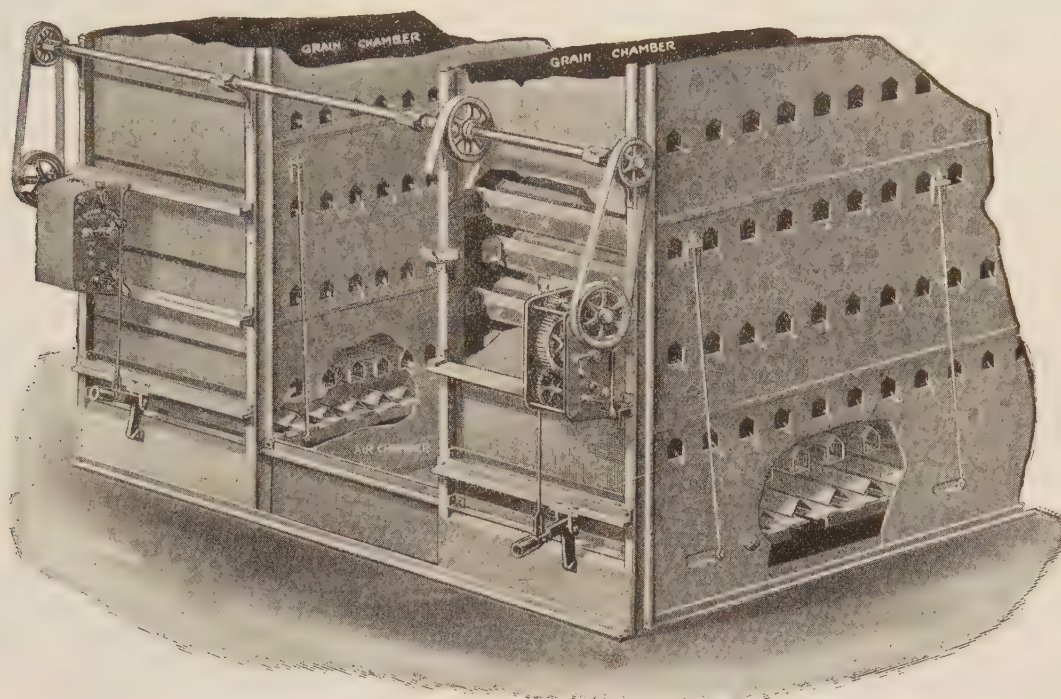
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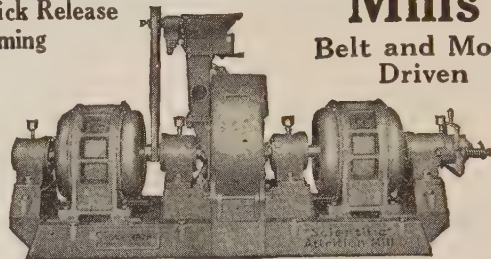
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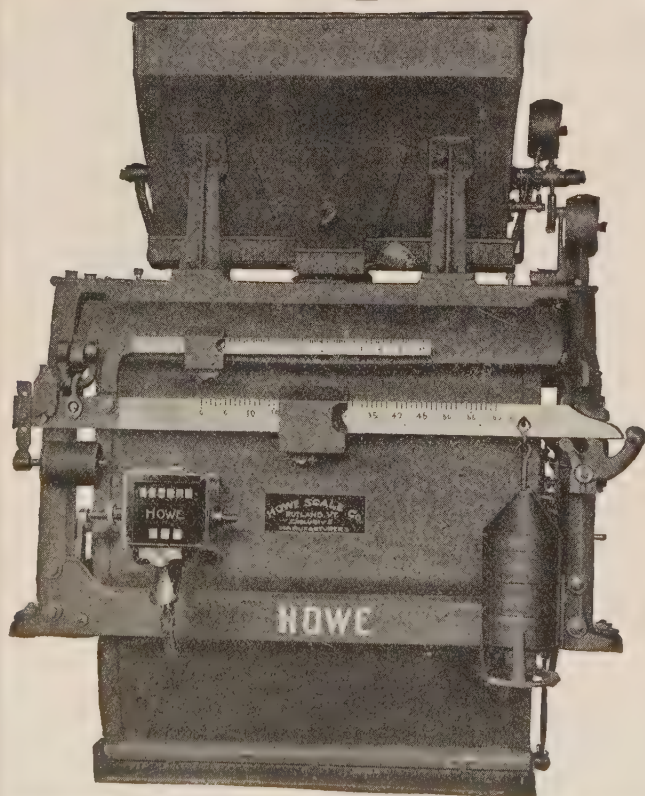
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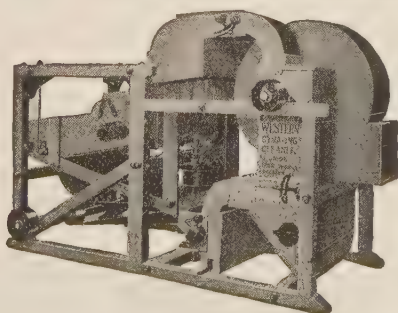
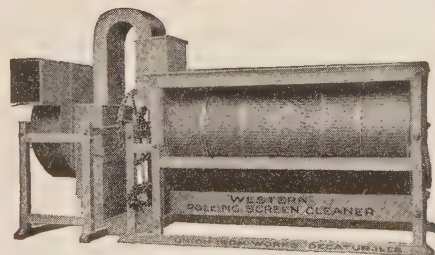
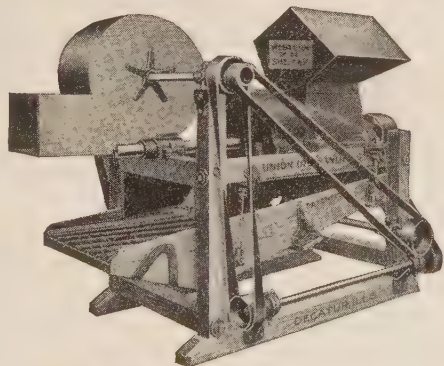
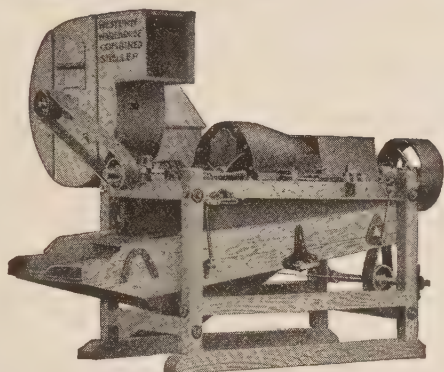
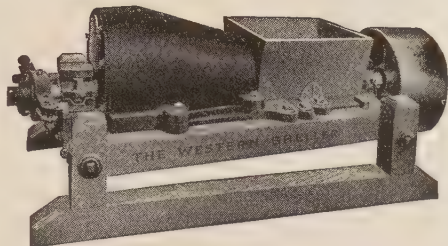
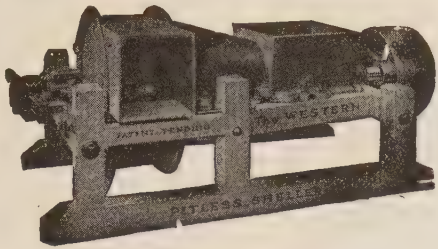
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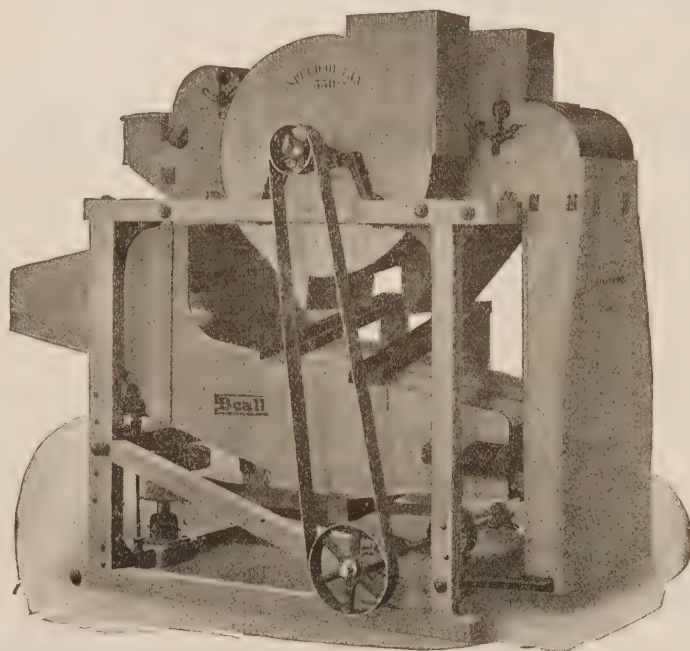
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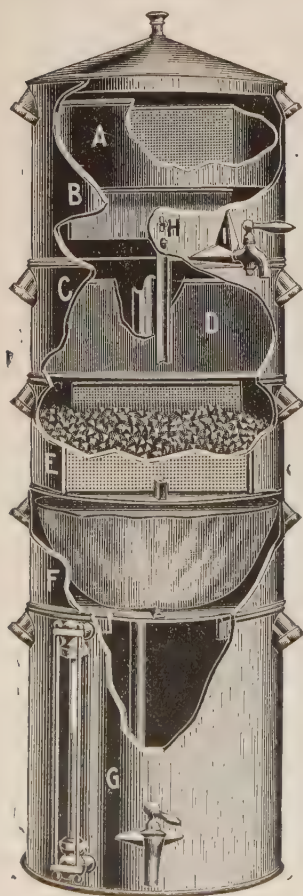


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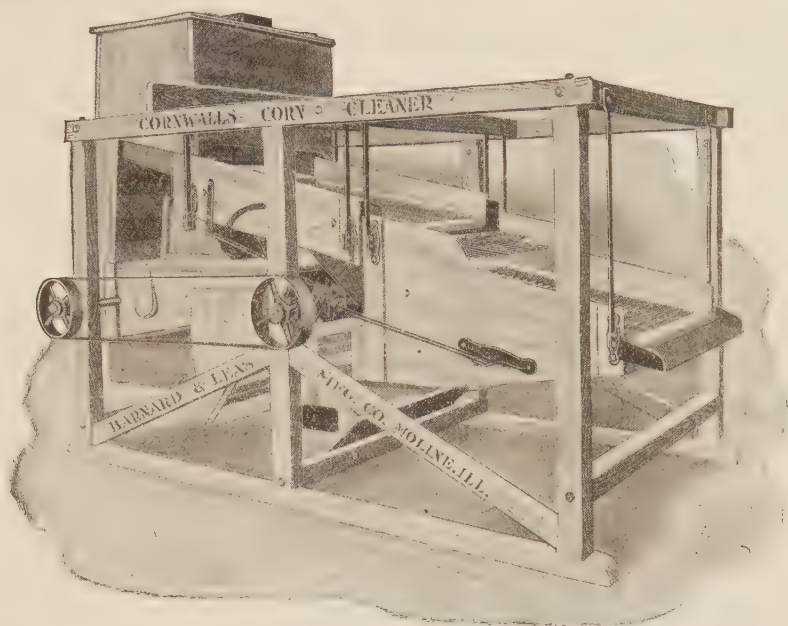
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We are specialists in furnishing and erecting ROOFING & SIDING of galvanized, painted or zinc sheets, corrugated or formed. Our mechanics are experts—they know how. Let us figure on your erection, no matter where located.

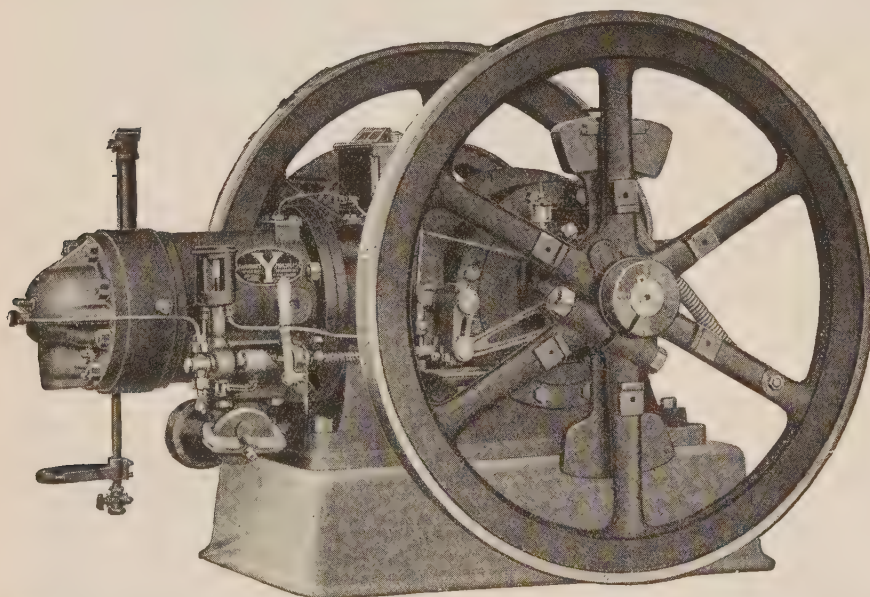
Our complete stock of galvanized, black, blue annealed sheet steel and zinc enable us to give immediate attention to all orders and inquiries received.

The Sykes Company

930 West 19th Place

Chicago, Ill.

FAIRBANKS-MORSE "Y" ENGINES



**"It is Mechanically
and Economically
All right"**

This endorsement of an experienced power user and elevator owner is backed by the thousands of "Y" engines in successful service everywhere.

"Y" oil engines, 10 H.P. to 200 H.P., economically use low grade fuel oils. They are easy to start—practically automatic in operation. Valveless, without carburetors, magnetos, igniters, timers, etc.

Full particulars about this dependable power on request.

Fairbanks, Morse & Co.

MANUFACTURERS

CHICAGO

THE CANADIAN FAIRBANKS-MORSE CO., LTD., MONTREAL



Oil Engines - Pumps - Electric Motors and Generators - Fairbanks Scales - Railway Appliances - Farm Power Machinery

BAD ORDER CARS

cause the loss of many hard earned dollars to shippers of grain and seed.

MUCH OF THIS LOSS can be saved by the use of Kennedy Car Liners. These car liners practically condition a bad order car and enable shippers to load cars that otherwise would be rejected.

KENNEDY SYSTEM of car liners prevent leakage in transit and we make car liners for all cases of bad order cars, consisting of full Standard Liners, End Liners and Door Liners.

WILL YOU NOT give us an opportunity to submit full details of our system and the low cost of same? We are confident this would demonstrate to you the efficiency and money saving merits of our proposition.

THE KENNEDY CAR LINER & BAG CO.
SHELBYVILLE, IND.

Canadian Factory at Woodstock, Ontario.

Wagon Loads Received

A book for the use of country grain buyers in keeping a record of grain received from farmers. Some dealers record oats receipts in front and corn receipts in the back of book; others use a separate book for each kind of grain.

Its column headings are: Month, Day, Name, Kind, Gross and Tare, Net Pounds, Bushels, Pounds, Price, Dollars, Cents and Remarks.

The book is 9½x12 inches, 100 pages, 20 lines to each page, giving room for recording 3,200 loads, printed on Linen Ledger Paper and is well bound in strong board covers with leather back and corners. Order Form 380.

Price, \$2.75

GRAIN DEALERS JOURNAL

305 South La Salle St., CHICAGO, ILL.

U. S. SHELLERS

CORN SHELLER VALUE IS A COMPOSITE—an INDEX composed of quality of materials and workmanship, mechanical soundness, durability, reliability, appearance and price.

To judge U. S. Value, we ask you to examine it exhaustively, interview owners, compare it with all other shellers.

You will be amazed to find that for actual superiority in all essentials U. S. prices are lowest.

U. S. prices are based upon former low costs of plant and equipment, and present costs of labor and material.

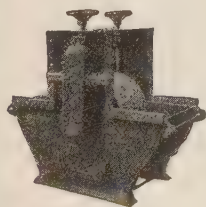
THE B. S. CONSTANT MFG. CO.

Bloomington, Illinois

CALDWELL MILL *and* ELEVATOR SUPPLIES



Steel Buckets for grain, flour and ear corn.
Cast Iron and Steel Boots.
Bucket Bolts.



Car Movers, Car Pullers, Automatic Power Grain Shovels, Turn Spouts, Turn Heads and Distributors, Steel Leg Casings and Spouting, Gears, Pul-

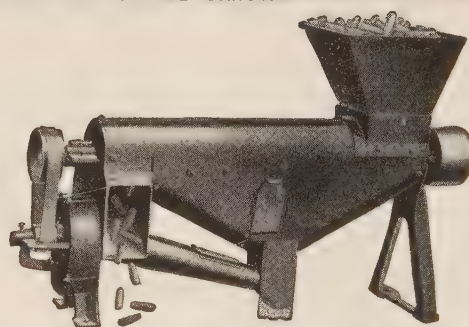


leys, Sprockets, Chain, Belting, Bearings, Shafting, Friction Clutches, Screw Conveyors, Belt Conveyors.

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H. W. CALDWELL & SON CO.
LINK-BELT COMPANY, OWNER.

Chicago, Ill., 17th S. and Western Ave.
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Style A Triumph Corn Sheller

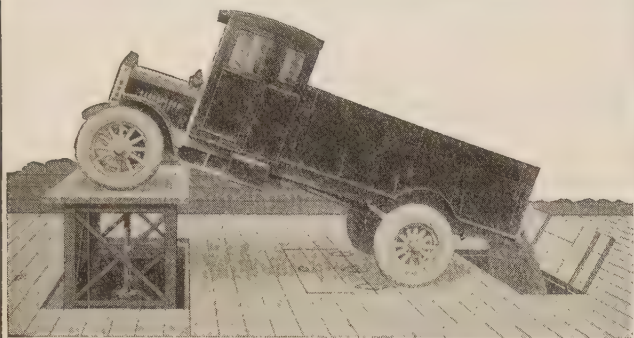
A GOOD SHELLER OF MODERATE CAPACITY

If you can use a Sheller that will properly shell 75 bushels of corn an hour; and if you want a good simple machine, you'll get just the sheller you ought to have in a Triumph.

Thousands of Triumph Shellers are at work in moderate sized mills and elevators both in this country and abroad, and every one of them is shelling corn properly and producing its share of profits.

We'll be glad to send you a copy of our new corn sheller circular. Just send us a postal or a letter asking for it.

THE C. O. BARTLETT & SNOW CO.
Main Office and Works: Cleveland, Ohio



KEWANEE LIFT ALL STEEL—BALL BEARING

We do not know of a single objection to lifts that has not been overcome in the KEWANEE LIFT. It is all steel—no wood to swell or rot. With the exception of the platform top there is no wood used in the construction of the KEWANEE LIFT. It is all of steel, cannot warp or buckle.

By combining the all-steel feature with ball-bearing operation the KEWANEE gives the utmost in satisfactory operation and long life. It is the only dump designed to operate in one unit with either trucks, wagons or sleds.

Regardless of what equipment you may use the KEWANEE is universally adaptable without change or addition. It will not get wobbly, operates smoothly under all loads and will support 50 tons. It has no chains, geared cables, pulleys or overhead framework. It is simple in its construction—easy to operate—quick. Raises and dumps in a few seconds, without locking front or rear wheels. Will not permit sliding while unloading. Accommodates any size or length truck, wagon or sled.

Simple and Durable

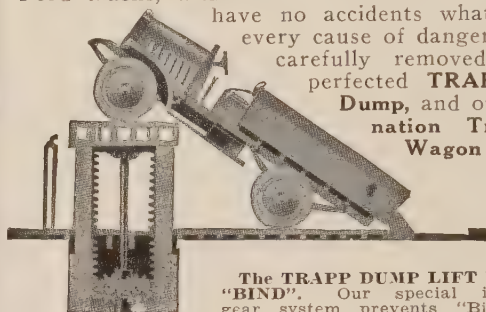
The illustration above shows the KEWANEE lift in operation. It operates with compressed air, the KEWANEE tank being in accordance with American Society of Mechanical Engineer's specifications. No complicated set of gears or sprockets to get out of order and throw your entire outfit out of working condition until repairs can be received. A single turn of the valve raises the lift any height desired. Opening the blowoff valve lowers the platform gently to its original level. No time lost—no doubt—no exertion—quick—positive—dependable. Write today for full description and details.

KEWANEE IMPLEMENT COMPANY
514 Commercial St. KEWANEE, ILLINOIS

You Will Have the Best of Service

if you get a TRAPP DUMP. You could dump the longest, and the heaviest trucks, as well as the light Ford trucks, with a TRAPP DUMP. You would

have no accidents whatever; for every cause of danger has been carefully removed in the perfected TRAPP Truck Dump, and our Combination Truck and Wagon Dump.



The TRAPP DUMP LIFT WILL NOT "BIND". Our special interlocking gear system prevents "Binding", as the gears are installed at all four corner posts of the dump lift; and they hold the platform perfectly rigid and solid.

SEND FOR OUR FREE DESCRIPTIVE CIRCULARS

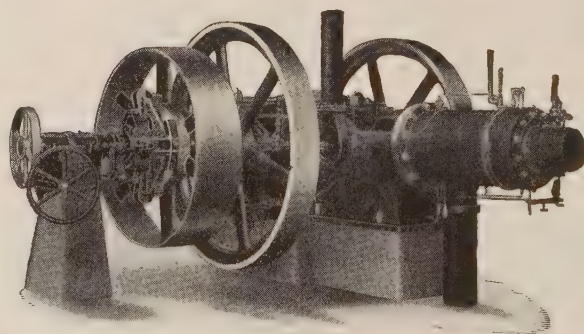
and price schedule. We will send you a list of installations we have made in LINE elevators.

Trapp-Gohr-Donovan Company

Factory and Offices, 1125-27-29 No. 22nd St.

OMAHA, NEBRASKA

MUNCIE Heavy Duty Oil Engine

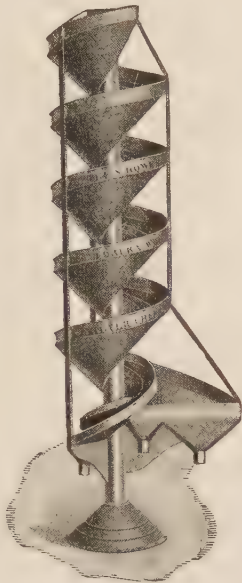


USE CRUDE OIL AND FUEL OIL

Reduce Your Fuel Bill 80 Percent
Sizes 10 to 250 H. P.

MUNCIE OIL ENGINE CO.
MUNCIE, IND., U.S.A.

Branches: Atlanta, Ga. Dallas, Tex. Export Dept.
36-40 W. 60th St., N. Y. City



"Eureka" MUSTARD SEED SEPARATOR

Conceded to be the most ingenious thing ever invented for removing mustard from screenings, grain and seeds.

**Automatic
No Moving Parts**

**Operates on the
Gravity Principle**

**Self-Acting
Requires No Power**

Sold at such a low price that it actually pays for itself in a few weeks.

READ THIS:

Gentlemen—I have O. K'd the bill for Mustard Seed Cleaner. It is surely a dandy little machine, and does the work to perfection. Wishing you success, I am yours truly,

STAR & CRESCENT MILLING CO.

Chicago, Ill.
W. W. SOPHER, Supt.

We'll test your samples and show you something interesting

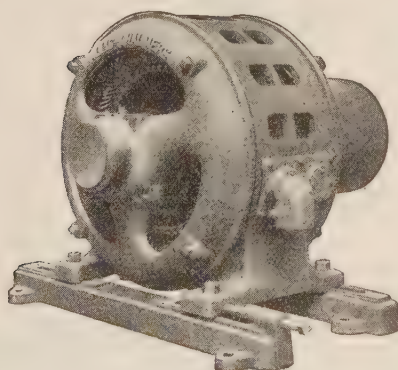


S. HOWES COMPANY, Inc. Silver Creek, N. Y.

European Branch: 64 Mark Lane, London, England

Alternating Current Motors

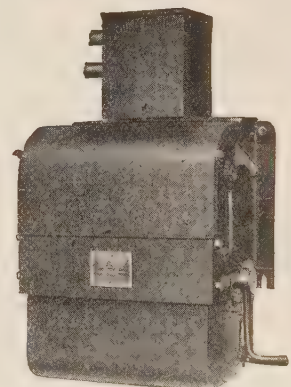
Allis-Chalmers Motors



Type "AN" Squirrel Cage
Induction Motor

are built in various types designed to meet the characteristics of the driven machine.

Our experience, covering a quarter of a century in the design, building and commercial application of motors of all kinds is at your service.



Type "N" Potential Starter
for Squirrel Cage Motor

**ALLIS-CHALMERS
PRODUCTS**
Electrical Machinery
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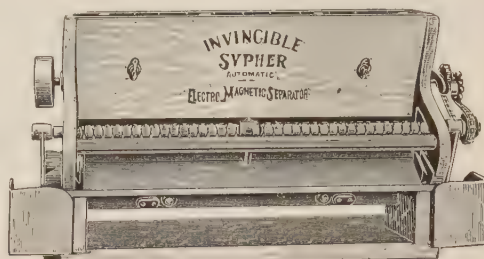
ALLIS CHALMERS
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Agricultural Machinery
Condensers

THIS WAY OUT

It is very annoying to a miller to see newly corrugated rolls all battered and marred by iron particles.

There is no good reason why it should be permitted.



The INVINCIBLE Sypher Electro Magnetic Separator will remove all the iron.* Attracting force of 500 lbs.

Small consumption of current. Attaches to lighting line or if Direct Current is not available, current is generated by an inexpensive miniature dynamo.

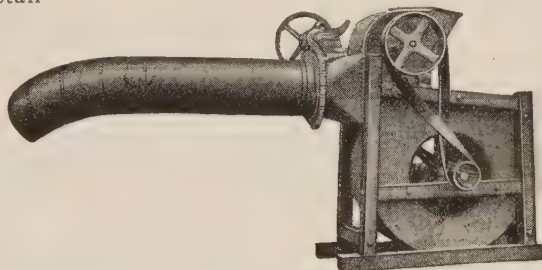
Write for special bulletin No. 27

INVINCIBLE GRAIN CLEANER COMPANY, Silver Creek, N. Y.

CLEAN AND LOAD IN ONE OPERATION

To make the most money these days the grain elevator operator must operate his plant as economically as possible; he must ship the highest grain possible and it must be clean. To get the maximum in loading facilities and at the same time clean the grain loaded every elevator operator should install

The
Combined
Grain
Cleaner
and
Pneumatic
Car
Loader

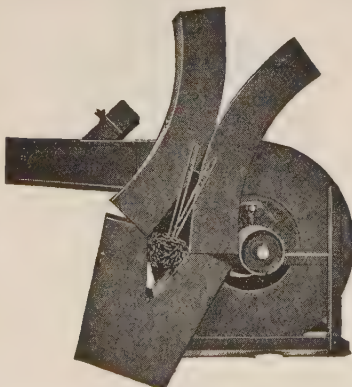


It does not mill or crack the grain; fills cars to full capacity; strong, durable, requires no attention after starting. Cools and dries the grain as it passes through the air.

Used by hundreds of elevator owners. List of users will be sent you upon request.

Write for List and Circulars

MATTOON GRAIN CONVEYOR CO., MATTON, ILLINOIS



30 DAYS' TRIAL

Dust is Dangerous to Health

There is no need to ruin your health or the health of your men by scooping grain in a dirty, dusty car. Our system is not only simple but effective.

You can load every car to full capacity with a Boss Air Blast Car Loader, without any scooping in the car. You can also clean the grain as you load if you desire. O. N. East, of Milmine, Ill., says it is superior to a regular grain cleaner.

We manufacture Stationary outfits for installation in elevators, also portable outfits with and without engines. Prices range from \$150.00 to \$1,185.00. They handle both EAR corn and loose grain. Write for Trial Offer and complete catalog. It will pay you.

MAROA MANUFACTURING CO.,

Dept. G.,

Maroa, Ill.

The Automatic Dump Controller



A Good Business man gives his helpers the best, most up-to-date useful articles he can purchase with which they may perform their duties, with the greatest ease and in the shortest time.

In this way his people become more efficient and more valuable to him.

It is just as necessary for you to give your Friend Dump the most up-to-date and useful equipment with which he may perform his work, in order to procure the desired results in your business.

The McMILLIN Automatic Dump Controller is to your Dump what the Adding machine is to any Business man's Bookkeeper.

Send for Circulars.

L. J. McMILLIN

525 Board of Trade Bldg.
INDIANAPOLIS, IND.



It PAYS TO GRIND ALL GRAINS

Look to the Grinders. They do the work! Bowsher's Cone-Shape grinders are the correct principle in Feed Mill construction. They mean larger grinding surface close to center of shaft; thus More Capacity, Lighter Draft, Longer Life.

"Desire to express my appreciation of the long-lasting, trouble-proof Bowsher. Have used a No. 4 ten years with less than One Dollar per year for repairs." E. W. Watt, Jacobburg, O.

10 sizes; 2 to 25 H. P. Write for free catalogue. G. N. P. BOWSHER CO., SOUTH BEND, IND.

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Grain Dealers Journal

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Passenger or Freight
For Grain Elevators & Mills
Electric or Hand Power
Complete Line

KIMBALL BROS. CO.
1129 Ninth Street
COUNCIL BLUFFS, IOWA

Branches: 202 Scott-Thompson Bldg., Oklahoma City,
610 Delaware St., Kansas City, Mo.

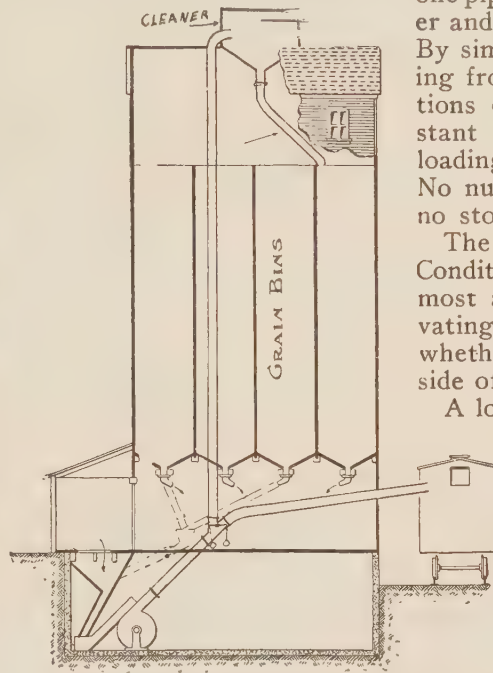
Greeting Cards

Keep up the Christmas Spirit with a message of cheerfulness, do it with holiday greeting cards. A complete set of samples, Business or Personal greetings, or both kinds with price list will be mailed promptly for your inspection.

The American Embossing Co.
193-95 Seneca St. Buffalo, N. Y.

Bernert Grain Blower and Conditioner

Sectional view elevator installation of the Bernert Grain Blower and Conditioner fitted with Combined Elevator and Loader attachment. Note how simple the equipment. Only one machine with



one pipe leading up into the Cleaner and another leading to the car. By simply pulling the chain leading from the valve lever, connections can be switched in an instant from either elevating to loading, or loading to elevating. No nuts or bolts to be loosened; no stops necessary.

The Bernert Grain Blower and Conditioner is the most simple and most adaptable machine for elevating grain and similar material whether stationed inside or outside of building.

A lower or entirely done away with basement, no overhead transmissions, faster and less expensive installation, all operating machinery on the ground floor, less fire hazard, etc., are all points in favor of this system.

Grain is being received the same as with the

bucket system, spouted or dumped direct into the hopper of the machine.

For further information, write for catalog to the

Bernert Mfg. Co.
NORTH MILWAUKEE, WIS.

What Would This Mean In Your Plant?

What would absolutely even and uniform grinding mean — continuous service — no tramming — substantial savings in power and lubricant bills — and absence of maintenance cost?

There's only one answer — profit — or more profit — or both.

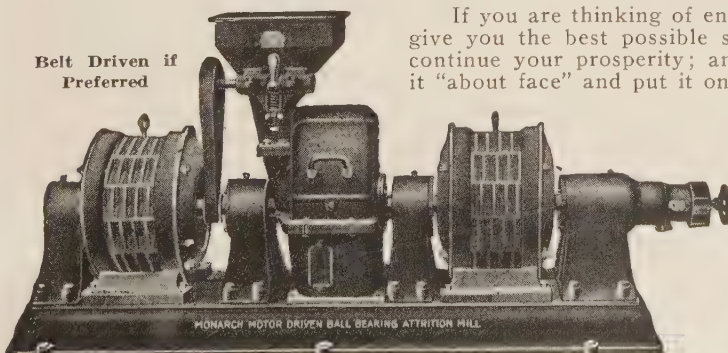
Profit and more profit from feed milling is the substantiated record from every installation of a

MONARCH BALL BEARING ATTRITION MILL

The Monarch is the "big stick" in the feed milling business. It drives out waste and expense, insures the automatic performance of pleasing and profitable results and helps in the cornering of custom and good will.

If you are thinking of entering the feed grinding business, The Monarch will give you the best possible start; if you are already in and prospering, it will continue your prosperity; and if your business is going down hill, it will turn it "about face" and put it on the up grade.

Belt Driven if
Preferred



These are not only promises but proved facts, backed by a guarantee which means what it says from start to finish.

If this means anything to you, write for the catalog now.

Sprout, Waldron & Co.
Milling Engineers

Main Office and Works, P. O. Box No. 26, Muncy, Pa.
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GRAIN ELEVATOR BUILDERS

A Reduced Cost of Handling Grain

makes the Best Elevator obtainable a profitable investment.

Ask for Suggestions on Saving Material, Power and Labor, before completing plans for your new elevator.

30 Years Practical Experience

YOUNGLOVE CONSTRUCTION CO.
Sioux City, Iowa



THE illustration shown is of an elevator designed and built by us. May we build one for you?

Write and tell us when to call and go over the proposition with you.

Corn Belt Engineering & Construction Co.
BLOOMINGTON ILLINOIS

Your Individual Needs are respected when your elevator is designed and built by
W. H. CRAMER CONSTRUCTION CO.
NORTH PLATTE, NEB.
Write for Details of Our System

BIRCHARD CONSTRUCTION CO.
CONTRACTORS GRAIN ELEVATORS
Mills and Warehouses
Especially Designed for Economy of Operation and Maintenance
704 Terminal Bldg. LINCOLN, NEB.

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Modern Grain Elevators
Grain Storage Tanks
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Designers and Builders—Grain Elevators, Mills and Warehouses
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ERECTS ELEVATORS
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Plans and Estimates Submitted
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R. M. VANNESS CONSTRUCTION CO.

Engineers and Builders of

MODERN WOOD and FIREPROOF ELEVATORS

We furnish plans and estimates
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Write us about your requirements
OMAHA, NEB.

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Designers and Constructors of
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CONTRACTOR and BUILDER of GRAIN
ELEVATORS—Wood or Concrete
Special Attention given to Repair Work

If you want regular country ship-
pers to become familiar with your
firm name, place your "ad" here.

SPEAR
Sample Envelopes

Grain Samples Sent
the Spear Way Always
Reach Destination

The value of supplying your customers with clean, promptly delivered samples is obvious. Do not neglect the important factor of good sample envelopes.

Write us for samples and prices today.

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420 N. 3rd St. Minneapolis, Minn.

Folwell-Sinks Form Lifting

JACKS

For Grain Elevator and Silo
Construction

Manufactured and Sold by

NELSON MACHINE CO.
WAUKEGAN, ILL.



Patented

CHECK your reductions of pounds to bushels by using

Clark's Carload Grain Tables

New edition revised and enlarged.

GRAIN DEALERS
JOURNAL
Chicago, Illinois

GRAIN ELEVATOR BUILDERS

The W. H. Wenholz Construction Co.
309 E. 4th Ave., HUTCHINSON, KANSAS

Contractors, Designers and Builders of
Concrete or Wood Elevators
JOBBER'S IN MILL AND ELEVATOR SUPPLIES



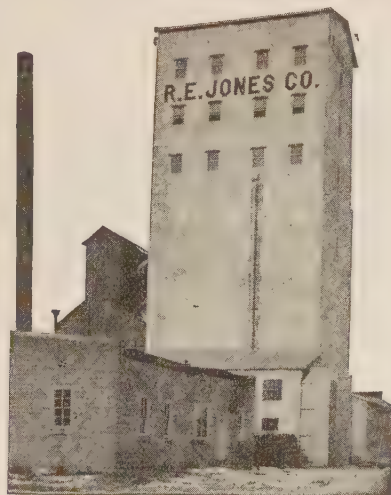
Another Reliance Success

Elevators of our design and construction are the best endorsement of our work. We study your particular problems and build the elevator best adapted to your needs. Write us for further particulars.

Reliance Construction Co.

Board of Trade

Indianapolis, Ind.



R. E. Jones Co., Wabasha, Minn.

We have the most complete organization in the Northwest for the construction of

GRAIN and COAL ELEVATORS

T. E. Ibberson Company
MINNEAPOLIS, MINN.



Design--Construction--Equipment of
GRAIN HANDLING PLANTS

HICKOK Construction Co. MINNEAPOLIS **ELEVATORS**

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ENGINEERS AND CONTRACTORS
Grain Elevators Driers Coal Chutes
Wood or Concrete
UNITY BLDG., BLOOMINGTON, ILL.

FOR
Concrete Elevators
TOWNSEND B. SMITH
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GRAIN ELEVATORS
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THE RYAN CONSTRUCTION CO.
SCHUYLER, NEBRASKA
We build Modern Fireproof Grain Elevators,
Mill Buildings and Storage Bins
SPECIAL ATTENTION GIVEN TO REPAIR WORK
Let Us Know Your Requirements

W. C. BAILEY
Contracts and Builds
Modern Grain Elevators
We can furnish and install equipment in old or new elevators, guaranteeing greater capacity with less power, and positive Non-Chokable working leg. Let us show you
433 Railway Exchange Bldg., OMAHA, NEB.

Concrete Grain Elevators

Designed and Built by

The Monolith Builders, Inc.
Independence, Missouri

GEO. A. SAATHOFF
DESIGNER & BUILDER
OF GRAIN ELEVATORS
MAYER HOTEL PEORIA, ILL.

The paper the Grain Dealer supports, because it supports the Grain Dealer—
GRAIN DEALERS JOURNAL

McMillin Truck and Wagon Dump

The principle on which this dump operates is very simple

It can be operated either by hand or power, through the use of a special friction which hoists as well as lowers the dump. The speed is reduced through a set of gears and sprocket chain to a gas pipe roller which runs across overhead of driveway. This roller is provided with roller bearings. To this roller we attach two ½" cables and run these parallel to whatever distance is required and make the ends fast.

Along these lines of cable we place 7" special open sheaves at intervals of four to five feet (depending on conditions). The slack in this cable can be pulled down between any of these sets of sheaves or the cable thrown off of any set of sheaves and by so doing hoisting can be accomplished at any point along the line of cable.

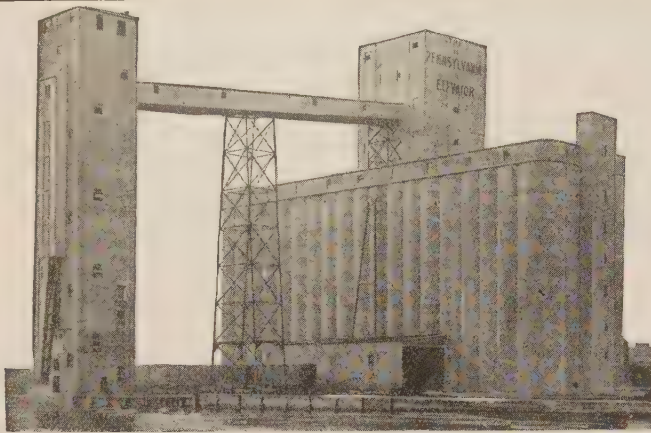
Our specially constructed clamp which is easily attached or detached to either a wagon or truck wheel is provided with a 7" open sheave and can be attached or detached on the cable at any point desired.

In this dump it is easily seen that the one mechanism will dump in any number of openings and it is a small expense to add sufficient cable and sheaves for each additional dump door.

When the dump is not in use it leaves the drive clear for other purposes.

PRACTICAL--SPEEDY--SUBSTANTIAL--INEXPENSIVE

L. J. McMILLIN, 525 BOARD OF TRADE BLDG.
INDIANAPOLIS, INDIANA



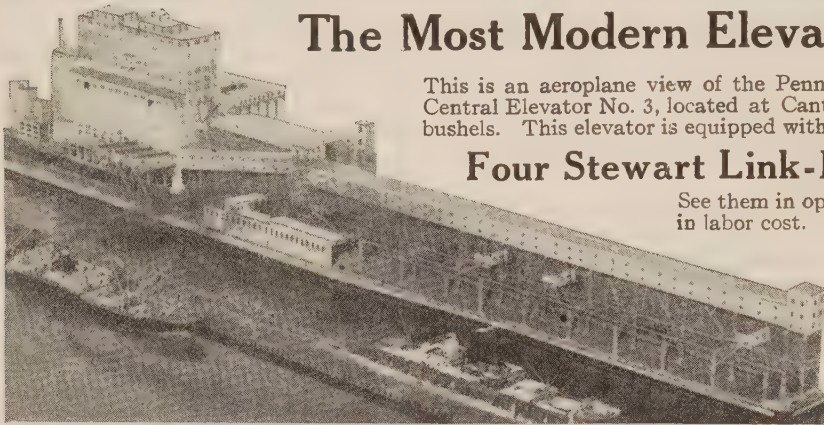
Pennsylvania Railroad Co.'s Terminal Elevator at Erie, Pa. 1,250,000 storage capacity, with marine leg, 25,000 bu. receiving capacity. All concrete, modern construction, with latest improvements.

Designed and built under the direction of

Folwell-Ahlskog Co.

McCormick Bldg. - Chicago, Ill.

Write us for Estimates and Proposals



The Most Modern Elevator in the World

This is an aeroplane view of the Pennsylvania Railroad Company's new Northern Central Elevator No. 3, located at Canton, Baltimore, Maryland, capacity 5,000,000 bushels. This elevator is equipped with

Four Stewart Link-Belt Grain Car Unloaders

See them in operation and be convinced of their great saving in labor cost.

James Stewart & Co., Inc.

Designers and Builders
GRAIN ELEVATORS
In All Parts of the World

Grain Elevator Dept., W. R. Sinks, Manager
1210 Fisher Building, Chicago, Ill.

"We have built for many of your friends. Eventually we will build for you. Why not now?"

GROUP OF TERMINAL ELEVATORS BUILT RECENTLY BY US AT

Port Arthur, Ontario

FOR

The Grain Growers' Grain Company, Limited.

The Saskatchewan Co-operative Elevator Co., Limited.

The James Richardson & Sons, Limited.

THE BARNETT-McQUEEN COMPANY, LIMITED

Designers and Builders of GRAIN ELEVATORS
Offices: Fort William, Ont., Duluth, Minn.
Minneapolis, Minn.

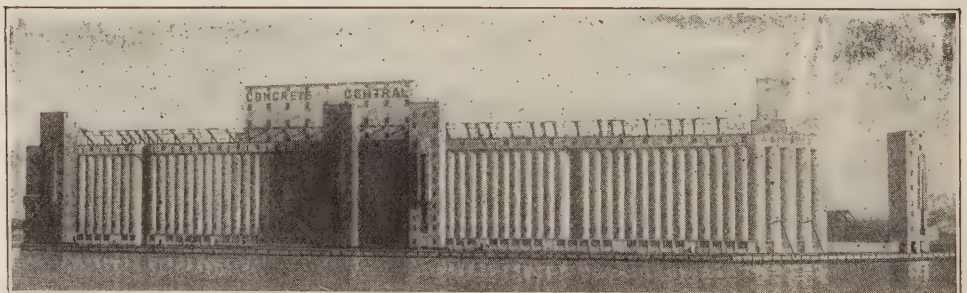


Monarch Built Elevators

assure you economical design, first class work, efficient operation.

SATISFACTION

Let us Submit Designs and Prices



Concrete Central Buffalo, 4,500,000 Bu.

One of the modern houses which has made a record for rapid and economical handling

MONARCH ENGINEERING CO.,

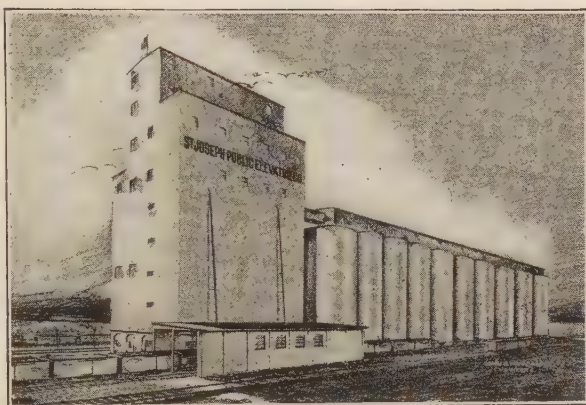
BUFFALO, N. Y.



New York State Barge Canal Terminal Elevator now under construction

\$148,345.00 saved by the State of New York in placing contract for this structure with us.
ADVANCED METHODS—INTENSIVELY DEVELOPED ORGANIZATION—MADE THIS POSSIBLE

FEGLES CONSTRUCTION COMPANY, Limited
Minneapolis, Minn. Fort William, Ontario



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St. Joseph, Mo.

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760-762 Gushard Building
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EFFICIENT ERECTING CO.

We make plans and build up-to-date
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Builders of
GRAIN ELEVATORS and COAL POCKETS
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Special study given to each plant—Each
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Any Size or Capacity
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Brandon Construction Company
The Southeast's Foremost ELEVATOR BUILDERS
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Company

Designers and Builders of
Modern Mills and
Elevators

Flour Mill and Elevator Machinery
SPRINGFIELD, MO.

IF you wish to build your elevator
right, my eighteen years
experience is at your command.

C. E. BIRD & CO.
MINNEAPOLIS MINNESOTA

HOLBROOK, WARREN & ANDREW

Successor to
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Designing Engineers
Reinforced Concrete Elevators. Large or Small
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The STAR ENGINEERING COMPANY
BUILDERS OF GOOD ELEVATORS
WICHITA, KANSAS

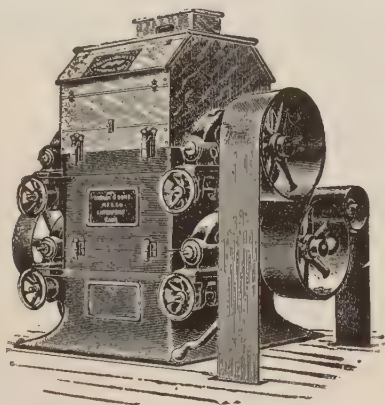
*Write, phone or call us when you are in need of
a Good Country Elevator or Engineering Service*

Successors to the Construction Department of the White Star Company

DIRECT HEAT OR STEAM CONTINUOUS FLOW MADE IN ALL SIZES

Randolph Grain Driers

WIRE WHITE PHONE O. W. RANDOLPH CO. TOLEDO OHIO U.S.A.



More Profits

The elevator owner who installs a feed mill in his elevator puts himself in line for more profits. No other sideline is as profitable. Your farmer patrons must have feed for stock. Are you going to let them go to your competitor? Drop us a line asking for further particulars regarding a feed mill for your elevator.

J. B. Ehrsam & Sons Mfg. Co.
Enterprise, Kansas

HALL SIGNALING GRAIN DISTRIBUTOR

is the best mechanical contrivance known, for distributing grain from the head of a grain elevator leg to the bins or to cars or elsewhere.

Its efficiency makes it economical. Its design convenient.



HALL SPECIAL ELEVATOR LEG

This leg is not a new departure—not an innovation.—not an experiment. It is refinement. It is an assembled device. A complete machine, its parts scientifically arranged and proportioned with every element specially designed for perfect, maximum and uniform performance.

There is not so much difference between a "Hall Special" and the ordinary leg in the work it has to do. The difference is between organization and chaos in doing it.

Hall Distributor Company, 222 Railway Exchange, Omaha, Nebr.

THE WORLD'S LARGEST ELEVATORS USE THIS BRUSH



For sweeping grain cars and elevators, the STAR BRUSH has no equal. Made of stiff selected fibre, 5 inches long. Guaranteed to outwear four or five corn brooms and do cleaner and faster work. Built on hardwood block 14 inches wide and flared to an 18-inch sweep. Largest elevators in Minneapolis, Duluth, Port Arthur and Ft. William now use this brush exclusively.

Order a dozen today. If within sixty days you do not find them entirely satisfactory, send them back. We'll pay the transportation charges both ways.

Price \$16.00 per dozen, F. O. B. Minneapolis

Flour City Brush Company

422-424 South Fourth Street

MINNEAPOLIS

When You Think of Electrical Construction or Repairing—THINK OF

Pierson-Wilcox Elect. Co.

MINNEAPOLIS, MINN.

ESTABLISHED 1898

For elevator and mill supplies we issue a net price catalog. If in the market write us for one.

WHITE ★ STAR ★ CO.
WICHITA, KANSAS



Dust
Collection
is
Profit Collection

When You Install a
NEW "1905"

CYCLONE DUST COLLECTOR

In the first place it is saving the possible total loss of your house by dust explosion; second, the dust and screenings so collected can be sold; and in the third place it effects a saving of one-half in power cost. The revenue thus produced by the collector will pay for it many times over.

Investigate before it's too late.

THE KNICKERBOCKER CO.
JACKSON, MICH.

**For Accurate Moisture Tests
use our Grain Dealers' Air
Tight Cans for forwarding
your grain samples.**
ST. LOUIS PAPER CAN & TUBE CO.
ST. LOUIS, MO.

**OLSON GRAVITY TRUCK
AND WAGON DUMP**

Get our Price and Circulars

HUTCHINSON

KANSAS

TYDEN CAR SEALS

Bearing shipper's name and consecutive numbers.

Prevent

CLAIM LOSSES

10,000 SHIPPERS

Are now using them

Write for samples and prices



INTERNATIONAL SEAL & LOCK CO.

Chas. J. Webb, Vice-President

617 Railway Exchange Bldg., Chicago, Ill.

Cover's Dust Protector

Rubber Protector, \$2.00

Sent postpaid on receipt price; or on trial to responsible parties. Has automatic valve and fine sponge.

H. S. COVER
Box 404 South Bend, Ind.



Wanted and For Sale

The rate for advertisements in this department is 25 cents per type line each insertion

ELEVATORS FOR SALE.

SOUTHEASTERN NEBRASKA—15,000 bushel capacity elevator; sixteen coal sheds. Good coal and feed trade. Good town. Price \$4,000. Address 47Q6, Grain Dealers Journal, Chicago, Ill.

IOWA—THREE elevators in north central Iowa for sale. All doing good business and in excellent territory. Price right. Write quick. Address 46M4, Grain Dealers Journal, Chicago.

SOUTHERN INDIANA—15,000-bushel cribbed elevator on private ground in County Seat town for sale. Good grain, flour and feed business. Address A. H. Richner, Crawfordsville, Ind.

MINNESOTA elevator of 25,000 bushels capacity for sale or rent. Located on C. M. & St. P. R. Coal and Feed business in connection. Address J. J. Dobie, Mapleton, Minn. 46A13.

ILLINOIS elevator, located on I. C. R. R. Good condition, excellent location, large territory. Full particulars and description upon inquiry. Address 47P14, Grain Dealers Journal, Chicago, Ill.

CENTRAL INDIANA 50,000 bushel modern elevator for sale. This elevator is well built and is located in best grain section of the state. Splendid proposition. Address 47R10, Grain Dealers Journal, Chicago, Ill.

WEST CENTRAL OHIO elevator and residence for sale at a bargain. Handle on an average of 65,000 bushels of grain annually, also side lines. \$7,000 will take it. Address 47R30, Grain Dealers Journal, Chicago, Ill.

COLORADO—A fifteen thousand bushel modern, up-to-date elevator for sale in Eastern Colorado. Large acreage; good crops; no competition. Side lines coal, flour and feed. Address 47R11, Grain Dealers Journal, Chicago, Ill.

ILLINOIS—Small flour mill and grain elevators with warehouses, etc., for conducting retail and wholesale flour and feed business in good Illinois town, well located. Address 47N1, Grain Dealers Journal, Chicago, Ill.

OHIO—15,000 bushel Elevator in western part of state for sale. Located in fine grain section and doing a good business in grain, seed, coal, flour and feed. On private grounds. Residence property included. Address 46F8, Grain Dealers Journal, Chicago, Illinois.

MONTANA—Two good cribbed elevators each handling 100,000 bushels of wheat annually besides doing a good coal, retail feed and seed business. Elevators located in the Yellowstone Valley with both irrigated and dry land to draw from; never have had a failure of crops in this territory. Good reasons for selling. If interested write Box No. 1104, Billings, Montana.

TEXAS PANHANDLE mill and elevator—\$100.00 of stock in newly incorporated company for sale. Station will handle 1,200 cars annually this year. Three other dealers. Can handle several sidelines. Unlimited opportunities. Good town and large territory, with development just started. Fine climate and very healthy. Want man with grain experience to buy and take active part in the business. Address Box 225, Texline, Texas.

BARGAIN IF TAKEN AT ONCE:—Someone's always looking for an elevator at a good grain point and read these ads just like you're doing now, so if you wish to dispose of your present property; to enlarge your present interests, or bark in the grain business USE these columns your best advantage just as others are doing. WE WILL assist you in the composition of your free. We are in business to be of service to YOU. There is no wrong time to put an ad in the columns of the Journal. TRY IT.

ELEVATORS FOR SALE.

NORTHWESTERN IOWA elevators wanted—cribbed. Must handle 100,000 bushels or more annually and be priced right. Give complete information and price in first letter. Address 47Q9, Grain Dealers Journal, Chicago, Ill.

WESTERN IOWA—TWO elevators at adjoining stations handling grain, coal and feed. One man can handle both with helper. Both modern equipment, and big grain stations. Address 46M7, Grain Dealers Journal, Chicago, Ill.

NORTHERN INDIANA elevator and implement business on main line of railroad for sale. Doing a good business; electric power. Must sell to dissolve partnership. Address 46L16, Grain Dealers Journal, Chicago, Illinois.

TWO TWENTY thousand bushel capacity elevators, located in North Central Oklahoma. On A. T. & S. F. R. R., at adjoining stations; can be operated from one station. Best crop in years; also handle Coal, Flour, Feed, Seeds and Salt. Wonderful opportunity for right parties. Address Norcenok, Box 9, Grain Dealers Journal, Chicago, Illinois.

CENTRAL ILLINOIS elevator for sale; capacity 30,000 bu., handling 200,000 annually; equipped with a Richardson Auto. scale, 2 dumps, electric power. Office with 5 ton Howe scale, one inclosed lumber shed 40'x80', coal shed. Also 8-room house, with some fruit. Good reasons for selling. Address 47N12, Grain Dealers Journal, Chicago, Ill.

OHIO'S BEST AND CLEANEST ELEVATOR. One of the best grain points in the state. Will sell at less than cost of replacement today. 25,000 bushel capacity, steam power. One other elevator in town. Will handle around 150,000 bushels annually. Good side line business. Good, modern town, 3000 population. For further particulars write Box 459, Hicksville, Ohio.

OHIO 20,000 bu. iron clad electric elevator for sale at a figure below present replacement cost. Built in 1913. Located on Toledo & Western Ry., with the very best service. 4,000 bu. corn crib; coal shed for 5 carloads. Handle flour, feed, fencing, posts, cement, etc. 4 room house and 2½ acres of land included. For further information write Whiteville Elevator Co., Metamora, Ohio.

CENTRAL ILLINOIS elevator for sale. Located on the Illinois Central Railroad, direct shot to Southern markets. Equipped with electric power, three dumps, and all up to date. Capacity 30,000 bushels. Town 650 inhabitants. Elevator handles 200 M. to 250 M. bus. annually. Coal business included. Part of money can remain in property. Price reasonable. Address 47N5, Grain Dealers Journal, Chicago.

NORTHERN ILLINOIS 10,000 bu. elevator, office attached, for sale; handle 200,000 bushels annually. Dump scales, International engine, cleaner, seed mill. Electrically lighted. Located in Ogle County, on C. & G. W.; railroad ground. Warehouse for seeds, feed, cement; 300 ton coal shed, two lumber sheds, one lath and lumber shed, enclosure for wire, posts, etc. Good wagon house and barn. Have handled over 40 carloads of merchandise annually for past 5 years. Modern 8 room house surrounded by fruit trees. Terms for half purchase price of entire property. A paying proposition—priced right. Owner must go west for health. Address 47R25, Grain Dealers Journal, Chicago.

ELEVATORS FOR SALE.

ALABAMA elevator doing fine wholesale grain and feed business, in town of 20,000, for sale. Also corn mill. Address 47R23, Grain Dealers Journal, Chicago, Ill.

IOWA elevator of 22,000 bushels capacity for sale or rent. Located on three railroads. Coal and feed business in connection. Address 47Q24, Grain Dealers Journal, Chicago, Ill.

FOR SALE—Elevator in best wheat belt in Kansas. 15,000-bushel house. Crops in fine condition. \$3,000 quick sale. Address 46J28, Grain Dealers Journal, Chicago, Illinois.

MARYLAND elevator 16,000 bu. capacity and large feed warehouse for sale. Two dwellings included. Located at Linwood and is best equipped one in western Maryland; on main line of Western Maryland R. R., near Baltimore. Address C. M. Horst, Hagerstown, Md.

NORTHERN MISSOURI elevator for sale. No competition. Big corn and wheat territory. Good flour, feed and coal business. A hustler can pay for plant in a year. Will sell for \$11,000.00. Some terms. Better investigate quick. Address 47R26, Grain Dealers Journal, Chicago, Ill.

KANSAS 4,000 bushel elevator and 50 barrel flour mill for sale. Located in the heart of the wheat section at Argonia. All buildings are built of concrete, tile and cement blocks covered with "V" crimped galvanized iron. Equipment is practically new. Good crop of wheat this year. This is a splendid proposition and an excellent locality for business. Write J. G. Pearce, Argonia, Kansas.

ELEVATORS WANTED.

WANT TO BUY country elevator or mill. Give full details. Address 47R4, Grain Dealers Journal, Chicago, Ill.

WILL BUY ELEVATORS showing good business. Address E. W. Davis, 513 North Seminary street, Galesburg, Ill.

ELEVATOR WANTED to lease, by experienced grain man. Give full particulars in first letter. Address 47R18, Grain Dealers Journal, Chicago, Ill.

WANT TO BUY for cash, elevator in Ohio or Indiana. Prefer Farmers Equity that wants to quit the grain business. Give all information possible in first letter. No dead ones need apply. Address 403 W. Fayette St., Celina, Ohio.

FARM LANDS FOR EXCHANGE.

WILL TRADE 240 acre farm and valuable town property for elevator in North Dakota or Minnesota. F. J. Johnson, Menahga, Minn.

WANTED—To trade half section good farm land in Oklahoma for elevator in Northern Oklahoma or Southern Kansas. Address P. O. Box 788, Enid Oklahoma.

BUSINESS OPPORTUNITIES.

PLANT OR INTEREST FOR SALE.

Complete new feed milling and mixing plant in eastern city of over 100,000 people, with own R. R. siding; milling-in-transit facilities available; good business, established many years ago and widely known. Is at limit of expansion for lack of capital. Will sell out entirely, part or controlling stock to get larger working capital. Worth investigating. Address 47Q1, Grain Dealers Journal, Chicago, Ill.

ELEVATOR BROKERS.

JOHN A. RICE ELMER N. SMITH
Elevator Brokers, Frankfort, Indiana.

WANT TO HEAR from owner having elevator or other business for sale. State cash price and particulars. John J. Black, 57th St., Chippewa Falls, Wisconsin.

CLAYBAUGH-McCOMAS
Offices

Frankfort, Ind. Indianapolis, Ind.
223 B. of T. Bldg. 601 Board of Trade.
If you want to buy, sell or trade an elevator write us at either address.

ALWAYS HAVE ELEVATORS For Sale in the Grain Belt of Illinois and Eastern Indiana. If you are in the market write me fully and I will try and satisfy you.

JAMES M. MAGUIRE.
432 Postal Tel. Bldg., Chicago, Ill.

MICHIGAN ELEVATOR BROKER.

Have several first class purchases in Michigan elevators handling beans, grain, hay, potatoes and good retail business.

Special value: a fifty barrel flour mill, own water power; two acres of land; on two railroads. Wonderful proposition.

A. L. Chamberlain,
Port Huron, Mich.

HELP WANTED.

WANTED—An experienced grain man to manage farmers' elevator. Address Farmers Co-operative Grain Ass'n, Fairmount, Ill.

WANTED—Experienced retail garden seed man. City of 12,000. Store operated in connection with large wholesale seed, grain, feed and fertilizer business. Unlimited opportunity for advancement. Will give free hand and ample backing to right man. Address 47Q3, Grain Dealers Journal, Chicago, Ill.

WANTED—Good live, experienced man to take a working interest in well established, rapidly growing seed business. Plant equipped with best new and modern cleaning machinery. Located in the heart of the clover seed producing section of Minnesota. At present handling all seeds, grain, potatoes and machinery. Investment absolutely secured. For details address 47P7, Grain Dealers Journal, Chicago, Ill.

MISCELLANEOUS.

FOR SALE—50 ft. 14 in. 22 gage galvanized spout. Practically new. Half price of new. Nickel Grain Company, Valparaiso, Indiana.

FOR SALE—Pulleys—1,000. ALL sizes, solid cast iron, wood and steel split. Standard Mill Supply Co., Kansas City, Mo.

THE LA RUE FARMERS EXCHANGE CO., La Rue, Ohio, is now ready to ship hay. They will be glad to receive bids from dealers who wish to buy.

10,000 elevator brooms. Will sacrifice at half cost price. 50c apiece in lots of 100 only. Wire your orders before the stock is gone.

Jos. Werner,
70 Pliny Street, Hartford, Conn.

SITUATIONS WANTED.

WANTED—Grain, Lumber and Implement man, 10 years' experience, wants position in central or western Kansas. Address 47Q17, Grain Dealers Journal, Chicago, Ill.

GRAIN BOOKKEEPER - ACCOUNTANT, thoroughly experienced in all departments of the business, and capable of taking entire charge of office, is available for position in any terminal market. First-class references in Minneapolis grain trade and elsewhere. Address P. O. Box 434, Minneapolis, Minn.

ASST. MGR. position with Farmers Elevator or Line House wanted, or as second man where manager needs help with books. Handy with tools and machinery. Wages reasonable. Married. Four years' experience as manager. Prefer Illinois or Wisconsin, in town with good school. Address 47R21, Grain Dealers Journal, Chicago, Ill.

MAN WITH TWENTY years' experience in the grain business wants position with good firm. Have managed Line and Farmers' elevators; familiar with side lines. Capable of handling branch office, or road position; traveled one season. Now employed—good reasons for desiring change. Address 46L31, Grain Dealers Journal, Chicago, Illinois.

WANTED—Position as manager of Farmers Elevator or line house. Have had six years' experience in handling all kinds of grain, live stock feed, flour and fuel. Am married; age 30. Employed now. Desire a change. Would come on trial. Best of references. Address 47Q15, Grain Dealers Journal, Chicago, Ill.

WANTED—Position as traveling representative for some good, reputable grain commission firm; Chicago preferred. 10 years' grain experience in country and terminal elevators. Can furnish best of references. Now employed, but for very good reasons wish to change. Am located in good grain territory and would want to keep my present home. Address 47Q16, Grain Dealers Journal, Chicago, Ill.

OFFICE APPLIANCES.

SAFES—Large stock of new and used safes on hand. Protect your valuable papers. Prices reasonable. The Howe Scale Co. of Ill., 512-514 St. Charles St., St. Louis, Mo.

USED CHECKWRITING Machines, wholesale, retail, all makes. Dealers and corporations needing one to fifty,—write me. R. B. Allen, 519 Farnham Building, Omaha, Nebraska.

FLOUR FOR SALE.

MIXED CARS of flour and mill feeds in 100 pound sacks are our specialties. We are now manufacturing a full line of corn goods, cracked corn, feed meal, corn and oats chop, Ohio Farm feed, shelled corn and standard oats in connection with our flouring mill. Would like to send you a trial to convince you of the superiority of our products. Ansted & Burk Co., Springfield, Ohio.

Helpful Books

FOR

Carlot Grain Handlers

Clark's Fractional Values: This table is on heavy cardboard. Size 9½x11 inches, showing the value of any quantity from 1 to 50,000 bushels, at ¼, ⅓, ½, ⅔, ¾ and ⅞ cents. The amount of bushels is shown in red and the value in black. They are the most conveniently arranged tables for showing fractional values or bushels. Price 25 cents.

Clark's Freight Tables: Show the freight rates per bushel from a given rate per hundred pounds, when the rate is from 2 to 50½ cents per hundred pounds, by one-half cent rises. The table is printed in two colors on heavy bristol board, size 7x9 inches, and may be used for determining the freight per bushel of 60, 56, 48 and 32 pounds. Price 25 cents.

Clark's Double Indexed Car Register: Is an index designed to afford ready reference to the entry or record of any car. Facing pages, 11x16 inches of heavy ledger paper are each ruled into five columns, those on the left being numbered 0, 1, 2, 3 and 4, while columns on the right are numbered 5, 6, 7, 8 and 9. Each column is ruled into three distinct divisions with the following sub-headings: "Initial," "Car No." and "Record." The marginal index figure represents the right hand or unit figure of the number entered, while the column heading is the second or tens figure. The required number can be instantly found if properly entered. Form No. 40, with space for 12,000 cars \$2.00. Form No. 42, with space for 21,600 cars \$3.00.

Purchase & Sale Contracts is a new book designed to meet an ever increasing demand for a record which will enable the dealer to balance his purchases and sales contracts to determine instantly whether he is long or short. Left hand purchase page column headings are Date, From Whom Bot, Bushels, Grade, Delivery, Price, By Whom, How and Remarks. Right hand sales page column headings are Date, To Whom, Bushels, Grade, Shipments, Price, By Whom, How and Remarks. Book contains 80 double pages, size 8½x14 inches, ruled and printed on heavy ledger paper and well bound in heavy canvas. Order Form 18 P&S. Price \$2.75.

Clark's Decimal Grain Values: Saves time and money and prevents errors. It shows at a glance, or with simple addition, the cost of any quantity of grain from 10 to 100,000 pounds at any given market price and reduces pounds to bushels on the same page. Values are shown directly from pounds without reducing to bushels. Pounds shown in red figures and values in black; price being given at top and bottom of each page. Prices for oats range from 10 cents to 79 cents a bushel; for corn, rye and flaxseed, 10 cents to \$1.09 per bushel; for wheat, clover, peas and potatoes, 30 cents to \$1.59 per bushel; for barley and buckwheat, 20 cents to \$1.49 per bushel. Order Form No. 36. Price \$6.00.

Use Universal Grain Code and Reduce Your Telegraph Tolls.

GRAIN DEALERS JOURNAL,
305 So. LaSalle St., Chicago, Ill.

CAREY-IZED SALT—A BETTER SALT FOR EVERY PURPOSE



Let us quote you on a car assorted to meet your requirements.

OMAHA NEB.
No. 4. Grain Exchange

THE CAREY SALT CO.

HUTCHINSON, KANS.
No. 4. Carey Bldg.

MACHINES FOR SALE.

FOR SALE—Morse Chain Drives, Motors, Belting, Hoists, Rope. Address Morse Engineering Co., St. Louis-Kansas City.

FOR SALE—Several No. 8 Boss Car Loaders good as new—complete ready to install. Address A. H. Richner, Crawfordsville, Indiana.

FOR SALE—1 Grain Cleaner, 100 bu. capacity; 1 14" Meadows Burr; 1 20" Nordyke & Marmon Burr; 1 two-hole Power Corn Sheller. In A1 condition. Bohannon & Son, Richland, Mo.

FOR SALE—One Boss Car Loader complete, \$50.00; one Barnard & Leas Size 1 Corn Cleaner, in good condition, \$75.00. Address 47N18, Grain Dealers Journal, Chicago, Ill.

FOR SALE—22-in. Robinson single runner attrition mill in good condition—run very little—belt drive. O. C. Bowers & Co., Oklahoma City, Oklahoma.

FOR SALE.

New Eureka Counter Balanced Corn Cleaner, size 361, 1,200 bushels capacity. Never been used. Reasonable price. Also large list of milling equipment in excellent condition.

Mead Johnson & Co., Evansville, Ind.

FOR SALE—New, never used No. 116 Clipper Seed Cleaner; crated; latest model; complete set seed sieves. Not used owing to illness of seed man. Will go at 10% less new list price, guaranteed in perfect condition, f. o. b. cars Brillion, Wisconsin. Brillion Mill & Elevator, Brillion, Wis.

FOR SALE—One King Buckhorn Machine, Model M, the practical size for the seed dealer that handles seed in large quantities. This machine is practically new, having cleaned less than 300 bu. seed, and is in first class order. Our seed crop is a failure this year and will not need this machine. Will sell it at a great sacrifice if sold at once. If interested write O. L. Barr Grain Co., Bicknell, Indiana.

HAVE NO KICK COMING: Cancel ad. We are so flooded with replies that we will be kept busy for months to come. We certainly were glad to know that every morning brought us queries for our equipment and always in their letters they would say that they noticed the ad in the Grain Dealers Journal. It is a great thing for us as long as we can supply the demand.—E. J.

REAL BARGAINS.

Prompt Attention. Quick Shipments. When in need of elevator or mill machinery, notify us. We are headquarters for power and transmission equipment, and have on hand several well-known makes of motors, boilers, engines, etc.

Send us list of all your wants. We can supply you with full line of machinery for elevators, flour, corn and cereal mills. Complete equipments for modern mills of all kinds, no-lashes stock, and poultry feed plants, plans specifications, flow sheets, etc., our specialty.

Write us without delay.

Geo. J. Noth, Mgr.,

9 S. Clinton St., Chicago, Ill.

MACHINES FOR SALE.

FOR SALE—28½ ft. 7 in. black well casing at reduced price. Suitable for loading spout. Write J. S. Cameron, Elliott, Ill.

FOR SALE—Ellis Grain Drier; perfect condition; right size for elevator or mill. Bargain price. The Lena Grain Co., Conover, O.

FOR SALE—One No. 54 Combination Western Cleaner; cleaned 400 bu. grain. Guaranteed as good as new. Box 147, Valparaiso, Ind.

FOR SALE—No. 3 Monitor grain cleaning mill and No. 2 Invincible Compound Shake Double Receiving Separator—\$150.00 each. In good condition. Forced to sell account too small for our needs. Gardner Seed Company, 39-43 Spencer st., Rochester, N. Y.

FOR SALE: One Monitor Model Separator, complete with motor, grain and seed screens; just the thing for testing grain samples.

One Timers all steel parcel post scale. Will weigh parcels up to 120 lbs. Address Toledo Produce Exchange, Toledo, Ohio.

FOR SALE

One 2 pair high 6x20 new roller mill.....\$455.00
Two 25 h. p. Mogul, special electric engines, run six months, each..... 450.00
One 300 bu. Monarch Hopper Scale..... 80.00
One 50 bu. Monarch Hopper Scale..... 60.00
One 150 bu. Monarch Hopper Scale..... 65.00
One 150 bu. Howe Hopper Scale..... 90.00
One 300 bu. Fairbanks Hopper Scale..... 120.00

The White Star Company,

209-211-213 North St. Francis, Wichita, Kans.

PORTABLE ELEVATOR and equipment for sale. I have for sale a complete outfit for loading grain in the car where no elevator is available. It includes one 32-foot portable John Deere grain elevator and loader, one 5 h. p. Waterloo engine, moisture tester, dockage sieves, tester kettle, probe, about 500 sax, a complete set of books for the grain business; in fact I have an outfit ready for some one to start handling grain at once. Will load ear corn, wheat, shell corn or slack coal. This equipment is all in good condition, having been used to load about ten cars wheat last year. Will sell cheap if taken at once, as I have gone out of business. O. L. Barr, Bicknell, Ind.

MACHINERY BARGAINS

1—20" Unique Ball Bearing Belt Driven Attrition Mill, complete with A-4 drive.
2—24" Unique Ball Bearing Belt Driven Single Runner Mills.
3—24" Unique Ball Bearing Belt Driven Attrition Mill, complete with A-4 drives.

1—McFeely Middlings Grinder.

1—Double Stand 9x18 Allis Rolls.

1—Double Stand 6x16 Allfree Rolls.

1—Excelsior Bran Duster.

1—Cutler Drier.

1—Allis Rolling Screen.

2—No. 2 Iron Clad Crushers.

2—Little Wonder Reels.

1—No. 15 Perfection Dust Collector.

1—No. 22 Perfection Dust Collector.

The above machinery is in good condition and is for sale at attractive prices. Write us for further information and prices. L. R. Veatch, 416-418 Prudential Bldg., Buffalo, N. Y.

MACHINES FOR SALE.

FOR SALE—Durable wire rope for car shovels; cast or plow steel. Manila rope, elevator buckets and everything in elevator supplies. Standard Mill Supply Co., Kansas City, Mo.

FOR SALE—1 used Emerson Kicker; 3-shoe, hand power, with all necessary appliances. In perfect condition. Guaranteed. Will sell cheap. Apply Texas City Board of Trade, Texas City, Texas.

OIL AND GAS ENGINES.

FOR SALE—One 45 h.p. Primm Fuel Oil Engine. Run less than 300 hours. Will sell at a reasonable price. If interested write Titus Bros., R. 6, Springfield, O.

FOR SALE—Used 25 h.p. Fairbanks-Morse type "N" gasoline engine. Can be equipped for kerosene. Cheap. Address 47Q8, Grain Dealers Journal, Chicago, Ill.

FOR SALE—40 h.p. Muncie Oil Engine, guaranteed running condition; alrstarter and pump. Minster clutch, oil tank, all excellent condition. Invite inspection. Address Milford Grain & Mlg. Co., Milford, Ind.

DYNAMOS—MOTORS.

FOR SALE—One G. E. 55 h.p. compound D. C. motor, 845 r.p.m., voltage 220. Renders splendid service and good for many years yet. Address 47P15, Grain Dealers Journal, Chicago, Ill.

SCALES WANTED.

WE ARE in the market for one or two second hand, dormant dial warehouse scales; capacity 1,000 lbs., minimum platform measurements 4'x4'. Prefer Fairbanks or Columbia type. State price crated f. o. b. railroad, condition and warranty first letter.

George O'Dwyer, Inc., St. Maries, Idaho.

ENGINES WANTED.

WANTED—Used Otto or Fairbanks-Morse engines. State condition and price. Address 47N23, Grain Dealers Journal, Chicago.

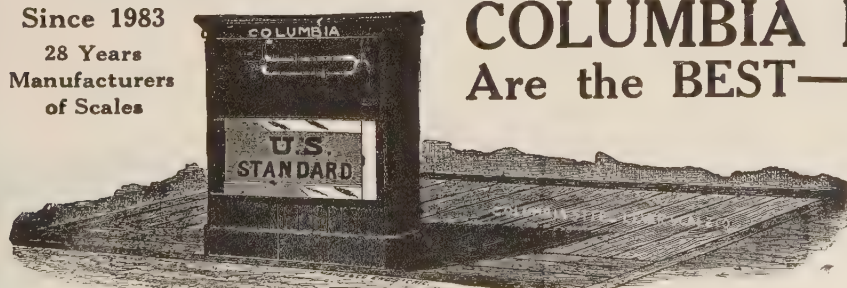
SCALES FOR SALE.

WANTED—Two 60 bushel Hopper, Fairbanks, Buffalo or Howe scales and one double motored Threshing Mill, motors 15 or 20 h.p. Address The Twining Bean & Grain Company, Bay City, Michigan.

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CHICAGO, ILLINOIS

Save money and send for list of our guaranteed rebuilt scales. All makes and capacities. Tell us what you want. Let us repair your scales—any make. We also carry parts. Finest equipment for scale work in Chicago.

BAGS—BAGGING—BURLAP.

FOR SALE—2500 second-hand cotton grain bags, 16 oz., 25c each f. o. b. St. Louis, large or small lots. Foell & Co., 123 Market St., St. Louis, Mo.

BURLAP BAGS OF EVERY KIND FOR SALE; new or second-hand, plain or printed with your brand; seamless Cotton Grain Bags; Sample Bags; Burlap, Cotton Sheeting or Paper for Car Lining, etc. Wanted: Second-hand bags, best prices paid.

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ELEVATOR OPERATORS who have wool to ship will find it to their advantage to tell the 6700 regular readers what they have to offer. An advertisement in this column will cost you but 25c per type line per insertion.

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MILLET SEED in car lots for sale. Early Fortune, Hog, Common (golden) and Siberian. Reimer Smith Grain Company, Holyoke, Colo.

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**THE
ILLINOIS SEED CO.
CHICAGO, ILL.**

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Field Seeds

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Mail Samples for Bids

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These books are slightly soiled and shelf worn. To clear out these books promptly we are offering them at greatly reduced prices.

RAILROAD CLAIM BLANKS—TWO 411-A, for Loss of Weight in Transit. These books contain 100 blanks in duplicate, with 4 sheets of carbon paper bound in back. Corners of each book slightly bent. Order Form 4112-A. Price each \$1.50.

GRAIN DEALERS JOURNAL.

305 South La Salle St., Chicago, Ill.

**Here's a record from
The Northwest**

The General Manager of a prominent Grain & Elevator Company in the Northwest wrote us recently:

"Otto engines have always given us much better satisfaction than any other engines we have used. Any time we can get an Otto for elevator work we are not going to consider anything else."

Otto owners all over the country have reported thousands of similar records—20 years is not at all unusual. Many of them add, "The engine seems good for twenty more years."

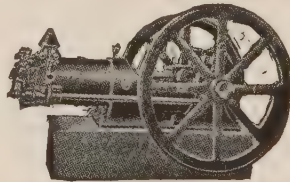
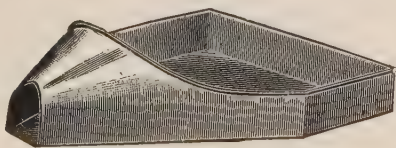
If you are interested in stationary engines—gas, gasoline, or kerosene—you need the Otto catalogue. Drop us a postal for your copy.

Models up to 50 H. P. in stock.

Otto Engine Manufacturing Co.

3219 Walnut Street, Philadelphia, Pa.

15-17 S. Clinton St., Chicago, Ill.

**SEED SAMPLE PANS**

Made of sheet aluminum, formed by bending, reinforced around top edge with copper wire. Strong, light and durable. The dull, non-reflecting surface of the metal, which will not rust or tarnish, assists the user to judge of the color and detect impurities.

Seed Size, 1½x9x11". Price \$1.65.

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GRAIN DEALERS JOURNAL, 305 So. La Salle St., Chicago, Ill.

KEEP POSTED

**GRAIN
DEALERS JOURNAL**

305 So. La Salle St., Chicago, Ill.

Gentlemen:—In order to keep us posted regarding what is going on in the grain trade outside our office, please send us the *Grain Dealers Journal* on the 10th and 25th of each month. Enclosed find Two Dollars for one year.

Name of Firm.....

Capacity of Elevator.....

Post Office.....

bus.

State.....

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Wm. E. Busgers & Co., European fancy natural gr. seeds.

ATCHISON, KANS.

Manglesdorf Seed Co., The, wholesale seeds.

BALTIMORE, MD.

Wm. G. Scarlett & Co., wholesale seed merchants.

BUFFALO, N. Y.

Stanford Seed Co., Inc., The, wholesale field seeds.
Whitney-Eckstein Seed Co., wholesale seeds.

CHICAGO, ILL.

Dickinson Co., The, Albert, seeds.
Illinois Seed Co., The, grass and field seeds.

CINCINNATI, OHIO.

McCullough's Sons, The, J. M., field and garden seeds.

CONCORDIA, KANS.

Bowman Bros. Seed Co., field seeds.

CRAWFORDSVILLE, IND.

Crabbs Reynolds Taylor Co., grass and field seeds.
Crawfordsville Seed Co., seed merchants.

ELKHART, KANS.

Muncy & Carson, grain and seeds.

KANSAS CITY, MO.

Penpard Seed Co., J. G., wholesale seeds.
Tobin Seed Co., alfalfa—bluegrass.

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Louisville Seed Co., clover & grasses.

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Nungesser-Dickinson Seed Co., wholesale seed merchants.

PHILADELPHIA, PA.

Philadelphia Seed Co., Inc., The, wholesale field seeds.

ST. JOSEPH, MO.

Mitchellhill Seed Co., clover, grasses, sorghums.

ST. LOUIS, MO.

Manglesdorf, Ed. F. & Bro., wholesale field seeds.

ST. PAUL, MINN.

Jameson Hevener Co., shippers of field seeds.

TOLEDO, OHIO.

Churchill Grain & Seed Co., field seed, pop corn.
Flower Co., The, S. W., seed merchants.
Hirsch, Henry, wholesale field seed.
Toledo Field Seed Co., The, clover, timothy.

HAY WANTED.

SEEDS FOR SALE—WANTED

HENRY HIRSCH
WHOLESALE FIELD SEEDS
CLOVER — ALSIKE — TIMOTHY — ALFALFA
Our Specialty
All Other Field Seeds
TOLEDO - - OHIO

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Field and Garden Seeds
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Clover, Timothy, Grass Seed
Grain Bags

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INCORPORATED
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Headquarters for
RED TOP AND ORCHARD GRASS
BUYERS AND SELLERS
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CORRESPONDENCE INVITED

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"THE HOUSE OF QUALITY"

HEADQUARTERS
on all
Imported
Clover
Grass and Field
SEEDS

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(Formerly Loewith, Larsen & Co.)

Missouri Grown Blue Grass
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We are now prepared to accept orders for both
TOBIN SEED CO.
KANSAS CITY, MO.

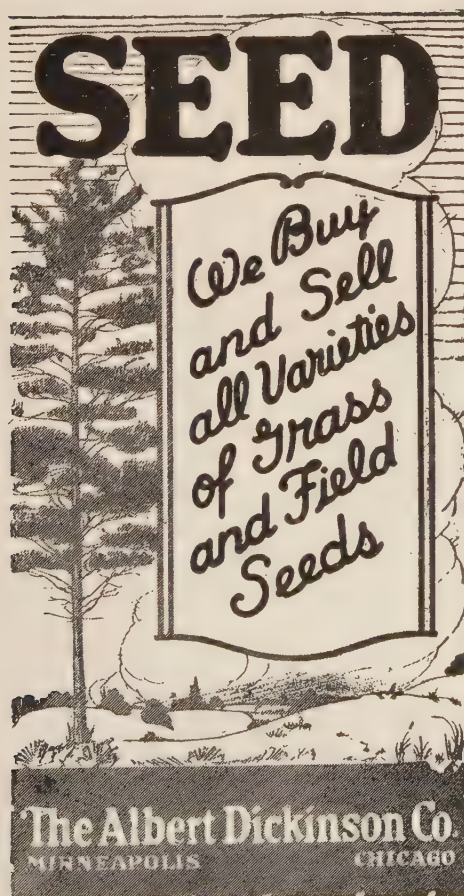
The S. W. Flower Co.
WHOLESALE
FIELD SEED
MERCHANTS
SPECIALTIES
RED CLOVER, TIMOTHY
ALSIKE
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The Stanford Seed Company, Inc.
Wholesale Field Seeds ... **BUFFALO, N. Y.**

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FIELD SEEDS
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CRAWFORDSVILLE, INDIANA
Buyers and Sellers
CLOVER AND TIMOTHY SEED—GRAIN

The Toledo Field Seed Co.
Clover and Timothy Seed
Consignments solicited. Send us your samples
TOLEDO, OHIO



SEED

We Buy and Sell all Varieties of Grass and Field Seeds

The Albert Dickinson Co.
MINNEAPOLIS CHICAGO

I COULD NOT get along without the Grain Dealers Journal.—F. Sieck, mgr. Craig Farmer's Milling & Elevator Co., Craig, Colo.

Do You Know

—that Milwaukee's grain receipts average about 80,000,000 bushels annually?

—that 21,000,000 tons of freight are moved into and out of Milwaukee annually?

—that Milwaukee elevators can store 20,000,000 bushels of grain?

—that the receipts of coal at Milwaukee are 5,000,000 tons annually?

—that Milwaukee is one of the largest clover seed markets in the United States?

—that Milwaukee is the second largest primary oats market in the United States?

—that ocean steamers sail from Milwaukee direct to the ports of Europe?

—that 50% of the grain received in Milwaukee is consumed or ground here by the mills?

These are just a few of the many pertinent facts that combine to make Milwaukee the logical market for your grain shipments.

A line to any of the Chamber of Commerce Members shown here will be the means of your getting more information about this market.

Blanchard Grain Co.

Flanley Grain Co.

Updike Grain Co.

J. V. Lauer & Co.

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MILWAUKEE

GRAIN DEALERS JOURNAL

305 So. LaSalle Street, Chicago, Ill., U. S. A.
Charles S. Clark, Manager

Published on the 10th and 25th of each month in the interests of better business methods and improved handling facilities for progressive wholesale dealers in grain and field seeds.

SUBSCRIPTION RATES to United States, semi-monthly, one year, cash with order, \$2.00; two years, \$3.60; three years, \$5.00; single copy, 15c.

To Foreign Countries within the Postal Union prepaid, one year, \$3.25; to Canada and Mexico, \$2.75.

THE ADVERTISING value of the Grain Dealers Journal as a medium for reaching progressive grain dealers and elevator men is unquestioned. The character and number of advertisements in each number tell of its worth. If you would be classed with the leading firms catering to the wholesale grain trade, place your announcements in the Journal.

Advertisements of meritorious grain elevator machinery and supplies and of responsible firms who seek to serve grain dealers are solicited. We will not knowingly permit our pages to be used by irresponsible firms for advertising a fake or a swindle.

LETTERS on subjects of interest to those engaged in the grain trade, news items, reports on crops, grain movement, new grain firms, new grain elevators, contemplated improvements, grain receipts, shipments, and cars leaving grain in transit, are always welcome. Let us hear from you.

QUERIES for grain trade information not found in the Journal are invited. Address "Asked-Answered" department. The service is free.

CHICAGO, AUGUST 25, 1921

REDUCED freight rates would save many bushels of corn from the flames and increase the volume shipped.

LOWER freight rates on grain will shortly increase the value of grain at country points so it may pay to hold for the rise.

TESTING of scales at elevators in Montana is now being done politically. That is, a recent law empowers the state dept of agriculture to do the work and it is to be assumed that politics will be the controlling test in employing scale "experts."

NOT ONE load of grain in a thousand, as it comes from the farm, is in condition for immediate use in the manufacture of the multitude of products that come from grain. Nor is it properly located. Therein lies two reasons for grain dealers and grain elevators.

THE BILLION DOLLAR appropriation to assist and encourage the exportation of American farm products is now law and the agricultural bloc in the U. S. Senate may be willing to permit citizens engaged in other lines of industry to proceed with their usual labor without any further discouragement from the government.

WEEVIL infesting wheat has caused many losses to unsuspecting shippers recently. If you cannot get rid of the weevil, at least run your wheat through your best cleaner the day before loading and give it a thorough blowing. Country shippers cannot afford to overlook the fact that a dirty elevator is the grain weevil's paradise.

"SIGNED without reading" is the statement credited to one grain grower's agitator when he told of the action of eight farmers whom he visited. It is to be expected. Have any who read and studied also signed?

IT IS NOT too early to begin making preparations for closing the books at the end of the year. Carefully thought out plans will result in the acquisition of information that may enable the dealer to serve his community better and more profitably next year.

IF YOU paid for the erection of a modern fire-proof grain elevator upon a foundation of your own construction, and it began settling and wobbling about before completion, would you put grain into it or a new foundation under it? Better think this over before you let the contract, it may help you to secure a dependable foundation.

PROSPECTIVE builders of grain elevators can not afford not to take advantage of the construction failures published in the Journal from time to time. Expensive failures are becoming so numerous as to be alarming. All serve to emphasize the advantage of investigating the work of unknown builders and of having plans carefully worked out before starting to build.

A PROBLEM of elevator operation that should be solved at the time of building is that of getting grain from dump sinks or bins to the boot without difficulty. Many elevator men are forced to labor with unsatisfactory means for spouting grain to the boot, altho this should be one of the things uppermost in mind when the design is being worked out. The pitch of spouts must be considered, and this will be influenced by the location of the several bins. In practically every case it is but an invitation to trouble to adopt a plan that requires the installation of spouts having one or more turns between bin and boot. An effort should be made to have short, straight spouts, with plenty of fall, even tho that necessitates the use of a conveyor. The better method is to make all spouts discharge into the front or "up" side of the boot.

THE EXPOSURE HAZARD of grain elevators is generally increasing at country stations with the age of the town or village, because age generally begets a larger number of citizens and more industries, and naturally these crowd in on the elevator in order to take advantage of the switching facilities of the railroad. Elsewhere in this number is illustrated a North Dakota fire which resulted in the complete destruction of three perfectly good elevators. Had the middle house been constructed of non-combustible material or completely covered with non-combustible material it seems very likely that two of them would still be standing. Three other elevators, at Langford, South Dakota, were recently burned by one fire, and another news item in this number tells of the burning of two houses at Eckman, North Dakota, one of which was sacrificed through exposure. But the greatest loss credited to exposure was told of in our Oregon news items of the July 25th Journal, in which one fire destroyed five grain warehouses and an elevator, all built too close for safety.

A FORECAST that is as good as some for which real money would be charged: Prices, being now so low, must inevitably go higher unless they stay where they are or continue downward.

FREIGHT rates higher than the traffic will bear may still be insufficient to insure the carrier a reasonable return on its investment, according to a speaker at the Chicago convention of the National Hay Ass'n. Reflection will disclose that herein lies a fully developed "vicious cycle" and the end is not in sight.

IDAHO shippers hereafter will get car of the capacity wanted and ordered or else be charged transportation only on the amount ordered or loaded, just like shippers in other states. Until recently the autocratic railroad officials of Idaho have insisted upon shippers accepting any car that was available and paying for its minimum whether loaded to that capacity or not.

CLAIM AGENTS, who collect shippers' claims against railroads and then neglect to remit balance due shippers deserve to be shown up to the trade so that others may not fall prey to their trickery. Not only should such sharpers be shown up to the trade through trade journals, but the shippers who know the facts should take advantage of every opportunity to enlighten their brother dealers on the practices of the so-called claim agents.

RECEIPTS of wheat at southwestern markets have fallen off somewhat, but spring wheat is moving to market in the northwest in sufficient volume to keep the total considerably above the receipts in similar periods last year. On Tuesday, Aug. 23, three southwestern and two northwestern markets received 1,492 cars of wheat. Receipts at the same markets the corresponding day last year, 646 cars. The increase of 846 cars this year over last reflects the good business that exists for the progressive grain handler.

WHEN a barefoot boy steps on one side of a discarded barrel hoop it whirls upward and gives him a rap on the shin. Every person who has had the experience, and most of us have had it, remember the sensation. There is unanimous belief in the doctrine that reaction always equals action, at least in the barrel hoop. A similar thing happens when self-seeking agitators revile speculation too long and too loudly. Speculators quit; the market is deprived of their support; prices decline. The farmer suffers because of the acts of his pernicious and misguided "friends." The only difference between unfriendly agitation against competitive marketing arrangements and the barrel hoop is this, that the barrel hoop hits the one who steps on it while the outraged marketing system hits the farmer and not the agitator. However, the farmer is not altogether an innocent bystander because he listened to the agitator in the first place. Effect follows cause. Reaction is equal to action. That's why prices of farm products have declined in greater ratio than the price of other commodities. Dispose of the agitator, individual, organized and official, and prices will seek their normal relation to each other.

FEEDSTUFFS manufacturers are much alarmed by the unusual regulation issued by Arkansas' Commissioner, which is complained of in "Asked Answered," in this number. It is very evident that the Commissioner is so green at his work that he does not appreciate the difference between a statement of all ingredients (which his regulation calls for) and the analysis specifically provided for in the law. It is indeed doubtful if any court would uphold any regulations of a state official requiring a manufacturer to disclose the secrets of his business. Surely if the feedstuffs manufacturers were to disclose the ingredients of their products the number of feedstuffs manufacturers would be quickly increased by inexperienced monkeys who would attempt to imitate the work of the more successful feedstuffs manufacturers. If all patent medicine manufacturers were required to disclose how they made their products few of them would long succeed in business.

LIGHTNING does not always strike elevators in the night, but in both our Illinois and Iowa news columns of this number are told of elevators which burst into flames when opened the morning following a thunderstorm. One had a large hole torn through roof and cupola and in the other one grain in the upper bins started to burn after it had been smoldering for some time. Every vigilant guardian of grain elevator property who reads carefully our news columns will make a thorough inspection of his elevator after every thunderstorm, whether it occurs in the middle of the night or the middle of the day. Experience proves conclusively that elevators often burst into flame long after being struck by lightning. The safer precaution would be to thoroughly equip the house with a standard lightning rod equipment or else cover it completely with iron roofing and siding and ground the siding so that all electrical currents will be equalized.

THE PROLONGED rush of grain to the terminal markets has so strained the storage facilities of many of the markets as to make more grain unwelcome, and no doubt both farmers and shippers would realize larger returns from the grain if they would not crowd it on the terminal markets so rapidly. The natural presumption is that the undue haste in marketing grain recently has been due first to the pressure brought to bear on both farmers and shippers by their creditors and secondly to the unwillingness on the part of both to assume the chances of a further decline in the market. Both should appreciate that the prolonged heavy receipts would depress the market, even though ample mechanical facilities were provided for handling it. Information from abroad carries the conviction that Europe will be a large buyer of all grades in North American markets for some months to come, but the European buyers have ample stocks on hand so that their needs are not now pressing. If shipments are forwarded to terminal markets in even greater volume, then European buyers can be depended upon to exercise every influence to bring about a congestion and a collapse of the market, to the detriment of everybody interested except European buyers.

THE ASPHYXIATION of three farm hands in a concrete silo half filled with cut corn near Geneva, Ill., again emphasizes the danger accompanying the personal inspection of damp vegetable matter stored in air tight bins. The asphyxiation of grain elevator employes in concrete bins at Ogden, Utah, and New Orleans, showed that it is extremely dangerous to enter air tight bins containing damp grain. Before going into a bin, drop a dog or any small animal into the bin and pull him out in fifteen minutes, if dog is alive, the deadly carbon dioxide gas is not present in dangerous quantity.

A WOULD-BE grain shipper at Jetmore, Kansas, experienced so much difficulty in obtaining sufficient cars to meet the needs of his business that he has purchased four grain-tight cars and henceforth will use them in transporting his own grain to market. At last accounts not only was his elevator full of grain, but 7,000 bushels more were piled on the ground about it. It would seem that during the present business depression the average railroad would take care of its shippers and supply all with cars needed for the transportation of freight. The grain dealer who owns his own cars does not have to pay demurrage and can always make permanent improvements on cars with every prospect of getting the full benefit therefrom. Shippers who have been able to market grain when others were blockaded because of their inability to get cars have generally realized a handsome profit from their investment.

Changing the Spring Wheat Grades.

The Steenerson bill published in full elsewhere in this number of the Journal is in a fair way to become law by default of the real farmers outside of Minnesota and North Dakota.

Wheat grown in drier climates west and south can always make the No. 1 grade on the score of moisture and command the price of the best quality; but Minnesota wheat often contains more than 14 per cent and falls into No. 2 or No. 3. Under the grades for spring wheat prescribed in the Steenerson bill the moisture content is disregarded entirely in grading, wheat containing as much as 15 per cent or more water being entitled to the No. 1 grade altho it may be unsafe to put into store.

The presence of an unlimited quantity of foreign inseparable material will not lower the grade under the proposed rules. The advocates of this measure assume that up-to-date cleaning machinery costs nothing to buy and no expense to operate and that the grain elevators have wheat washing equipment such as is used by a few mills, and that it costs nothing to run the wheat laundry.

By the breaking down of the standards so carefully worked out by the trade and adopted by the federal government the bill will throw the grain of the careful farmer into the same grade as that of the wheat grower whose land is foul with weeds. All sellers of grain by grade alone will suffer loss and the inducement to make all sales and purchases by inspection of sample will be greater than ever. For the one cent gained by a few farmers in Minnesota the wheat growers of Montana, Colorado, Wyoming, Idaho, Washington and Oregon will lose three cents.

Passage of the Capper-Tincher Bill.

Assuming that it is constitutional, which it is not, the Capper-Tincher bill, signed by the President yesterday, will have an effect in four principal ways.

First: The farm bureau element is completely satisfied with the law because it accords them the class privilege of rebating commissions. In other words the co-operative elevator in the country will have the unfair advantage over the independent elevator of having its grain sold at the terminal at a cut rate of commission.

Second: The larger grain exchanges are pleased with the monopoly it gives them of short selling, short sellers on other than contract exchanges being required to pay the prohibitive tax of 20 cents per bushel.

Third: Reformers who are familiar with the gambling side only of puts and calls are gratified that the small bets on these privileges are done away with.

Fourth: The complete control over exchange transactions given the Sec'y of Agriculture pleases the socialists and bolsheviks immensely as promising an eventual bureaucratic tyranny as an outgrowth of this first intrusion of government into the grain business.

Exactly what will be the law can be determined by a study of the changes reported on page 273 of this number of the Journal compared with the draft of the law on pages 128 and 129 of July 25 number. Practically the enforcement will depend upon how far the Sec'y of Agriculture chooses to go in keeping the exchanges in leading strings. Quite likely, after a brief experience he will claim his certain failure to get the expected results is due to lack of power, and will request Congress to authorize him to write *all* the rules under which grain shall be sold in public places.

The grain trade did not desire this law. The great mass of the real farmers did not care for it. There was a real demand from the small band of agitators for rebating of commissions; but this could have been accomplished without regulation by the Sec'y of Agriculture. This legislation was demanded by an extremely small percentage of our citizens who made up in clamor what they lacked in numbers, and the average Congressman is a coward before farmer interests demanding legislative favors.

When a state has chartered a grain exchange to do certain things it is difficult to understand how the federal government can forbid the exchange to do those things when they are done entirely within the state. How can the federal government require an exchange to disregard its rules against rebating of commissions when the state authorizes the exchange to make its own rules? As recently as Aug. 22 the Federal Court of Greensboro, N. C., held it to be an unconstitutional invasion of the rights of the states for the federal government to impose a tax of 10 per cent on the products of child labor. The court held the state had power to impose a penalty, while even the most rabid advocate of federal power will admit the federal government can not *fine* an individual for selling grain on an exchange that has not been licensed by the federal government.

As soon as the co-operative companies begin distributing their commission earnings as patronage dividends there will arise a demand from all non-co-operative dealers that they be permitted to participate in rebates.

It will lead to a breaking down of the commission rule that has been the real source of strength, upbuilding the exchanges to their present power of service. Hitherto the co-operatives paying full commissions have been forced to contribute to the upbuilding of the organized markets.

Trade always follows the path of least resistance, and if it is made hard to deal in grain in the United States or on contract exchanges the business will go to exchanges outside the States and to non-contract exchanges in the States. A cash grain exchange, of which there are several in the smaller cities, does not come under the "Future Trading Act." By sedulously avoiding contracts for the future delivery of other than cash grain these exchanges can retain the commission rule. Even in the larger markets the cash grain dealers could segregate themselves and put up the bars against all rebaters.

As to puts and calls these traders can be trusted to take care of themselves. When the Chicago Board ruled against them they went across the street, and when the Board pursued them still farther they went to Milwaukee. Now they can go to Winnipeg, which is a perfectly good market for wheat and oats. Unfortunately for the U. S. brokers the commission will have to be paid in Winnipeg.

All in all, this law is of no benefit to anyone. The co-operatives will not benefit thru ruining the exchanges by rebating. The monopoly of short selling will not benefit the exchanges. The ban on puts and calls will not stop such dealings, and the Sec'y of Agriculture and his successors in office will derive no satisfaction from their attempts to handle a question that is too big for bureaucrats.

Illegal to Pay Unearned Dividends.

The laws of many states specifically prohibit the payment of dividends, by corporations, out of any money except *actual net earnings*. Dividend payments, however disguised, out of monies other than earnings have been held illegal. Directors making the appropriation, and stockholders receiving it, become liable to creditors for the amount so appropriated and received. In some cases each becomes liable for double the amount, and assignees can demand refund when the affairs of the corporation pass into their hands thru court action.

The state of Indiana has recently enacted a very strong law on the subject. In effect it makes it illegal to pay dividends on stock except out of actual net earnings, surplus or undivided profits. This law applies also to persons, firms, companies and associations not incorporated, and it makes any person assenting to the payment of such an illegal dividend subject to fine of \$200 to \$5,000.

The general rule of law, where there is no specific statute, is in direct line with the statutes which exist.

It is the intention of all law on this subject to prohibit the payment of dividends out of funds that have been subscribed by stockholders, and to protect creditors by preventing the use of funds that may have been earned as profits, but which are needed to liquidate debts. In effect it is the intention to prohibit the payment of dividends which reduce the true book value of its capital stock below its par value.

The law applies equally to cash dividends, stock dividends and service or patronage dividends.

While it is the purpose of these statutes and rules of law to curb the operations of the swindler it applies as well to honest men and these should not violate it thru ignorance.

Grain dealers should investigate to familiarize themselves with the laws in their own state. They will need, also, to exercise caution in conducting their corporate affairs to make sure that enough money is retained to cover all debts that may be outstanding.

The Predicament of Business.

The business world is in a peculiar situation, a situation brought on by that greatest of all human weakness—ignorance.

We are fond of deluding ourselves in the belief that we are an enlightened race, and we have learned many things about more or less abstract matters, but we know almost nothing about each other. Each one of us is engrossed in a task of his own, growing wheat or selling goods or running trains, taking care of our own job as best we can, but when it comes to the things the other man is doing we seldom know more about it than any jungle native.

When, by chance, we are brought face to face with some phase of our neighbor's job we seize upon that one element and by the force of our reasoning we assume it to embrace the whole of our neighbor's activities and to be all of his reason for existence. We are superficial in considering the other fellow's task. And always the superficial things we find are construed by us to be in the direct interest of our neighbor and contrary to our welfare.

If we happen to be of that turn of mind which can be no better described than by the use of the word *agitator*, we immediately set out to acquaint the world with our discovery. We take that little superficiality and we enlarge upon it. We touch it up here and there as our fancy dictates to make it more like the thing we think it to be even though its actuality may little resemble our conception.

The thing grows. Propaganda attracts. Others bring out their discoveries and set them alongside ours. Among us there may be some differences of opinion. My discovery is more important than yours; mine will hasten to millenium while yours will but correct a minor inaccuracy in the eddying of the whirlpools of time. But we agitators are agreed upon one thing. The other fellow is wrong. We are right. He must be superseded. Society must cast him out. We will persist in pointing out to society the need for this action.

Just now, and for several recent years, agitators have been pounding excitedly against business. Agitators started their attacks quite a long time ago and by now they have run a small part of the line of business activities, at one time or another subjecting this endeavor or that to their unjust, untruthful and unfair accusations.

Agitators harped against the railroads until they poisoned the mind of the public so that it could not think clearly on the subject. Shameful things resulted, and today we are paying for it in high rates, in reduced efficiency, and in the depressed values of rail securities. We already have paid a fearful price—and we may pay much more.

The packing industry has been assailed, the fuel industry, and big and little business here and there. Now the agitators are centering

their offensive against the grain trade. Untruthful premises are laid down as representing defects in the machinery for marketing grain and demagogic nostrums are put forth as panaceas to cure the alleged disease.

Where will it stop? Many other forms of business activity remain for the agitator's attention. They will get it if they do not act to dispel the ignorance about themselves existing in the mind of the public. The banking business tempts the agitator. He looks upon it with longing eyes, and upon manufacturing and shipping and farming and every work the other fellow does.

It's time for honest men to dispel this pall of ignorance. A greater or more important task does not exist. It demands the attention and merits the best efforts of all business organizations. A forceful campaign will be required and perhaps a long one. It is not the duty of the grain trade to do all of it, nor of bankers or manufacturers, packers or cotton spinners, wholesalers or retailers. It is the duty of all to participate. It should be counted a privilege to help.

If it is not done and done promptly the whole business structure will feel the effect of the agitators' onslaughts. Will business wait—and suffer? Or will business tell the truth about itself? Agitators are already telling plenty of lies. What is to be the answer of business to these lies? The truth—or silence that to the mind of the average man is little but admission.

Confirmation Binding on Failure to Object.

The Earlton Grange Co-operative Ass'n of Earlton, Kan., wrote the Trusler Grain Co., of Emporia, Kan.:

"On reflection I have decided to load you one or two cars of wheat at your offer of 8c above government price, no Com. If we can get the cars 15 or 20 days shipment. Let us know where to order the cars to."

Buyer wired an acceptance of two cars and followed up by confirmation containing the clause:

"Receipt of this contract by the seller, without immediate notice to us of error, is an acknowledgment of the acceptance of all the conditions thereof."

The district court of Neosho County struck out the confirmation, but the Supreme Court of Kansas on June 11, 1921, held this was erroneous and reversed the decision, stating:

In *Bossmeyer Bros. v. Nielson*, 108 Kan. 534, 196 Pac. 431, it was held that the rules of the grain association under which the parties were operating entered into and formed part of the contract. Under the allegations of the petition now before us the known and understood custom of the grain trade gave to the letter of confirmation the office of supplying the necessary matters of detail upon which the contract was silent. In *Cardwell v. Uhl*, 105 Kan. 249, 182 Pac. 415, it was held that, where there is evidence of a practice among grain dealers followed in prior transactions of mailing letters of confirmation of oral contracts, such confirmations are admissible in corroboration of testimony that such oral contracts were made.

In *Wallingsford v. Grain Co.*, 164 Pac. 275, of the written confirmation of a "phone contract" it was said that, stating the terms, the price, the destination and weight, it reserved "also the right to buy in the grain for the seller if shipments were not made according to contract." Strong v. Thurston, 107 Kan. 368, 191 Pac. 575, held that the failure of the defendant to make objection to the letter of confirmation amounted to an acceptance thereof.

As the matter stricken from the petition took that part of the cause of action out of the case, the order to strike was appealable.

It is held that the offer meant that the seller would furnish one or two cars as desired by the buyer, and this, together with the acceptance, was a contract for two cars; that these, together with the letter of confirmation, gave the plaintiff the right to buy in case the defendant refused to ship; and, further, that it was error to strike from the petition such letter of confirmation.—198 Pac. Rep. 964.

Asked— Answered

[Readers who fail to find information desired on any subject of interest to grain dealers should send us their query for free publication here. The experience of your brother dealers is worth consulting. Replies to queries are solicited.]

Standard Half Bushel?

Grain Dealers Journal: Where can we get a half bushel measure for testing grain, that is accurate and not expensive?—Stifel & Levy, Fort Wayne, Ind.

Ans.: These are supplied by the scale manufacturers, are made of galvanized steel with a brass rim, and come, if desired, with a graduated beam balance.

Prices of Cash Wheat 1911 to 1920?

Grain Dealers Journal: I am endeavoring to secure figures which will give me the highest and lowest price per month for number two soft red winter wheat and number two hard red winter wheat at Chicago, Ill., by years for the ten year period, 1911 to 1920 inclusive. I would greatly appreciate receiving same. I am tabulating the prices of these two wheats at Kansas City, St. Louis and Chicago, and need the Chicago figures in order to complete my tabulation.—D. W. Frear, associated extension professor in field crops, University of Missouri, Columbia, Mo.

Ans.: The high and low each week on soft and hard and 8 other grades of wheat are given on page 8 of the annual report of the Chicago Board of Trade for 1920. In the preceding volumes on the corresponding pages will be found the same statistics for many years back.

Storing Under Nebraska Law?

Grain Dealers Journal: There seems to be quite a difference of opinion in regard to storing grain under the new storage law. Are elevator owners who have no storage license, permitted to store grain after it has been in the elevator over ten days? Must they make settlement on all grain ten days after grain has been delivered?

Some of the elevator owners are storing grain for the farmers and giving them their own time to sell. Our opinion is all grain must be settled for ten days after it has been delivered to elevator.—J. W. McCleery, Pauline, Neb.

Ans.: An unlicensed elevator holding grain for a farmer over 10 days is subject to fine, as fully explained on page 762 of the Grain Dealers Journal for May 10, 1921, under the Nebraska law of Mar. 23, 1917.

Refund of Live Stock "Broker" Tax?

Grain Dealers Journal: In the Journal of June 10, page 947, we notice an item to the effect that "livestock commission men are not taxable as brokers under the federal law of 1918—a decision of this effect having been given in the United States District Court of Chicago, May 23rd." Could you cite us to the case involved so that we might investigate the decision further?—J. T. Buchanan, Sec'y Omaha Elevator Co., Omaha, Neb.

Ans.: This decision was given May 23 by Judge Page in the U. S. District Court for the Northern District of Illinois at Chicago in case No. 33167, entitled Everett C. Brown and S. B. St. John v. Smetanka, Collector of Internal Revenue, plaintiff seeking to recover \$25 tax paid.

The Government, under the Revenue Law of 1918 has been collecting annually \$25 from livestock commission merchants under the allegation that they were brokers. The court held that live stock commission merchants were not brokers within the meaning of the Revenue Act of 1918. This decision not only relieves the live stock commission merchants of the country from further payment of the annual broker's tax, but under it all taxes heretofore paid since the law went into effect in January, 1919, will have to be refunded, the aggregate amount

running into hundreds of thousands of dollars. Plaintiffs were ably represented by Sims, Welch, Godman & DeYoung, attorneys, of Chicago.

Claims for refund of the brokerage tax should be presented on Form No. 46, revised January, 1921, issued by the Treasury Department, Internal Revenue Service.

The suit was started Oct. 6, 1919, and the Court asked that no publication be made of the decision of May 23, evidently to give the Government time to appeal; but on June 24 the Government by its attorney acquiesced in the decision and refunds are now being made.

Notation "Subject to Delay"?

Grain Dealers Journal: Will the Journal kindly inform us as early as possible whether there has been to its knowledge a supreme court decision in favor of the shipper where the railroad company has accepted freight with a notation on the B/L "subject to delays" and the freight arrived destination in a damaged condition account of delay?—Nowak Milling Corporation, Hammond, Ind.

Ans.: In general the notation "subject to delay" absolves the carrier from liability; but when in fact the delay is due to negligence rather than causes beyond the carrier's control, the carrier is liable, and this depends on the facts in each case relative to the handling of the shipment en route.

In some states, as Missouri, the statute shifts the burden of proof on the carrier when a shipper has proved the delay, that the shipment was too long a time in transit.

When a railroad company, knowing that conditions are likely to cause delay, withholds this information from the shipper by failing to indorse the fact on the B/L, it becomes liable. By writing "subject to delay" on the B/L the carrier puts the shipper on notice and avoids the liability contemplated at the time. While there are innumerable decisions holding the carrier liable for negligence there are none where the carrier employed the notation "subject to delay."

Illinois Law on Storage?

Grain Dealers Journal: A number of years ago I took the following form of storage ticket from the Journal, as I understood it avoided the necessity of reporting to the Utilities Commission:

No. Grade
TRIUMPH 19....
Grain Price

RECEIVED ON CONTRACT

OF

For H. G. Smith

Gross lbs.
Tare lbs.
Net lbs.

Net bu. \$

To be settled for at market price at Triumph, when the party may wish to sell, after paying all charges; storage one-half cent per bushel per month or fraction of a month, after thirty days. Identity of stored grain not to be preserved.

..... Weigher
This ticket must be produced in settlement.

I have used the form ever since and have not reported anything to the Commission. The position taken when this form was printed was that when the farmer left his grain under this form of ticket it was sold the day he hauled it in and the dealer was responsible for the market price when the farmer elected to sell, and must pay for the grain even if it burned, while the dealer could protect himself by insurance and buying futures on what he shipped out. Is this form still good?—H. G. Smith, Triumph, Ill.

Ans.: A country elevator storing grain for hire would seem to be classified as a public utility and therefore under the jurisdiction of the State Public Utilities Commission; and the Commission, acting under the advice of the attorney of the Illinois Grain Dealers Ass'n that it had such jurisdiction, undertook to prescribe regulations for country elevator operators, as published in full in the Grain Dealers Journal at the time, July 10, 1914, page 45.

But, as pointed out by the Journal on page 47 of the same number, no penalty is provided for failure to obey the Commission's regulations, and the law and regulations not being enforceable, it has become a dead letter. In fact, the Monarch Refrigerating Co. went into court and denied the jurisdiction of the Commission to require the filing of schedules of storage charges.

In his final settlement with the farmer when paying for the grain the dealer should deduct

all his charges; for the reason it is doubtful that he would have standing in court to collect by suit as he has no license to store and charge for it. The farmer would have no ground to sue to recover for the charges paid, because he had agreed to pay them.

The foregoing storage ticket seems to be still good.

On Oct. 1 the new law regulating the business of storing personal property for hire will go into effect. It provides for an annual license July 1 to June 30, costing \$20, the giving of bonds of \$5,000 to \$100,000 and authorizes the Commission to make regulations. Sec. 8 provides a fine of not exceeding \$1,000 or imprisonment not exceeding six months for violating any provision of the Act.

It remains to be seen whether the Illinois Commerce Commission of 7 members created by the law approved June 27, 1921, will hold it has jurisdiction over country grain elevators. The whole intent of the law seems to be directed to control such public utilities as are natural monopolies, water companies, street car lines, electric light and gas corporations. A country grain elevator storing grain is not a natural monopoly.

Is the Drastic Ruling of the Arkansas Feed Commissioner Constitutional?

Grain Dealers Journal: The following circular letter is being distributed by the Arkansas Feed Commissioner at Little Rock, Ark:

STATE OF ARKANSAS.

Bureau of Mines, Manufactures and Agriculture—Jim G. Ferguson, Commissioner;
John E. Casey, Deputy.

Little Rock, Ark.

To Feed Manufacturers and Jobbers:

Notice is hereby given that effective Oct. 1st, 1921, all manufacturers, jobbers and other firms responsible for the sale of commercial concentrated feeds in Arkansas will be required in registering such feeds with this department to state the percentage of each of the ingredients of each feed so registered. There will be no exception to this rule.

It is suggested that in the meantime all manufacturers, jobbers, or persons who offer registrations for approval submit them in keeping with the foregoing ruling.

This does not mean that feeds already registered will have to be re-registered Oct. 1st. However, in line with this ruling all feeds now registered must be re-registered Jan. 1, 1922. All feeds offered for sale in this state must bear an analysis tag to conform to the registration. The items must be stated as follows:

Brand of Feed, Number of Pounds in Bag or Package, Name and Address of Manufacturer, Guaranteed Analysis, Statement of Ingredients.

Failure to comply with these requirements will be considered sufficient grounds for withholding such feeds from sale until the irregularity has been corrected.

Yours very truly,
(Signed) Jim G. Ferguson,
Commissioner.

This is the first formula disclosure ruling that has come to our attention in this country. It would appear to be unconstitutional and practically confiscatory.

Will the Journal give us its views on whether or not the above ruling is enforceable?—Pratt Food Co., Philadelphia, Pa.

Ans.: The Arkansas Feed Law contains no section which gives the Commissioner of the Bureau of Mines, Manufactures and Agriculture the power to require such information.

The sections of the Arkansas law which cover this point require not the percentage of ingredients but the percentage of protein, fat, fiber and carbohydrates. These sections follow in full:

Section 1. That every lot or parcel of concentrated commercial feeding stuff sold, offered or exposed for sale in this State, shall have affixed thereto, or printed thereon in a conspicuous place on the outside thereof, a legible and plainly printed statement, in the English language, clearly and truly certifying the weight of the package; (provided, that all concentrated commercial feeding stuffs shall be in standard weight bags or packages of 5, 10, 25, 50, 75, 100, 150, 175, 200 pounds); the name, brand or trade mark under which the article is sold; the name and address of the manufacturer, jobber or importer; the names of each and all ingredients of which the article is composed; a statement of the maximum percentage it contains of crude fiber, and the percentage of crude fat, and the percentage of crude protein, and the percentage of carbohydrates, allowing 1 per cent of nitrogen to equal 6 1/4 per cent. of protein; all four constituents to be determined by the methods in use at the time by the Association of Official Agricultural Chemists of the United States.

Section III—Be it further enacted, That each and every manufacturer, jobber, importer,

agent or seller, before selling, offering or exposing for sale in this State, any concentrated commercial feeding stuff, shall, for each and every feeding stuff bearing a distinguishing name or trade mark, file for registration with the Commissioner of Mines, Manufactures and Agriculture, a copy of the statement required in Section 1 of this Act, and accompany said statement by a sealed glass jar or bottle containing at least one pound of such feeding stuff to be sold, exposed or offered for sale, which sample shall correspond within reasonable limits to the feeding stuff which it represents in the percentage of crude protein, crude fat, crude fiber and carbohydrates which it contains.

Evidently the feed business is all new to Commissioner Ferguson, and he needs enlightenment.

Letters

[Here is the grain dealers' forum for the discussion of grain trade problems, practices and needed reforms. When you have anything to say of interest to members of the grain trade, send it to the Journal for publication. It may draw out the views of others.]

Will Burn Corn Instead of Coal.

Grain Dealers Journal: With corn selling at 25 cents per bushel this year, seventy-five per cent of our farmers will burn corn instead of \$15 coal during the coming winter.—The Brooking Grain Co., Inland, Neb.

Keeping Market Open Later.

Grain Dealers Journal: Chicago is talking of keeping their market open till 2:20. It is argued this should be done because the stock exchanges close one hour later than the grain boards.

It might be a good idea but some sections would be affected because of their inability to get their mail out in time to reach the country the following morning.

Speaking of long sessions, when we were "kids" the Toledo Produce Exchange sessions opened at 10 o'clock and ran until 12:30. Then everybody went out and got a cup of tea, and had another opening at 2:30, and trading went on until about 3 o'clock. That was about 30 years ago.—J. F. Zahm & Co., Toledo, O.

About Grinding Costs.

Grain Dealers Journal: The request of W. O. Morgan printed on page 196 of the Journal for August 10th for information about the cost of grinding feed has been read by us.

Mr. Morgan is not allowing enough when he sets a cost of 5c per cwt. as his cost of grinding. The power actually consumed may not represent a greater expense than 5c per cwt., but there are many other items to take into consideration. In most elevators, the feed grinder as it is usually installed adds to the insurance rate. This, together with the interest on the additional investment, extra labor, added taxes, and an allowance for depreciation must be included with the cost of the power in determining the grinding cost. The cost per cwt. will fluctuate from year to year, dependent upon the quantity that is ground, so that the figure which is correct for one period might be wide of the mark for another.

We regret the fact that we cannot furnish for Mr. Morgan's information a detailed analysis of our costs, but the truth of the matter is we have not given this subject much consideration in the past, only being awakened to the need of studying it when we read his query. We have resolved to give it attention in the future and after a time we expect to be in position to give some definite data on grinding costs. In the meantime we, too, will appreciate a thoro discussion of the subject in the pages of the Journal by men who are prepared to support their statements by actual figures.—Borden Grain Co.

Private Wire Prohibition Struck Out of Capper-Tincher Bill.

The Capper-Tincher bill as reported by Senator Capper July 8 was published in full in the Grain Dealers Journal for July 25, pages 128 and 129.

This draft has since been twice changed, first by the Senate on Aug. 8 and later by the conference com'tee of the Senate and House.

Aug. 23 the House adopted the conference report and sent it to the President, with the changes as follows:

The Senate made five changes in the bill. It struck out the prohibition of private wires that formed paragraph (g) of Sec. 5. It added to paragraph (a) of Sec. 6 the following provision for an appeal by the Board of Trade:

Provided further, that if the Sec'y of Agriculture shall refuse to designate as a contract market any Board of Trade that has made application therefor, then such Board of Trade may appeal from such decision to the Commission described therein, consisting of the Sec'y of Agriculture, the Sec'y of Commerce and the Attorney General of the United States, with the right to appeal as provided for in other cases in this section, the decision on such appeal to be final and binding on all parties interested.

The Senate struck out the words "patronage profits" and "substantially all of whose business consists of handling grain produced by its members" so that this sentence of paragraph (e) now reads:

Provided that no rule of a contract market against rebating commissions shall apply to the distribution of earnings among the bona fide members of any such co-operative association.

The words "undue or unfair" manipulation of prices were inserted in paragraph (d) of Sec. 5 by the Senate and struck out by the conferees.

The conferees made about sixteen changes in the bill as it came to them from the Senate, but the changes did not affect the principles involved and were directed to clarifying the language. After the words "cash grain for deferred shipment" in Sec. 2 the conferees added "or delivery." The conferees struck out the requirement that contract markets must have "adequate storage facilities" from Sec. 5, paragraph (a), and enlarged the powers of the sec'y of agriculture over the keeping of records, and provided that the Board instead of members may keep the records. To paragraph (d) of Sec. 5 the conferees added a clause against "cornering of any grain."

The conferees added to Sec. 6 a paragraph providing that the provisions of Sec. 12 of the Interstate Commerce Act regarding the testimony of witnesses, the production of documents and immunities be made applicable to the power, jurisdiction and authority of the Sec'y of Agriculture, the Commission or the referee.

The conferees inserted the old Sec. 7 providing for the collection of taxes.

LATER: The President signed the bill Aug. 24.

IN MEXICO no state has produced more than one-half a crop of rice and beans and the grain crop will also be short, according to an official report of the Mexican Department of Agriculture.

So FARMERS IN 1922 will have plenty of help, the Kansas Industrial Commission is already trying to have western carriers establish a rate of 1 cent per mile to workers bound for the harvest fields next year.

W. R. Scott, transportation Commissioner of the Kansas City Board of Trade said on Aug. 23, after his return from the hearings: A reduction in grain and hay rates is probable as there is considerable political agitation for anything that resembles a farmer relief measure. A decision in the general freight question is expected within ten days.

Leaking in Transit

Grain dealers can help brother sufferers in the collection of claims for loss by reporting to Grain Dealers Journal, for free publication, car initials, number, place, date and condition of car seen leaking grain in transit.

Recently we have received reports of the following leaking or bad order cars:

M. P. 20806 passing eastbound thru Levasy, Mo., Aug. 23, was leaking wheat at the side.—Levasy Elevator Co., by Julius Welter.

Pa. 96570 passed thru La Rue, O., on Aug. 22, going east, leaking corn badly at the door.—La Rue Grain & Supply Co., per C. C. Metz, mgr.

C. M. & St. P. 76464 passed thru Valparaiso Aug. 21 leaking wheat badly under the siding.—Nickel Grain Co.

D. & H. 19574 passed thru Whitestown, Ind., over the Big Four, eastbound, on Aug. 19, leaking wheat at the door.—The Kern & Kertley Grain Co.

C. B. & Q. 98286 passed thru Holyoke, Colo., on Aug. 17 leaking wheat at the side of car.—Paul Renner, mgr. Reimer-Smith Grain Co.

U. P. 77131 passed thru Central City, Neb., Aug. 14 in an extra eastbound train, leaking wheat at the side. The car was not set out for repairs. Reported by our Central City agent, William Palmeter.—T. B. Hord Grain Co., per T. B. King, treas.

C. B. & Q. 113499 passed through Chester, Neb., on Aug. 13 in an eastbound train leaking wheat at the corner post. The railroad agent was notified.—The Brown Lumber Co., per E. L. Brown.

C. B. & Q. 108832 passed thru Hendley, Neb., Aug. 13 leaking wheat at the corner of the car.—G. W. Query, mgr., Farmers Business Ass'n.

95691, loaded with wheat, was leaking badly along side when it went thru Laird, Colo., on an extra going east on Aug. 12.—The Farmers Union Co-op. Elevator Co., G. H. Burnett.

C. B. & Q. 106738 passed thru Holyoke, Colo., on Aug. 12 leaking wheat at the side of the car.—Reimer Smith Grain Co.

C. B. & Q. 111993 was in a train that was moving eastbound out of Oxford, Neb., on Aug. 6. The car was leaking wheat badly at the side.—The O. M. Kellogg Grain Co. of Denver, Colo.

M. P. 30221 was enroute thru the Hutchison, Kan., yards on the M. P. on Aug. 11 and was leaking under the left side in a number of places.—W. B. Bradshaw, Strong Trading Co., Wichita, Kan.

C. R. I. & P. 40341 passed thru El Paso, Ill., July 24, going south on the Illinois Central leaking wheat.—El Paso Elevator Co.

The Grain Dealer.

He thought the dump was locked. It wasn't. Mule dead.

He didn't believe in dust explosions. Light-a pipe. He will never learn his mistake.

He supposed his scale was correct. It wasn't. Overpaid each farmer 20 pounds per thousand.

He thought the bearings had plenty of oil. They hadn't. Plant down for repairs.

He thought the can contained kerosene. Fire needed coaxing. Gasoline. Elevator up in smoke.

He thought he could replace the belt without stopping machine. He couldn't. Hand gone.

He thought there should be room enuf in the bin for another load. There wasn't. Mixed wheat and oats.

He thought the locomotive had been uncoupled from the car just spotted. It hadn't. He's now short a foot.

He considered Mike Moran's word good. It isn't. Wheat up 20c. Mike claims he contracted 300 bus., not 1,300 bus. Hereafter his contracts will be recorded in writing.

Crop Reports

Reports on the acreage, condition and yield of grain and field seeds, as well as on the movement to country markets, are always welcome.

CANADA.

Ottawa, Ont., Aug. 12.—Yields of the various grains in Canada this year follow: Wheat 288,493,000 bus.; oats 457,544,000.; and rye 11,707,000 bus.; compared with a yield in 1920 of wheat 263,289,000 bus.; oats 530,709,000 bus.; and rye 11,306,000 bus.—Dominion Bureau of Statistics.

COLORADO.

Sterling, Colo., Aug. 16.—Returns from harvesting are very light and think crop will not average more than 60% normal.—The White Milling & Elevator Co.

IDAHO.

Midvale, Ida., Aug. 14.—Harvesting is in full swing at this point. Winter wheat is averaging about 23 bu. per acre on dry land and 50 to 80 bu. on irrigated. More or less smut in all wheat.—George A. Urquhart.

ILLINOIS.

Galt, Ill.—Oats in this section are lighter than usual. Wheat acreage was smaller than last year.—The Agnew Farmers Elevator Co.

Covel, Ill., Aug. 15.—Oats yield about 30 bus.; testing 24 lbs. Wheat yield 30 bus.; testing 59 lbs.; grading No. 2 yellow. Corn condition about normal.—A. L. Nicol, mgr. Farmers Grain Co.

Springfield, Ill., Aug. 20.—Climatic and other conditions have been generally favorable for corn, and the plant has made some improvement in most areas. The grain is now denting generally. Some lodging has been reported from a few localities, due to high winds. The corn ear worm is at work in many places and threatens to be quite a factor in reducing yields. Reports have come of an undue proportion of poorly filled ears and nubbins from numerous localities. If this condition proves to be general, yields may be smaller than expected.—S. D. Fessenden, agri. statistician.

Chicago, Ill., Aug. 20.—Corn all over the country ranges from fair to extra good. In the great corn belt, the crop is becoming more and more certain due to favorable moisture and other weather conditions. The uneven ripening condition reported the last few weeks seems to be disappearing to some extent, as showers and medium warm weather have favored the rapid filling of ears. There has been some damage to corn from wind storms, which knocked down quite a large amount of corn, but unless this is followed by severely wet weather, little actual damage will result. In the middle western and the southwestern states plowing has started for the 1922 wheat crop.—American Steel & Wire Co., per F. Baackes, vice pres.

Valparaiso, Ind., Aug. 12.—Rye is good here; wheat 25%; oats 20%; corn fired and dead. Some low spots good.—S. J. Brown, mgr. Chatfield Grain Co.

Bicknell, Ind., Aug. 11.—Wheat threshing is now over and the yield is very disappointing. The wheat that has been threshed since the heavy rains is badly sprouted much of it not being marketable at all. Altho the corn has fired in some places the prospects look good. Oats are a complete failure and what few were raised were ruined by the recent rains.—O. L. Barr.

IOWA.

Osceola, Ia., Aug. 15.—Excessive moisture has lowered grade of wheat from No. 2 hard to No. 4 and sample. Oats are weighing from 22 to 26 lbs.—Curnes Grain Co.

Albia, Ia., Aug. 12.—Oats crop very light; poor quality. Wheat crop fair and fair yield and quality. Lots of wheat damaged now by wet weather.—Wilkin Grain Co.

Inwood, Ia., Aug. 15.—Grain will move quite freely if price strengthens up a little. Shock threshing wound up here this week.—G. A. Lyon.

Des Moines, Ia., Aug. 17.—In the south corn has been firing on thin soil and the grub worm is reported to have caused slight damage. In

northwestern Iowa corn is two weeks early, and in northeastern Iowa oats threshing has been delayed by the rain.—S. H. Johnson, v.-p. C. R. I. & P.

KANSAS.

Almena, Kan., Aug. 15.—Corn is badly in need of rain.—J. H. Gailey, mgr. Roller & Jennings.

Geneseo, Kan., Aug. 12.—Wheat quality good; will average 12 to 15 bus.; no corn; oats fair.—E. E. Lorenz, mgr. elvtr. of Larabee Flour Mills Corp. here.

Hutchinson, Kan., Aug. 17.—Hot winds did considerable damage to the early corn in the southern and southeastern parts of the state. In the southwestern part of the state corn is making fine growth.—S. H. Johnson, v.-p. C. R. I. & P.

Lorraine, Kan.—The quality of the wheat is good and the average yield was about 12 bus. The ground has been too dry for the farmers to prepare for the coming crop. Corn will make practically nothing and but very little planted around here.—C. H. Veatch.

MICHIGAN.

Lansing, Mich., Aug. 20.—Corn is well advanced and is in generally good condition over the state. In the southwestern counties, where the weather is very dry at present, and on light soils in other sections, it is rolling and firing more or less. More moisture during the last two weeks would have produced a larger yield. The outlook is for a crop of 60,479,000 bus., as compared with 65,000,000 last year. Winter wheat shows an average yield of 16 bus. per acre, which is about one bushel below the ten-year average. The forecasted production is 13,088,000 bus., or about 700,000 less than last year. There is much shrunken and light weight grain, the quality being 84 per cent as compared with an average of 90. Spring wheat is a poor crop, the estimated yield being only 53 per cent of normal. Oats was almost a failure in the northwestern counties. In many parts of the state the straw was too short to cut with a binder. Some fields were too poor to harvest, and some that were harvested were not threshed. The estimated production is only 55 per cent of a crop, or 31,042,000 bushels, the smallest for the state since 1907.—Verne, E. Church, agri. statistician.

MINNESOTA.

Woolverton, Minn., Aug. 15.—No crops this year.—Woolverton Elvtr. Co.

West Concord, Minn., Aug. 15.—Crops very light; oats brot 22 to 24 lbs. to bu.; small yield.—A. J. Wilson.

Woolverton, Minn., Aug. 15.—Season's crops very poor this year.—H. Evenson, agt. St. Anthony & Dakota Elvtr. Co.

Chandler, Minn., Aug. 12.—Oats are yielding about 40 bus. and corn promises a large yield.—H. D. Rarnard, agt. Bennett Grain Co.

Motley, Minn., Aug. 15.—Rye crop about average. Wheat and oats about half a crop; poor quality. Corn fair.—C. E. Steely, agt. Monarch Elvtr. Co.

West Concord, Minn., Aug. 15.—Crops are fairly good here; better than expected. Corn crop can't be beat; mostly out of danger.—J. J. Christy, Mgr. Farmers Elvtr. Co.

Middle River, Minn., Aug. 10.—Cutting not quite finished; weather rather rainy. Danger of bleached grain, also of grain going tough. Yield estimated below average.—E. O. Ramstead, mgr. M. D. Co-op. Elvtr. Co.

Worthington, Minn., Aug. 13.—Oats testing from 23 to 31 lbs., average run 28 to 29 lbs.; yield 25 to 59 bus., average 40 bus. Corn looking fine. Sufficient moisture at present. Looks like a record crop if ears fill well.—Farmers Co-op. Co.

Brooks, Minn., Aug. 13.—Rye about 50% of a crop; weighing 46 to 54 lbs. Wheat yielding 8 to 12 bus.; weight 48 to 55 lbs. Oats running from 10 to 30 bus.; weighing 28 to 30 lbs. Flax, buckwheat and corn looking good.—The Farmers Co-op. Exchange Co.

MISSOURI.

Rush Hill, Mo., Aug. 15.—Corn is normal, oats are 40% short and wheat is normal. Wheat yield is from 8 to 26 bus. per acre. Not the usual amount will be sown this fall.—Ed Hilbrand.

Agency, Mo., Aug. 17.—Wheat is poor in yield and quality. It is averaging from 5 to 20 bus. to the acre and is grading 3, 4 and 5. None of it is grading 1 or 2. Some wheat is in shock. About one-half of it has sprouted. No stacking until recently. Corn is good, but needs dry weather for maturity before frost.—E. E. Powell, mgr., Agency Elevator & Grain Co.

MONTANA.

Marsh, Mont., Aug. 22.—Wheat is not plentiful here, but it is of fine quality.—W. F. Schultz. Sidney, Mont., Aug. 17.—Crops are poor. The quality of the wheat this year is very good.—Farmers Merc. Elevator Co.

NEBRASKA.

Charleston, Neb., Aug. 16.—Shock threshing is all done around here. Wheat averaged from 15 to 30 bus. per acre and the quality was good. Oats averaged from 25 to 50 bus. per acre and of good quality. Corn will make from 50 to 60 bus. per acre.—H. H. Otto.

Kansas City, Mo., Aug. 19.—William Craig of Blue Springs, Neb., writes as follows: Soil is rather dry for good plowing. Corn fields look superb. The corn stalk average is taller than ordinarily. Farmers do not believe the corn yield will equal the 1920 yield. Several farmers have reported that many of the stalks do not shoot ears which if true will cut down the yield. We hear similar complaints in other localities.—W. S. Nicholson Grain Co., per Robert Nicholson.

NEW MEXICO.

San Jon, N. M., Aug. 20.—Wheat is of very poor quality in this section this year.—San Jon Elevator, per R. C. Mundell.

NORTH DAKOTA.

Powers Lake, N. D., Aug. 18.—Crops are good here.—R. E. Shoemaker.

Rock Lake, N. D., Aug. 18.—Crops are slim here as the weather was too hot and dry during July, when we had no rain from the 2nd until the 26th. We also had some black rust and some hail. Some fields will not be cut. Threshing is going on now. Durum is making from 5 to 10 bus. per acre and will grade mostly No. 2. The rye is grading No. 1 and yielded from 10 to 12 bus. per acre. Farmers are disappointed with the crops.—The Armourdale Equity Elevator Co.

OHIO.

Sylvania, O., Aug. 17.—The severe rainstorm thru this section knocked down a great deal of corn.—D

Wilmington, O., Aug. 24.—Corn is coming along fine and will make about $\frac{3}{4}$ of a crop.—Buckley Bros.

Kingston, O., Aug. 10.—Wheat is grading around No. 2 and is averaging from 8 to 10 bus. to the acre.—Jesse Brundige.

Sylvania, O., Aug. 19.—Wheat is only half a crop thru this section. The average is only 10 bus. to the acre.—E. G. Howard & Son.

Bloomington, O., Aug. 11.—The oats crop is 20%; wheat 50% and corn 60% normal.—F. E. McDermott, mgr. Bloomington Grain Co.

Columbus, O., Aug. 13.—Recent rains have improved the corn crop prospects from 10 to 20%.—C. J. West, agricultural statistician.

Ashville, O., Aug. 15.—Wheat was very little more than half a crop and that was poor quality. Corn, owing to drought in our section, is badly damaged.—G. C. Cline, mgr., Ashville Grain Co.

Marysville, O., Aug. 12.—Wheat is about one-half a crop in central Ohio this year. The recent rains have improved the corn. About 75% of a crop in Union County, but the surrounding counties are some better.—Spurrier Bros.

OKLAHOMA.

Oklahoma City, Okla., Aug. 17.—Oklahoma's oats crop is of poor quality and the estimated total yield of 30,460,000 bus. is 30% less than last year. Altho the broom corn acreage is considerably less this year, the yield will be good.—S. H. Johnson, v.-p. C. R. I. & P.

SOUTH DAKOTA.

Crandall, S. D., Aug. 16.—Crops are very light. Grain is a very low average per acre. Some wheat is of very good quality.—The Farmers Elevator Co.

Frاندt, S. D., Aug. 19.—Considerable buckwheat in this section of this country this year. It looks good so far and we expect a very good crop.—P. J. Peterson.

Kennebec, S. D., Aug. 16.—Yield and quality of the small grain crop is light. Oats are almost a total failure. Corn looks good. Hay is short and weedy.—A. I. Sinclair.

Moritz, S. D., Aug. 16.—Crops are average thru here. About 90% of the grain raised in this section is oats and they are testing from 27 to 31 lbs. to the bushel.—Farmers Elevator Co.

Parkston, S. D., Aug. 10.—The grain around here is fair. The oats are averaging from 25 to 40 bus., the wheat from 7 to 25 bus., rye 18 and barley from 20 to 30. Corn looks good for 50 bus.—The Farmers Elevator Co., per J. P. Arend, mgr.

TEXAS.

Ft. Worth, Tex., Aug. 17.—Some corn is reported firing in the Panhandle section. Wheat is almost all cut and threshing is nearly completed. Rye and barley are being cut. In the west and northern sections of the state corn needs rain. In the east central and northern central districts corn is mature and is being harvested. Rain is needed so farmers can plow.—S. H. Johnson, v.-p. C. R. I. & P.

VERMONT.

St. Albans, Vt., Aug. 16.—The dry weather has seriously affected the hay crop in northern New England.—The St. Albans Grain Co.

WASHINGTON.

Auburn, Wash., Aug. 17.—Crop conditions in this section are the very best this year.—Smith Feed Co.

WISCONSIN.

Owen, Wis., Aug. 12.—Oats of poor quality, having practically no feeding value; very light in weight, yield averaging around 10 bushels to the acre. Most of our farmers cut their oats for hay. Barley of fair quality, rye good. Threshing in full swing here. Corn is unusually good and if the weather is favorable will have a bumper crop of corn. Hay crop about 75% of normal, mostly timothy; clover froze out last spring. Blight and the hot, dry weather killed our potato crop. Will have to get potatoes shipped in here.—Green Grove Farmers Produce Co., per Eph Ohmen, mgr.

ILLINOIS.

Springfield, Ill., Aug. 20.—The price of winter wheat has been falling of late and the farmers are showing a tendency to sell less freely than they have been doing.—S. D. Fessenden, agri. statistician.

Elevator Casualties; Take Warning.

With the rush of grain to market recently several grain men suffered minor injuries, a few of which are enumerated in the following paragraphs:

In an attempt to open a grain door on a car E. C. Baker of the Western Flour Mills, Davenport, Ia., slipped and one of his fingers caught between the grain door and the car. The finger was badly bruised and crushed.

While dumping a load of wheat Frank Clingman of the Horner Elevator & Mill Co., Lawrenceville, Ill., sustained a fractured rib when the dumping lever sprung back and struck him on the chest.

Lee Harrison, employed by the Kansas Flour Mills Co., Kansas City, Mo., ran a nail into his hand while removing a grain door.

C. E. Grover of the Gooch Milling & Elevator Co., Lincoln, Neb., cut the back of one of his hands when he reached into a chute for a sack of wheat.

A power shovel which kicked back injured L. McDonald, employed by the Diamond Mill Co. of Sherman, Tex.

By losing his footing while repairing a belt Curtis Ballard of Iglehart Bros., Evansville, Ind., received a severe wound in the top of his head.

Exports of Grain Weekly.

[From Atlantic and Gulf Ports, in Bus., 000 Omitted.]

	Wheat.		Corn.		Oats.	
	1921.	1920.	1921.	1920.	1921.	1920.
July 2...	5,586	6,851	1,848	56	1,064	26
July 9...	5,981	5,771	2,817	35	1,154	800
July 16...	5,807	8,556	3,016	89	1,159	322
July 23...	5,359	8,990	3,132	157	908	1,006
July 30...	7,015	7,033	3,192	43	1,895	867
Aug. 6...	10,355	6,375	2,897	52	2,068	353
Aug. 13...	7,777	7,220	1,787	102	511	9
Aug. 20...	9,682	6,919	2,254	63	800	46
Total since July 1...	57,562	57,715	21,343	597	9,679	3,429

Grain Movement

Reports on the movement of grain from farm to country elevator and movement from interior points are always welcome.

INDIANA.

Hudson, Ind., Aug. 24.—Grain is moving quite freely.—Frank Stroock.

Valparaiso, Ind., Aug. 12.—Think we will have to ship in hay and oats for feed.—S. J. Brown, mgr. Chatfield Grain Co.

Anderson, Ind., Aug. 5.—Wheat and oats are 85% sold. Grain moving from the farmers nothing.—Union Grain & Feed Co.

Bicknell, Ind., Aug. 11.—On account of the wheat sprouting after threshing as a result of the heavy rains much of it will not be marketable at all.—O. L. Barr.

Evansville, Ind., Aug. 22.—Few farmers in southern Indiana and southern Illinois are selling this year's wheat crop, but are holding in the belief that the price will go higher before next spring. Most of the farmers are also holding their old corn not being satisfied with prices offered.—C.

IOWA.

Osceola, Ia., Aug. 15.—Movement of grain has eased up owing to recent rains.—Curnes Grain Co.

Inwood, Ia., Aug. 15.—Oats will average about 70 bushels, but are light in weight. Corn will be a bumper crop and is starting to dent.—G. A. Lyon.

KANSAS.

Lorraine, Kan.—The farmers are inclined to hold their grain for higher prices.—C. H. Veatch.

Burdett, Kan., Aug. 12.—Cars are scarce; all full here. Threshing is well along; weather dry and hot.—C. C. Brandon, Bauer-Vaughen Grain Co.

LOUISIANA.

New Orleans, La.—Vessels clearing this port during July carried 6,351,259 bus. wheat; 441,150 bus. corn; 28,580 bus. oats; 36,346 bus. barley and 25,714 bus. rye; compared with clearances last year in July of 5,277,656 bus. wheat; 98,020 bus. corn; 35,400 bus. oats; 415,334 bus. barley, and 25,000 bus. rye.—Geo. S. Colby, chief grain inspector & weighmaster.

MINNESOTA.

Worthington, Minn., Aug. 13.—Oat movement from farms very heavy.—Farmers Co-op. Co.

Middle River, Minn., Aug. 10.—No movement of new crop here yet.—E. O. Ramstead, mgr. M. R. Co-op. Elvtr. Co.

Brooks, Minn., Aug. 13.—Rye moving rather freely; not a great deal of wheat marketed.—The Farmers Co-op. Exchange Co.

West Concord, Minn., Aug. 15.—Grain coming in fine; but farmers are not selling much; storing as prices are too low. Holding on to oats and barley.—J. J. Christy, mgr. Farmers Elvtr. Co.

MISSOURI.

Arlington, Mo., Aug. 13.—Grain is moving fairly well.—O. P. Duncan.

Rush Hill, Mo., Aug. 15.—About two-thirds of the wheat crop has been shipped out.—Ed Hildebrand.

Agency, Mo., Aug. 17.—This station is falling 20,000 bus. short of its regular shipments.—E. Powell, mgr. Agency Elevator & Grain Co.

St. Louis, Mo., Aug. 22.—The bulk of the Missouri wheat crop has gone to market and what little is left is in the country elevators and the local mills will take it. This 1921 crop is the smallest in Missouri for the past 15 years.—Ed Hasenwinkle, Ed Hasenwinkle Commission Co.

Kansas City, Mo., Aug. 11.—William Craig, from Blue Springs, Neb., writes us as follows: Shock threshing will be about finished this week in this territory. There will be some stack threshing later on but no great amount. The wheat yield in this vicinity has been very disappointing indeed, averaging from 10 to 12 bus. per acre. The quality has also been seriously reduced since harvest by the frequent rains. The corn promise is excellent at this time and no doubt will be a bumper crop. Oats are poor.—Robert Nicholson, of the W. S. Nicholson Grain Co.

Kansas City, Mo., Aug. 11.—William Craig writes us from Blue Springs, Neb., as follows:

"There is some choice old corn around this section of the country yet."—Robert Nicholson of the W. S. Nicholson Grain Co.

Kansas City, Mo., Aug. 19.—William Craig of Blue Springs, Neb., writes as follows: Farmers are disinclined to market wheat and corn at the present declining prices. Most of them are busy plowing their wheat stubble and making hay.—W. S. Nicholson Grain Co., per Robert Nicholson.

MONTANA.

Vananda, Mont., Aug. 17.—Grain threshing will start in a few days.—X.

Sidney, Mont., Aug. 17.—Wheat is moving slowly.—Farmers Merc. Elevator Co.

Marsh, Mont., Aug. 22.—Corn, hay and oats will be insufficient for home consumption.—W. F. Schultz.

Shonkin, Mont.—The farmers are bringing grain in from the machines and are selling it as fast as they can haul it.—Charles Krull.

San Jon, N. M., Aug. 20.—Wheat is being marketed freely.—San Jon Elevator, per R. C. Mundell.

OHIO.

Ashville, O., Aug. 12.—Wheat movement during threshing is over and 75% of the crop is marketed.—G. C. Cline, mgr. Ashville Grain Co.

Columbus, O., Aug. 13.—Much wheat is being sold soon after threshing and averaged price received is \$1.11. Many farmers are also selling their hay while it is still in the meadows.—C. J. West, agricultural statistician.

OKLAHOMA.

Hopeton, Okla., Aug. 17.—Wheat is all threshed and 90% of it is marketed.—James Fay.

SOUTH DAKOTA.

Kennebec, S. D., Aug. 16.—Farmers are holding their wheat for a higher market.—A. I. Sinclair.

Moritz, S. D., Aug. 16.—Grain movement has just started and will be heavy from now on, especially the movement of oats.—Farmers Elevator Co.

Brandt, S. D., Aug. 19.—Threshing has just started here and the oats are yielding from 20 to 30 bus. to the acre and the barley from 10 to 20 bus. No wheat has been threshed yet.—P. J. Peterson.

TEXAS.

Galveston, Tex.—Exports of wheat for July amounted to 11,012,096 bus. compared with exports during July, 1920, of 4,906,242 bus.—H. A. Wickstrom, chief inspector, Galveston Cotton Exchange and Board of Trade.

UTAH.

Tremonton, Utah, Aug. 15.—Wheat is moving freely at present.—Mutual Fuel & Grain Co., per William R. Horne, sec'y.

THE YIELD OF WHEAT in China this year will be 20% less than in 1920 on account of the excessive rains, according to a Chinese press report. The 1920 crop of wheat was only 80% normal.

THE PROPOSED REPEAL of the Agricultural Act has come as a great surprise to the nation. The Act was popularly supposed to provide for at least four years minimum prices for wheat and oats. Imputations of bad faith on the part of the government are sure to be heard of from many quarters, but the Cabinet has probably acted discreetly in deciding now to scrap the measure, for it could not have remained for long on the statute book before it became a theme of dissension in a coalition government. Minimum prices will be guaranteed for the crop now growing, but afterwards all government control, subsidies and restrictions are to disappear. Many people looked upon the Act as a reactionary one and it is, in a sense, appropriate that it should be abandoned suddenly, for it came into existence hastily on Xmas Eve in a tired House and has never been really approved of by any important body of agriculturists.—*Corn Trade News*, Liverpool.

Coming Conventions.

Aug. 26. The Michigan Hay & Grain Ass'n at Detroit, Mich.

Oct. 3, 4, 5. Grain Dealers National Ass'n at Chicago, Ill.

Plea for Just Rates on Grain and Hay.

Commencing Aug. 15 the Interstate Commerce Commission for several days conducted hearings on the reasonableness of the rates for transporting grain and hay in the western territory. To save time all hearings were held at Washington, D. C. The hearings grew out of the complaint of the Kansas Commission that the present level of rates were too high for moving the deflated priced grain and hay.

When the hearings commenced there was present more than 100 interested persons representing the grain exchanges, grain ass'ns, hay ass'ns, farm ass'ns and the state commissions.

At the start of the hearings, Frank Smith, chairman of the Illinois Public Utilities Commission, asked that Illinois be included in the Western territory. The request was taken under consideration by the I. C. C.

A few of the complaints and suggestions offered the I. C. C. follow:

C. M. Reed repty. Kansas Commission: Grain is being sold at a loss to the growers. If conditions are not changed production will decline until the hunger of the world demands a greater production at prices that would net the grower a profit. Transportation charges have increased 175% over the levels of 1913. Transportation rates are wholly out of proportion. They are more than the traffic will bear.

A. E. Ramsey, chairman Federal Reserve Bank, Kansas City: Producers are unable to sell their products above the cost of production and distribution.

Curtis Mosier of the Minneapolis Federal Reserve Bank, suggested a reduction in rates to help the farmer liquidate.

J. Cassidy and Floyd Williams, both alfalfa meal millers in Colorado told that their business was threatened with destruction thru high rates because the principal markets averaged 1,200 miles from points of production. They both said they had lost much of their business thru the high freight rates.

J. W. Shorthill, Omaha, Neb.: In 1914 it took 114 bus. of corn to pay freight from Hampton, Neb. to Omaha on a car containing 1,566 bus. In 1921 it takes 392 bus. to bring the same amount of corn to Omaha from the same place.

Sec'y of Agriculture Wallace: High freight rates have nearly stopped agricultural production. If the rates are continued agricultural production will be much reduced. It will take 5 years to restore to productiveness, the western farms which are being forced out of operation.

J. P. Larson, repty. the Iowa Co-op. Grain Dealers Ass'n: It cost from 10 to 12% of the gross corn crop of Iowa to move it to market in pre-war times and now it costs from 25 to 29%. Farmers will get the benefit of any rate reduction.

T. B. Tilson, Kansas City, Mo., repty. Kansas City Hay Exchange: Hay can be shipped from the Imperial Valley in California to New York via the Panama Canal for \$12.75 per ton compared with a charge for shipping hay by rail from Kansas City to New York of \$18.50.

J. T. Stuttie, repty. Omaha Hay Dealers Ass'n: Because of the high rates restricting the market much hay in Nebraska is rotting in the stacks.

Several farmers and persons in close contact with them told of how the farmers would benefit from a reduction in rates. T. E. Cashman, a Minnesota farmer, said: Lower freight rates on grain and hay would increase the volume of traffic, not only in those commodities, but the increase would be reflected in added purchases by farmers.

Of the conditions in South Dakota, M. R. Benedict said: Many South Dakota farms will go back into grazing lands unless conditions are improved. Bankers are much concerned and merchants hold little hope farmers will be able to pay their bills from

present crops, even if prices improve. Taxes on wheat land in South Dakota averaged \$1.50 an acre and freight rates on an average acre yield were \$1.44. Oats, costing the farmer \$6.94 an acre to produce sold for \$5.70, leaving the farmer \$1.24 out of pocket, without considering freight rates. A decrease of freight rates amounting to the recent general increase would add to the volume of traffic, and I think would increase the incomes of the railroads. Wisconsin dairy farmers are willing to pay \$18 a ton for alfalfa, of which the South Dakota farmers have plenty, but which cannot be shipped on account of high rates.

One of the most interesting bits of testimony was that of C. W. Hillman of Newark, N. J., who said that considering all freight, it cost the carriers 78.10 cents to move and handle revenue freight for which they received one dollar. He said the operating expenses of the roads in handling grain was but 62.73 cents per dollar.

Evidence presented by several western grain men on Aug. 23 was that eastern roads had reduced their rates on grain from Chicago to the Atlantic ports to meet the competition of the Canadian carriers. They also pointed out that further reduction in the rates on export grain would benefit the farmer by materially increasing the amount of grain exported.

THE BUREAU OF CHEMISTRY will hereafter be in charge of Walter G. Campbell who succeeds Dr. Carl Alsberg. Mr. Campbell has held the position of ass't chief of the Bureau since 1916.

Chicago Board to Stop Trading in Puts and Calls.

The directors of the Chicago Board of Trade on Aug. 23 ordered posted for a ballot by the members an amendment to the rules rescinding Sections 19 and 20 of Rule XXII effective on and after Oct. 1, 1921.

These sections cover trading in bids and offers for deferred acceptance, at one time called ups and downs and more accurately described as puts and calls. If adopted, as it probably will be under the Capper-Tincher bill, the abolition of privileges is expected to have the effect of giving a wider swing to the daily range of prices and to cut down the commission earnings of brokers in futures by a considerable percentage.



Chas. J. Peters, Chicago, Ill., Chief Grain Inspector.

Galveston Flood Damage an "Act of God."

The Court of Civil Appeals of Texas on Apr. 1, 1921, decided against the Fort Worth Elevators Co. and in favor of Keel & Son, defendants, in a suit brot for the value of two cars of wheat destroyed in the flood of Aug. 16, 1915, at Galveston.

A sale of three cars was made over the phone by Jules Smith, of Fort Worth, and J. Z. Keel of Gainesville for Galveston delivery, Galveston official weights and grades. The grain was immediately loaded out and drafts paid. In a vain endeavor to save the two cars in question the Wharf Commission undertook to haul them to the mainland but was forced to leave them in a yard with a carload of lime that caught fire when the water rose, the fire spreading to the cars of wheat.

The Fort Worth Elevators Co. brot suit against Keel & Son, the Galveston Wharf Co., and the Santa Fe Railroad Co., but as to the carriers the court held the loss was an act of God, and as to Keel & Son that the title had passed to the Fort Worth Elevators Co., stating that

Where goods are purchased and are shipped by rail, consigned to shipper's order, and drafts for the price are sent through banks with B/L attached, and such drafts are paid by the purchaser and Bs/L delivered to purchaser, title to such goods passes to the purchaser. Where the entire shipment is sold and paid for, as in this case, but must be measured or weighed at destination, with a view of definitely determining the exact sum to be paid for the entire mass, the title passes to the purchaser, notwithstanding the necessity of such measurement or weighing. Robinson & Martin v. Houston & Tex. Cent. R. Co., 105 Tex. 185, 146 S. W. 537; Boaz & Co. v. Schneider & Davis, 69 Tex. 128, 6 S. W. 402. This proposition is, we think, too well settled to require citation of other authorities.—231 S. W. Rep. 481.

New Chief Grain Inspector at Chicago.

Chas. J. Peters, the new chief grain inspector at Chicago, is a native of Indiana, having been born at Hobart Sept. 29, 1876. At the age of 12 his father died and the lad became one of the breadwinners of the family and his business education was gained at night in commercial schools. As a youth he was always an interested participant in athletics and baseball. He has been a member of one of the best bowling teams in the central West. He is married and has one child, aged 15.

When Tom Hunter, who was well known as a seed expert on the Board of Trade, became bailiff of the Municipal Court, young Peters was given his first entry into business as chief clerk in that court, his ability as a stenographer having attracted attention. This was followed by the position of chief clerk in the city attorney's office after which he engaged in the real estate business on his own account, with offices in West North Avenue. Here he gained an experience that won for him the office of Condemnation Commissioner on widening of streets for the Board of Local Improvements.

Mr. Peters has always been affiliated with the Republican party, and is that type of party man that stands for the highest ideals in public life and for those things which he believes will help to advance the best interests of the city, state and nation.

In 1916 he was elected Republican Ward Committeeman for the 35th Ward, and has filled this difficult office with a patience, tact and experience that demonstrated the possession of the executive ability and force needed to head the state grain inspection department. He was one of the first organizers of the movement to nominate and elect William Hale Thompson Mayor of Chicago. He is a member of several fraternal orders and he has a multitude of friends on the Northwest side of the city. A portrait of Mr. Peters is given herewith.

Buyer Can Reject for Carrier's Error in Denying Inspection.

Edwin Fore, Pittsburg, Tex., sold 1,000 bus. of peas to correspond with sample to the Plant Seed Co., St. Louis, Mo. It was agreed that inspection should be permitted and Fore so billed out the carload, but the railroad agent at Pittsburg by mistake wrote on bill "No inspection."

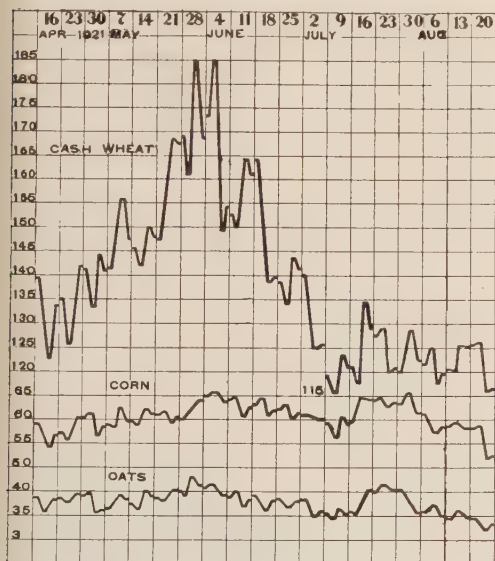
On arrival at St. Louis the Terminal Railroad gave postal card notification, and on the card copied the notation "No inspection." By 'phone and by visit to the Terminal office buyer demanded inspection, but was informed inspection could not be granted until authorized by the Cotton Belt Railroad, issuing the original way-bill. Buyer then 'phoned Fore that as there was no inspection allowed he refused this and all other shipments on the contract.

Fore brot suit and was given judgment in the St. Louis Circuit Court, but on appeal this was reversed in favor of the Plant Seed Co., defendant, by the St. Louis Court of Appeals June 29, 1921, stating:

It is said that, because the invoice and B/L contained the notation, "Allow inspection," and

Cash Wheat, Corn and Oats Fluctuations from Apr. 11 to Aug. 20.

Opening, high, low and closing average prices of No. 2 red winter wheat, No. 2 mixed corn and No. 2 white oats at Chicago each week are given on the chart herewith. The daily average is used in charting; actual prices were made each week a few cents above or below the extreme charted.



Daily Closing Prices.

The daily closing prices of wheat, corn and oats for September delivery at the following markets for the past two weeks have been as follows:

SEPTEMBER WHEAT.

	Aug. 10	Aug. 11	Aug. 12	Aug. 13	Aug. 15	Aug. 16	Aug. 17	Aug. 18	Aug. 19	Aug. 20	Aug. 22	Aug. 23	Aug. 24
Chicago	122	124 1/4	123 3/4	124 1/4	124 1/2	122 1/2	119 3/4	116 3/4	116 3/4	114 1/2	116 3/4	118	118
Kansas City	111 1/2	114 1/4	113 3/4	114 1/4	115	112 1/2	110	106 3/4	107 3/4	105	107	108	108 3/4
St. Louis	118 3/4	121 1/4	120 3/4	122 1/4	122 3/4	120 3/4	117 3/4	114 1/4	114 3/4	112 1/4	113 3/4	115 3/4	116 3/4
Minneapolis	129 1/4	132 1/2	131 1/4	131 3/4	132 1/4	130 3/4	128	124 3/4	124 3/4	121 3/4	124 1/4	125 3/4	127
Duluth	127 1/4	130	130	129 3/4	130	147 3/4	145 1/4	138 3/4	142 3/4	138 1/2	137 3/4	139 1/4	139
*Winnipeg	147 3/4	150 3/4	149	150 3/4	149 1/4	147 3/4	145 1/4	138 3/4	142 3/4	138 1/2	137 3/4	139 1/4	139
Toledo	128 1/2	131	130	130 3/4	130 3/4	128 1/2	125	120 3/4	121 1/2	119	122	122 1/2	123 1/4
Milwaukee	122	124 1/4	123 3/4	124 1/4	124 1/2	122 1/2	119 3/4	116 3/4	117	114 3/4	116 3/4	118 1/4	118 1/4

SEPTEMBER CORN.

	Aug. 10	Aug. 11	Aug. 12	Aug. 13	Aug. 15	Aug. 16	Aug. 17	Aug. 18	Aug. 19	Aug. 20	Aug. 22	Aug. 23	Aug. 24
Chicago	56 1/4	56 1/4	56 3/4	56 3/4	56 3/4	55 3/4	54 3/4	52 3/4	52 3/4	51 3/4	53 3/4	53 3/4	54 3/4
Kansas City	45 3/4	45 3/4	45 3/4	46 1/4	46	44 3/4	43 3/4	42 3/4	42	41 3/4	42 3/4	42 3/4	42 3/4
St. Louis	53 3/4	54 3/4	53 3/4	54 3/4	54	52 3/4	51 3/4	49 3/4	49 3/4	50 1/4	50 1/4	50 1/4	51
Milwaukee	56 3/4	57 3/4	56 3/4	56 3/4	56 3/4	55 3/4	54 3/4	52 3/4	52 3/4	52	53 3/4	53 3/4	54

SEPTEMBER OATS.

	Aug. 10	Aug. 11	Aug. 12	Aug. 13	Aug. 15	Aug. 16	Aug. 17	Aug. 18	Aug. 19	Aug. 20	Aug. 22	Aug. 23	Aug. 24
Chicago	36 3/4	36 3/4	35 3/4	34 3/4	34 3/4	33 3/4	31 3/4	30 3/4	30 3/4	30 3/4	31 3/4	31 3/4	31 3/4
Kansas City	32	32 3/4	32 3/4	32 3/4	32 3/4	31 3/4	30 3/4	29 3/4	30 3/4	30 3/4	31 3/4	31 3/4	31 3/4
St. Louis	34	34 3/4	34 3/4	32 3/4	32 3/4	32	31 3/4	30 3/4	30 3/4	31 3/4	31 3/4	31 3/4	32 1/4
Minneapolis	31 1/2	49 3/4	49 3/4	30 3/4	30 3/4	27 3/4	28 3/4	27 3/4	27 3/4	28 3/4	29 3/4	29 3/4	29 3/4
*Winnipeg	49	49 3/4	49 3/4	49 3/4	49 3/4	48 3/4	47 3/4	45 3/4	45 3/4	46 3/4	46 3/4	46 3/4	46 3/4
Milwaukee	36 3/4	36 3/4	36	35 3/4	35	33 3/4	33 3/4	32 3/4	32 3/4	33 3/4	34	34	34 3/4

*October delivery.

because the draft with the B/L attached was presented to defendants, therefore the plaintiff has discharged his duties toward the defendants. This, we think, falls short of the respondent's duty under the contract to the defendants. This did not give defendants the right of inspection which was agreed upon and to which defendants were entitled. It clearly appears that the plaintiff's agents actually denied inspection to defendants, a necessary step before delivery. Plaintiffs' contention that, because holes were found cut in some of the sacks, the inference should be drawn that defendants in some manner inspected the peas, cannot be credited as evidence. Nor do we consider it necessary, in the light of the positive proof to the contrary, to discuss the insistence that there was a custom on the part of the Terminal agents of granting inspection. At any rate, inspection was refused by them in this case.—232 S. W. Rep. 169.

Profit in Mixing Oats.

The poor quality of the new oats on this crop has created a situation that is proving very profitable to the mixing houses and warehousemen who are in a position to store oats.

In August, 1918, the receipts of No. 4 oats at Chicago were only 189 cars, white, against 2,339 cars of No. 2 white; that is, the No. 4 were less than 9 per cent of the No. 2's.

This season the No. 4 are 56 per cent of the No. 2's, being 784 cars in August, against 1,385 cars No. 2. Consequently the mixers and warehousemen have more than 100 cars daily of No. 4 oats from which to select qualities suitable for mixing, and that grade is selling 6 to 7 cents under the September future. This would be of no particular advantage to the warehouseman were it not for the fact that No. 3 oats are deliverable on contract at only 1 1/2 cents per bushel discount.

The mixer can take one bushel of No. 4, required test weight 23 lbs., costing 29 cents and mix it with one bushel of No. 2, testing 29 lbs., costing 34 1/2 cents, and get two bushels of No. 3 oats, testing the required 26 lbs. for that grade, approximately but not quite, as either a little more No. 2 or a higher test of each must be used to get the test up. The mixture would cost the warehouseman about 31 3/4 cents per bushel and can be sold for the September delivery, less 1 1/2 cents discount, showing a net profit at the start of 2 3/4 cents per bushel, besides storage charges adding 3 cents to December.

A few bull speculators have invested in December oats heavily and the Chicago warehousemen are going to fill them up. The United States visible supply on Aug. 6 was the largest at any time in four years, standing at 41,699,000 bus., of which Chicago held 13,352,000, and much of the visible is last year's heavy oats. Usually the visible supply of oats keeps on increasing during the fall of the year until some time in November, December or January. A year ago, for example, the visible

increased from 4,000,000 Aug. 14 to 35,000,000 Nov. 6. At this rate the bulls will have a tremendous load of oats to carry when December rolls round. To add to their troubles the Government in its Aug. 9 report placed the amount of oats remaining on farms Aug. 1 as 161,199,000 bus., of last year's crop, against only 56,128,000 bus. so held a year ago.

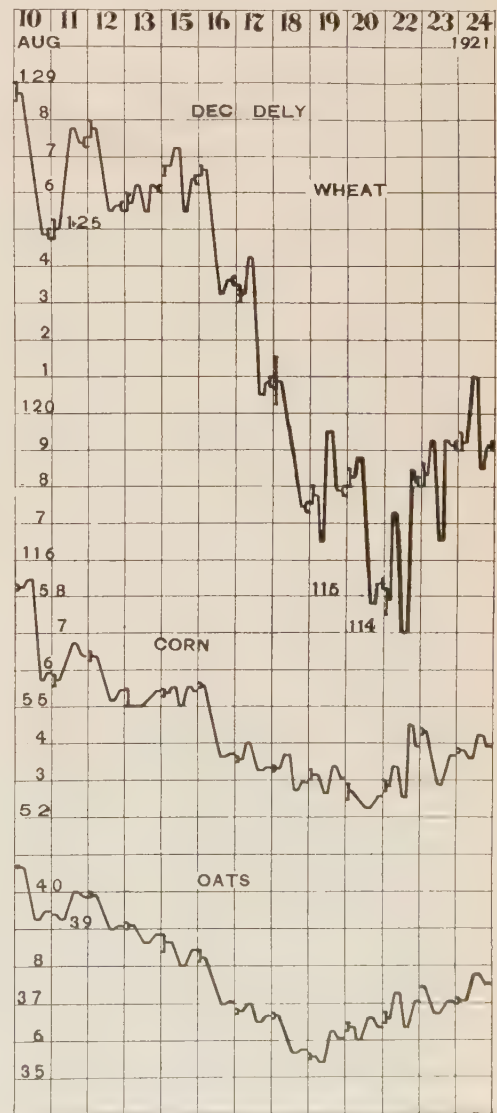
Loosen Grain Trade Restrictions in Germany.

The German Government is slowly loosening its control of the grain trade. The latest development was the announcement that grain in the future could enter Germany without the payment of an import duty and without restriction of any kind. As in the past, the products of the German flour mills will be afforded protection.

Of the grain raised in Germany this year the government has decided to take only 2,500,000 tons of the estimated 15,000,000 ton crop. It has also announced that all grain not required by the government could be freely dealt with in home markets.

Chicago Futures

Opening high, low and close on wheat, corn and oats for the December delivery at Chicago for 2 weeks past are given on the chart herewith.



Elevator At Portland, Ore., Which Settled.

The 1,000,000-bu. reinforced concrete grain handling plant shown in the photographs reproduced herewith was built as a municipal enterprise by the Public Dock Commission of Portland, Ore. The funds for its construction were obtained from a bond issue of \$3,000,000 voted by the people of Portland in 1917 for the building of the elevator and for other purposes connected with the providing of terminal facilities. The plant is known as the St. Johns Municipal Terminal Grain Elevator, it being located on a slip dredged to connect with the Willamette River in a section of the city called the St. Johns District. The plant consists of head house, a storage annex, a track shed, a transformer house and a conveyor gallery.

The operating house is 182 feet high, its base dimensions being 106 ft. 2 in. by 46 ft. 2 in. It contains 78 bins with aggregate capacity of 293,510 bus.

The operating house comprises five stories above the bins. These, in their order from bottom to top, are the distributing story, 18 ft. high; transfer story, 16 ft.; scale story, 16 ft.; garner story, 15 ft.; and top or head story, 18 ft.

The storage annex is 135 ft. 7 in. by 105 ft. 7 in. at the base, the tanks being 100 ft. high. It consists of a total of 99 main and interstice bins with combined capacity of 755,100 bus. The total storage capacity of the elevator is 1,048,610 bus.

The concrete walls of the operating house are 8 in. thick and those of the storage annex 7 in. thick. All bins in operating house and storage annex have concrete slab tops to confine the dust arising from grain as it is spouted into the bins. This makes it easier to keep the house clean and lessens the danger of dust explosions.

There are 12 legs in the operating house. Three receiving legs take grain from the belt conveyors in the basement of the track shed and loft it for weighing; 3 shipping legs receive grain from the bins in the operating house and from belt conveyors from the storage annex and discharge it to the transfer conveyor, the car loading spout, or the conveyors running to the shipping gallery; 1 receives clean grain and another smutty grain from the belt conveyor running from the pier; while 2 legs receive grain from the separators and two from the smutters. The 6 receiving and shipping legs each have 2 rows

of 7x7x14 buckets, staggered. All other legs are fitted with single rows of buckets, the size for the separator leg, the clean and smutty grain legs being 7x7x16; those of the smutter leg 7x7x12 and those of the leg in the tower on the pier, 7x7x14. All buckets are spaced 13-in. centers. Each leg in the operating house is fitted with a back stop on the head shaft to prevent the belt from running backward when the power is shut off.

Leg casings are of steel; spouts, also, are of steel, all rectangular spouts being formed with the bottom and two sides, in a single piece.

The conveyor gallery extends from the head house to the slip and thence runs parallel with the slip. In addition to the bulk handling facilities, a covered pier 1,200 ft. long and 180 ft. wide is provided for sacked grain and general cargo, and the portion of the conveyor gallery along the slip is above this pier. The warehouse portion of the pier is built to permit the sacking of grain coming to it from the head house; and grain can be received in bags in the pier warehouse, there bulked, and the bulk grain conveyed to the head house by means of a supplementary conveyor installed for this purpose.

Grain is taken from the head house to the storage annex by belt conveyors operating above the tanks, and it is removed from the tanks by conveyors operating in tunnels beneath them. Other belt conveyors operate in the main gallery from the head house to the discharge spouts along the pier.

In addition to the belt conveyors operating above the storage tanks, and those in the basement below the tanks, there is one 36-in. transfer conveyor in the cupola of the operating house; three 36-in. receiving conveyors from track shed to receiving legs; three 36-in. transfer conveyors in the track shed basement for shifting grain from one conveyor to another; two 36-in. shipping conveyors in the gallery running from the operating house to the pier; two 36-in. in the shipping gallery along the pier; one 36-in. from the pier gallery to sacking bins; one 24-in. conveyor below the floor of the pier; and one 24-in. in the gallery extending from the pier to the operating house. The transfer conveyor in the cupola of the working house, as well as the conveyors above the storage annex and those in the shipping gallery are provided with suitable trippers.

Grain received in cars is unloaded in the track shed. If in bulk it is discharged into one of the four track hoppers in the usual

manner by means of power shovels. Bagged grain received in cars is unloaded in the shed onto a spacious platform and the bags are then opened to empty the grain into the receiving bins beneath the platform. Belt conveyors remove grain from track hoppers and discharge it to either of the three loft leg operating in the head house.

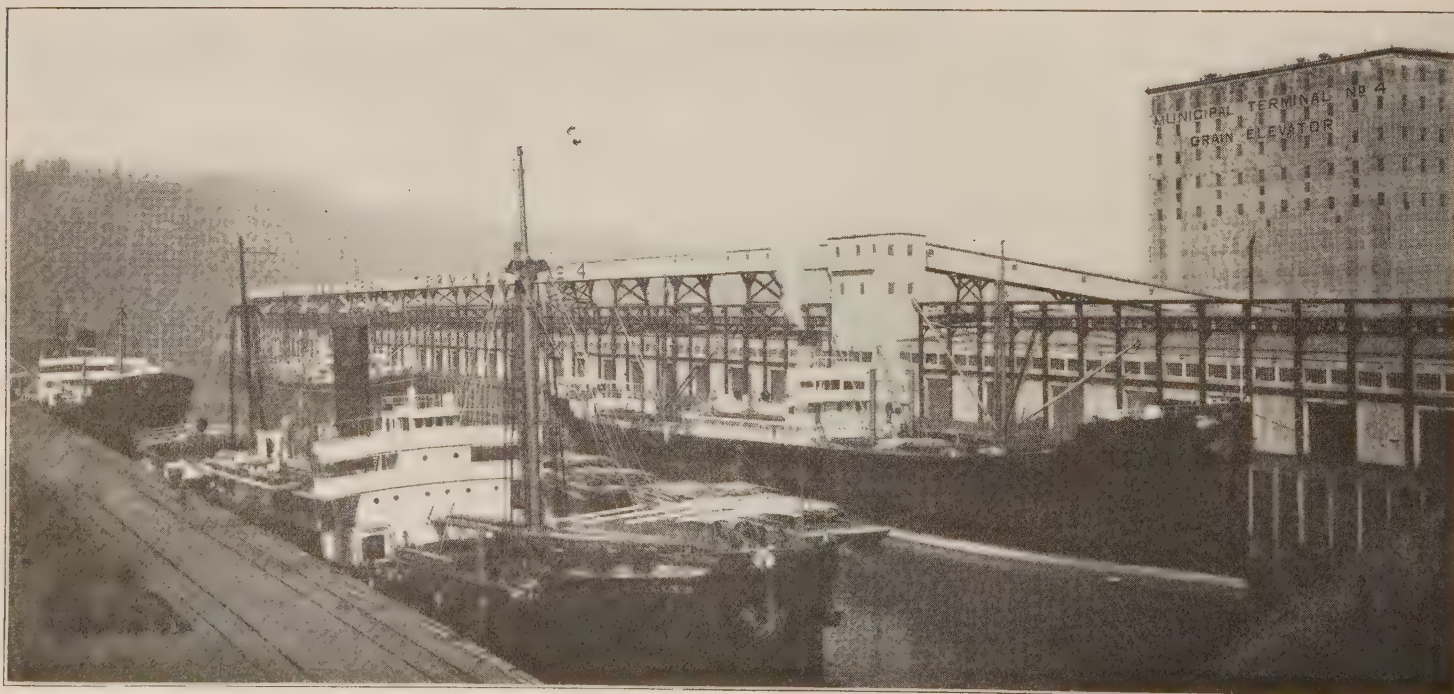
Twelve unloading sinks are located under the two tracks in the track shed and 3 cars can be unloaded simultaneously on each of the 2 tracks. Discharge gates of the sinks are controlled by interlocking levers so that only one of a set can be open at any time.

Grain received into the operating house from cars is weighed in any of the 10 hopper scales located on the scale floor. Nine of these scales are of 2,000-bu. capacity and one of 1,000 bu. All have steel hoppers and suitable garners are located above the scales in the garner story.

An unusual feature of the elevator is the arrangement permitting grain that is received in a smutty condition to be kept separate from grain free from this infection. This is accomplished by setting aside certain conveyors a leg and a set of bins for use exclusively in handling smutty grain. The head house also contains machinery for removing smut from grain. All this is especially valuable in view of the fact that a rather large percentage of the grain which the elevator is expected to handle will probably be infected with smut.

Power for the operation of the plant is furnished by 47 Allis-Chalmers electric motors. These can develop a total of 1516 horse power, the ratings ranging from 3 h.p. for the employees' belt elevator to 225 h.p. for each of the 6 receiving and shipping legs. In most cases the drive from motor to driver machine is by silent chain, belt and rope being used for some, while a few are direct connected. Electric current is obtained from a central station. It is received at the transformer house at a tension of 10,500 volts. Here it is stepped down to 440 volts for power purposes and to 110 volts for lighting. For this purpose three 250 k.v.a. and one 50 k.v.a. transformers are used. The electrical system comprises all approved safety devices.

Communication is carried on thruout the plant by means of an inter-communicating telephone system and papers can be passed from scale floor to foreman's room by means of a dummy elevator. Employees' belt elevators and a passenger elevator enable workmen to go from one floor to another, and



Conveyor Gallery and Loading Piers, Municipal Elevator, Portland, Ore.

These mechanical lifts are supplemented by a complete system of ladders.

Cleaning machinery comprises a total of six machines. These are 2 No. 9 Style "B" Monitor Warehouse Separators, 2 No. 12 Monitor Warehouse Separators, and 2 No. 9 Monitor Smutters. The machines are of improved type, and all wood portions are steel covered to make them more nearly fireproof.

A complete dust collecting system is installed. This includes fans, trunking, cyclones, sweeps, etc., and it extends thruout the entire plant, effort having been made in designing and installing it to guard as fully as possible against dust explosions. The removal of dust to obviate the physical difficulties its presence brings was also a consideration.

THE ELEVATOR PLANT, which was designed by the Witherspoon-Englar Co. and built by Grant Smith & Co., is an up-to-date grain handling plant fully equipped with grain cleaning and scouring machinery. It is located on tide water, where ships in the Pacific trade can reach its slips readily, and was designed to provide rapid handling facilities for export grain.

When filled with grain, it is estimated that the elevator will impose upon its foundations a weight of 105,813 tons, variously distributed between each of the buildings, the storage annex being the heaviest with an estimated weight when loaded of 59,713 tons. The estimated weight of the whole elevator when empty is 74,363 tons, of which 37,063 tons are contributed by the storage annex, 30,300 tons by the head house and 7,000 tons by the track shed. This weight is exclusive of that of the conveyor gallery, which is comparatively light. It is thus seen that the storage annex, when empty, imposes about 62% of the weight on the foundation that it imposes when loaded.

The driving of the piles for the foundation was done under separate contract let by

the Dock Commission. The designer had no voice in this work except to prepare a plan showing the number of piles, their location, the weight they would be required to support, the distribution of this weight, and other similar matters that had to do with the relation of the piles to the foundation and superstructure. The piling plan was approved by the Dock Commission.

After the piles had been driven, the foundation slab was poured, the foundation piers were erected, and the work of pouring the walls of the superstructure progressed. When this had advanced to a stage nearing completion, it was discovered that one corner of the storage annex had subsided slightly. Further investigation disclosed the fact that this subsidence or settling affected the annex at more than one point. It continued settling until one corner has subsided about 18 inches and the diagonally opposite corner about 9 inches. When the settling was discovered the walls of the conveyor tunnels were cut away from the head house in order that they might be prevented from breaking loose.

The building of the plant was stopped when the subsidence was discovered, and the subsidence being clearly due to failure of the piles to carry the load it was the responsibility of the Dock Commission to determine the nature of the cause and to plan and execute a remedy.

Engineers of the Dock Commission studied the matter and a board of consulting engineers selected by an engineering society of Portland was appointed to investigate it. This board of engineers was able to agree on the cause of the trouble, namely, that as the elevator had been built on made land the piles placed under the foundation did not provide sufficient support; but they were unable to agree on the remedy. Four of the five members of the board concurred in a report to the effect that a spread foundation was needed under the storage annex and that concrete

cylinders extended to gravel should be placed under the head house. The other member recommended that concrete cylinders be placed beneath the entire structure. He explained in detail the method he would adopt for placing them under the buildings which were then practically completed.

A plan adopted at one time, and put into execution, was that of driving extra "pinch" piles around the structure on the sides where the subsidence had occurred. The start of this work was made by driving a line of piles around the side of the structure some distance from the wall lines; later driving more piles between the first row of piles and the tanks, gradually driving others nearer to the building. In this way the earth was compressed until it was reported that the elevator actually had been pushed upward somewhat from the point to which it had sunk.

The difficulty of deciding upon a remedy for the subsidence continued for a number of months, engineers disagreeing among themselves and the officials apparently being unable to decide which of the proposed courses to adopt. Construction work meanwhile was halted, the contractor not being permitted to proceed with the superstructure. Finally, by agreement with the Dock Commission, the contractor was paid an agreed sum for the surrender of the contract and the Commission took over all work of completing the plant, relieving the contractor and designer of further responsibility. Thruout the negotiations and investigations incident to the settling of the plant it was recognized that as the failure was readily traceable to the piling, the designer and contractor were not in any sense responsible.

It has been impossible to obtain information as to whether the plant has yet been placed in operation.

The cause of the subsidence, as agreed upon by all engineers who studied the case, was the failure of the piles to support the weight.



1,000,000-bu. Municipal Terminal Elevator at Portland, Ore.

This failure, in turn, was conceded to be due to the nature of the soil; and the nature of the soil had not been determined before the piles were driven. All the engineers were of the opinion it would have been possible to have built a satisfactory foundation preliminary to the erection of the superstructure if there had been enough investigation to determine the piling support required, but the borings made after the subsidence gave knowledge only too late of what should have been learned in the beginning.

The character of the earth underlying the elevator site may be gained from a report of experts from the Oregon Bureau of Mines and Geology, who said that what had previously been described as "cemented gravel" 190 feet below the ground level is merely loosely deposited river wash gravel mixed with muck. They found no sheeted material or solid strata at any point reached by the borings, nor any evidence of any regularity or uniformity in the deposits of muck, silt, clay, quicksand and light gravel, samples of which were brought to the surface.

Bulgaria Still Considering Bulk Handling.

On account of the considerable losses suffered by the producers in all parts of Bulgaria because of the inadequate facilities for handling export grain, the Bulgarian Grain Consortium is still considering the establishment of a system of grain elevators. The members of the Consortium feel that the grain saved by the use of grain elevators will more than pay for their construction in a few years.

To further study the matter the Consortium has deposited 160,000,000 leva with the Agricultural Bank of Bulgaria to pay the expenses of a technical commission who are to plan a scheme of bulk handling suitable to the needs of Bulgaria.

The Commission is now at work and its findings are to be presented to the Council of Ministers for approval. Of the probable decision of the Commission Consul Graham Kemper at Sofia, Bulgaria, writes as follows: The first elevators will probably be established at Varna and Bourgas, followed by elevators at the Danubian ports and interior points. After the plans have been definitely approved, a date will be fixed for international competition for the contract.

IN ROUMANIA the trade in all cereals will continue unrestricted according to a recent government report.

Wheat Here and in Europe.

To prate of lack of money, lack of confidence, the economic situation, and unsettled business conditions as reasons for the demoralized grain markets in a country like the United States seems rather silly. The real reason is that the general public knows nothing whatever concerning probable supplies of wheat for the coming year and cares less. Europe is using her debased currency for the purpose of buying wheat and other cheap food necessities to the full extent of its buying power, and is straining her credit still further to secure supplies, for Europe realizes that no luxury, no matter how precious, or how much desired, is worthy of consideration in the face of starvation.

In the United States the situation is reversed: luxuries still command high prices, while food necessities are ignored. The wild spending orgy for banquets, for entertainment, for sports and recreations, goes on with almost undiminished force, but we have no money with which to buy wheat or flour ahead of our daily needs. In the course of a few months, the general public may become aroused to the situation, when wheat will be considered one of the most valuable things that money can buy.—Hulburd, Warren & Chandler.

How Marketing Plan Hits Co-operative Elevator.

Coincident with its effect on the farmer, the scheme proposed under the name "Grain Marketing Plan of the Com'te of 17" will react upon the established co-operative elevator companies participating in it. In the case of the elevators, however, there will be lacking the factor of more or less money for their stocks as a rule with which to measure the benefits, or their opposite. The elevator company will not be affected so adversely or helped so much by the receipt of a lower or a higher price for the grain. Its profits will always depend upon the difference between the price it pays for grain and the price at which the same grain is sold.

The effects on the elevator company will, therefore, be of another nature altho none the less positive. To state some of them before their causes are enumerated, the operation of the plan will do this to the co-operative elevator company:

1. It will make the elevator a mere accumulating depot for the promoters.
2. It will result in a run down mechanical and physical condition of the plant.
3. It will make the elevator manager a mere clerk.
4. It will kill the opportunity on the part of the manager, his assistants, and the directors to excel.
5. It will kill local pride in the institution.
6. It will remove the need for study, application and work to process and to handle the grain advantageously.
7. Ultimately it will result in the disintegration of every co-operative elevator company it ensnares.

A peculiar feature of the plan, as it is explained by the booklet of the U. S. Grain Growers, Inc., is that which precludes the elevator companies from selling thru the U. S. Grain Growers, Inc., any grain from growers who have not signed contracts. This means that the local elevator will be doing two classes of business, that is, it will be buying grain from both signers and non-signers of the contract. Grain from signers must be disposed of thru the U. S. G. G., while that from non-signers must be sold elsewhere. Just how it will be physically possible to keep the grain of signers and non-signers separate is not clear, but that is a detail and details never worry dreamers.

The manager needs only to be a clerk to handle signers' grain. He must be a manager and he must understand marketing to handle non-signers' grain. The elevator company will have to pay a manager's salary for one who spends part of his time doing very ordinary clerical jobs. A good manager will find this an intolerable situation; and an ordinary clerk will fall down on the managerial work.

As to that part of the grain coming from signers, the manager, his assistants, and the directors will have no incentive to handle it in a careful, efficient manner to get the highest possible price for it, because they would receive no credit for any extra effort. Its course thru a stipulated agency is fixed by the contract. The agency may charge whatever it is pleased to charge for handling it, and it is the farmer's loss and not that of the elevator. To be sure, there is a clause in the elevator contract about "negligence," but what is negligence?

Negligence, it may be pointed out, is in the same category with that elastic "reasonable" found so many times in the food control act. There, "reasonable" gave us much humor and more seriousness during the war. "Negligence" may do the same thing here. In fact, one clause in the elevator contract printed in the booklet reads:

"The Elevator Company shall be responsible for and charged with allowances, deductions or losses made or sustained by the U. S. As-

sociation arising from the negligence of the Elevator Company." That brief statement could become big enuf to wreck any elevator company that runs afoul of it, for, again—what is "negligence?"

DEPRECIATION, deterioration and disintegration of the physical property of the elevator company, the elevator plant itself, will begin as soon as the contract is signed and made effective. There will be no incentive to keep the property in good condition, and because of the thing which will happen to profits there will seldom be money to pay for the doing of it regardless of the incentive. The result will be that the signing of this contract and participation in the plan will ultimately result in the physical annihilation of good elevator plants that have required the efforts of years in their establishment.

No person lacking in the actual experience can properly appreciate the pride of a community in a successful and efficient local industry. As the business of the co-operative elevator becomes more disorganized under the operation of the plan the community will see evidences of the retrogression. Local pride will wane and its potent force will no longer be felt in maintaining these elevators at their previous efficiency.

The plan professes to purpose the conditioning of the grain in farmer owned conditioning plants. Just where these are to come from is not apparent, but it is to be inferred that they are to be located at central points. This will remove from the local elevator their opportunity to place the grain in proper condition for most satisfactory marketing and there will no longer be need for study, application and work to care for this important phase of country grain handling.

The distribution of the money received by the U. S. Grain Growers for grain sold for signers of the contract is really of greater concern to the growers, but much of the money will of necessity be handled thru and by the local elevator companies and these companies are vitally interested in the percentage of the total amount which will be held back by the U. S. G. G. for any and all purposes. The elevator company's interest in this is likely to increase by leaps and bounds as time passes because there is no limit to the portion that may be retained in most cases and this is certain to be a source of trouble and misunderstanding between the elevator company and its farmer patrons. Imagine what fun it will be for the manager to tell Buck Jones that he has only 87c per bushel coming from his pooled wheat when the manager knows Buck had a chance to sell for \$1.09 spot cash.

Cost of handling is an important factor in all grain handling. It will not require long for the farmer to gain a wholesome respect for the ability of the present marketing agencies in that direction. He will soon understand that it does cost money to handle grain and he will not find it difficult to determine that the cost under the new system is infinitely greater than he was ever asked to pay under the present system of the competitive marketing.

The elevator contract and the grower's contract contain the same provision for cancellation within a certain 15-day period. There is also a requirement that "The party desiring to make such termination must, prior to the effective date of the same, pay any indebtedness then due the other party." Now, just suppose there is a controversy between the U. S. G. G. and one of its elevator companies. The U. S. G. G. claims the elevator company owes a certain sum of money and the elevator company disputes the debt. The elevator company gives notice of intention to terminate the contract, but refuses to pay the disputed amount. In that case the contract will not be terminable on the desired date. Furthermore the contract reads: "This contract shall be in force from its execution to June 30, 1922, and thereafter shall continue in full force and

effect as to each of the parties hereto from year to year It is automatically extended indefinitely.

The elevator company agrees to market all grain from signers thru the U. S. G. G., but the U. S. G. G. only agrees to endeavor to sell that grain. If it fails to sell it, the grain presumably will be in that puddle called a status quo or some other equally thorny place.

The pastures are always greener on the other side of the mountain. Chimerical marketing plans are ever more attractive on paper than the one we have—until operation brings disillusionment.

The grain marketing plan of the com'te of '17 is but another effort to set aside the workings of natural and economic laws and it will fail. The only question remaining is that which has to do with the number of farmers, co-operative elevator companies and others who will suffer themselves to be drawn into the maelstrom created by the collapse of the plan and thus be carried down with it in its inevitable failure.

SUGGESTIONS FOR AMENDMENTS to the trade rules of the Grain Dealers National Ass'n to be considered at the coming October meeting at Chicago will be welcomed by H. L. Strong of Wichita, Kan., a member of the com'te.

THE SCROGGINS GRAIN CO., of Minneapolis, Minn., has been named by the Department of Agriculture as guilty of invoicing wheat as containing no dockage when the inspection certificates showed 1 to 5 per cent dockage.

LOW PRICED WHEAT is all right, but low priced wheat with high priced flour and high prices for other things the wheat growers buys, has the same effect on business as a monkey-wrench in your gear case.—Babson's Barometer.

THE BUREAU OF MARKETS and Crop Estimates during the period of reorganization is to be in charge of Prof. G. F. Warren of Cornell University, according to the sec'y of agriculture. Mr. Warren is on leave from Cornell and during his stay with the department expects to make a special study of competition in agriculture and foreign demand for agricultural products.

Books Received

IS THE CHICAGO BOARD OF TRADE A GAMBLING HOUSE is unquestionably the best written and most up-to-date brochure covering the ethics of grain speculation and the economic service of the grain exchanges in the language understood by the man in the street and quoting the best authorities. By Thomas F. Hoyne (Argus). Paper, 55 pages, the Edgar A. Russell Co., Chicago, Ill.

Fire Destroys Three Elevators.

The recent burning of three elevators at one time at Turtle Lake, N. D., is renewed evidence of the necessity for careful consideration of the exposure hazard in connection with the other problems when selecting the site for your new elevator. In many cases the grain elevator stands in an isolated location and the owner seldom gives thought to the danger of fire communicating to it from other structures. Gradually, however, this position of isolation is being removed from the elevator. More buildings are being built at practically every station and the exposure hazard becomes each year a matter of greater concern to fire insurance companies specializing in elevator risks.

The distance between the elevator and other nearby buildings governs the degree of the exposure hazard, the closer the building the greater being the danger. It is true, however, that there can be no hard and fast rule because under certain conditions the distance to which fire will communicate may vary.

The three Turtle Lake elevators are shown in the photograph reproduced herewith which was made after one house had burned to the ground and while the other two were burning. The ruins at the left represent what then remained of the elevator of the Occident Elevator Co.; the elevator and office in the middle was that of the Equity Elevator & Trading Co.; while that at the right belonged to the Andrews Grain Co.

Fire was started in the Occident Elevator Co.'s plant by lightning. As this structure burned the flames successfully reached and ignited the other elevators. A fourth, that of the Regan & Lyness Co., also caught fire but heroic efforts on the part of the fire fighters saved it. This elevator is not shown in the photograph.

There is no information to show whether the spread of the fire was aided by wind but the photograph would indicate that whatever breeze there was did not blow directly from the first elevator toward the others.

Neither of the elevators was iron clad and this, together with the fact that all had shingle roofs, undoubtedly assisted the fire to spread. If the plant first to take fire had been protected by properly grounded iron siding and roof there probably would have been no fire. If the other elevators had been fitted with metal roof and siding they might have been saved from the flames.

All of the elevators are to be rebuilt. The Equity Elevator & Trading Co. has let contract to the T. E. Ibberson Co. for a 40,000-bu. house equipped with a truck scale, 20-h.p. engine, 2 legs, a 2,000-bu. automatic scale, and having 16 bins, all hoppers. It is to rest on a reinforced concrete slab foundation and the driveway level is to be lower than usual, thus eliminating a steep drive.

The Occident Elevator Co. and the Andrews Grain Co. will each build a 25,000-bu. elevator.

Agricultural Ass'n Not to be Immune.

Associations of producers of agricultural products will not be enabled by class legislation to disregard anti-trust laws, as the Senate struck out all after the enacting clause in the proposed law and substituted a milder measure providing that

Nothing herein contained shall be deemed to authorize the creation of or attempt to create, a monopoly, or to exempt any association organized hereunder from any proceedings instituted under the act entitled "An act to create a Federal Trade Commission" on account of unfair methods of competition in commerce."

Under the original bill the raisin growers monopoly that had to be dissolved could have been reformed under the wing of the Sec'y of Agriculture who was made all-powerful, and even he had no power to restrain their greed unless in his opinion prices had been "unduly enhanced."

All grain buyers of long experience will agree that no matter how high a price a grower is paid he never is satisfied in his own mind that he was paid enough; and if the Sec'y were like his farmer friends he never would make a finding that prices were "unduly enhanced."

AUSTRALIA'S exports of flour to the Orient are on the increase according to a report of the American Commercial Attache at Melbourne, Australia.

Billion for Grain Exports.

The House of Representatives on Aug. 22 passed the Senate bill appropriating \$1,000,000,000 thru the War Finance Corporation for the stimulation of agricultural exports, by a vote of 314 to 21.

The House eliminated Senate sections authorizing the purchase by the War Finance Corporation of \$200,000,000 worth of farm loan bonds and the creation of a new bureau in the Department of Commerce to obtain foreign trade information. The House further overruled its committee's action in eliminating a section to permit government loans to accredited foreigners, but added an amendment providing rigid restriction of such loans. Another House amendment would fix June 30, 1922, as the date on which liquidation of the War Finance Corporation would begin, with the capital stock of the corporation canceled in proportion to the surplus funds turned into the treasury.

By a vote of 196 to 136 the House rejected a motion to recommit the bill offered by Representative Wingo of Arkansas, to reinsert the eliminated Senate provisions for purchase of \$200,000,000 worth of farm loan bonds and to add an amendment authorizing the War Finance Corporation to make direct loans to agriculturists. Efforts will be made, Chairman McFadden of the banking committee said, to obtain an agreement between the House and Senate on disputed provisions of the bill so it can be sent to the president before the forthcoming recess. The Senate, without debate, disagreed with the House amendment and Senators Kenyon of Iowa, McNary of Oregon, and Smith of South Carolina were appointed conferees.

Injunction to Restrain Enforcement of North Dakota Margin Law.

W. C. Palmer, North Dakota state inspector, having defied the federal court decision declaring the law unconstitutional and having undertaken to enforce it, the grain elevator operators have applied to the federal district court at Minneapolis for an order enjoining the state from enforcing the law.



Three Elevators Burning Simultaneously at Turtle Lake, N. D. Left, Elevator of Occident Elevator Co.; Middle, Elevator and Office Equity Elevator & Trading Co.; Right, Elevator Andrews Grain Co.

Proposed Grades for Spring Wheat

Having failed to gain the approval of the Department of Agriculture for the desired changes in the grades of spring wheat the Minnesota congressmen have turned to the legislative method of forcing their ideas on the remainder of the country by a bill known as H. R. 7401, introduced in the House by Representative Halvor Steenerson June 27 and on Aug. 19 reported with amendments to the House and ordered to be printed. The bill follows, in full.

Be it enacted by the State and House of Representatives of the United States of America in Congress assembled, That the grain standards and grades for spring wheat heretofore fixed, established and promulgated by the Secretary of Agriculture under authority of the United States Grain Standards Act and now in force are hereby changed and modified so that that part of the regulations relating to spring wheat shall read as follows:

Class I. Hard Red Spring.

This class shall include all varieties of hard red spring wheat or wheats. This class shall be divided into two subclasses, as follows: Subclass (a) and subclass (b).

Subclass (a)—Dark northern spring: This subclass shall include wheat of the class hard red spring, consisting of 75 per centum or more of dark, hard and vitreous kernels. This subclass shall not include more than 10 per centum of wheat of the variety Humpback.

Subclass (b)—Northern spring: This subclass shall include wheat of the class hard red spring consisting of less than 75 per centum of dark, hard and vitreous kernels. If composed of more than 10 per centum of the variety Humpback, shall not grade better than number two of this subclass.

All foreign material in wheat, except rye, shall be classed as dockage, and all dockage shall be designated on the grade certificate but shall not affect the grade.

All moisture in excess of 14½ per centum shall be designated on the grade certificate and shall not affect the grade.

The minimum requirements for all grades shall be as follows:

Grade No.	Minimum test weight per bushel.		Maximum limits of—				
	Hard red spring. Pounds.	Durum. Pounds.	Rye. Per cent.	Total. Per cent.	Heat damaged. Per cent.	Wheat of other classes. Common white or club. Per cent.	Total. Per cent.
1	58	60	2	2	0.1	2.5	5
2	56	57	3	4	.2	3	10
3	53	54	4	7	.5	4	10
4	50	51	5	10	1.0	5	10
Sample (C).							

The above requirements to apply to amber durum and durum subclasses also.

Class II. Durum.

This class shall include all varieties of durum wheat and may include not more than 10 per centum of other wheat or wheats. This class shall be divided into three subclasses, as follows: Subclass (a), subclass (b), and subclass (c).

Subclass (a)—Amber durum: This subclass shall include wheat of the class durum consisting of 75 per centum or more of hard and vitreous kernels of amber color. This subclass shall not include more than 10 per centum of kernels of distinctively red color.

Subclass (b)—Durum: This subclass shall include wheat of the class durum consisting of less than 75 per centum of hard and vitreous kernels of amber color. This subclass shall not include more than 10 per centum of wheat of the variety red durum.

Subclass (c)—Red durum: This subclass shall include wheat of the class durum, consisting of more than 10 per centum of the variety of red durum.

Grade Requirements. Hard Red Spring Wheat.

This class shall be divided into four grades, as follows:

Grade one:

(a) The wheat in grade one shall be cool and sweet;

(b) Shall have a test weight per bushel of at least fifty-eight pounds;

(c) May contain not more than 2 per centum of rye;

(d) May contain not more than 2 per centum of damaged kernels, which may include not more than one-tenth of 1 per centum of heat-damaged kernels;

(e) May contain not more than 5 per centum of wheat other than red spring, including humpback, which 5 per centum may include not more than 2½ per centum of common white and club wheat, either singly or in any combination.

Grade two:

(a) The wheat in grade two shall be cool and sweet;

(b) Shall have a test weight per bushel of at least fifty-six pounds;

(c) May contain not more than 3 per centum of rye;

(d) May contain not more than 4 per centum of damaged kernels, which may include not more than two-tenths of 1 per centum of heat-damaged kernels; and

(e) May contain not more than 10 per centum of wheat other than red spring, which 10 per centum may include not more than 3 per centum of common white or club wheat, either singly or in any combination.

Grade three:

(a) The wheat in grade three shall be cool and sweet;

(b) Shall have a test weight per bushel of at least fifty-three pounds;

(c) May contain not more than 4 per centum of rye;

(d) May contain not more than 7 per centum of damaged kernels, which may include not more than five-tenths of 1 per centum of heat-damaged kernels; and

(e) May contain not more than 10 per centum of wheat other than red spring, which 10 per centum may include not more than 4 per centum of common white and club wheat, either singly or in any combination.

Grade four:

(a) The wheat in this grade shall be cool and sweet;

(b) Shall have a test weight per bushel of at least fifty pounds;

(c) May contain not more than 5 per centum of rye;

(d) May contain not more than 10 per centum of wheat other than red spring, which 10 per centum may include not more than 5 per centum common white and club wheat, either singly or in any combination; and

(e) May contain not more than 10 per centum of damaged kernels, which 10 per centum may include not more than 1 per centum of heat-damaged kernels.

Sample: Shall be wheat which does not come within the requirements of any of the grades from number one to number four, inclusive, or which has any commercially objectionable for-

eign odor except of smut, garlic, or wild onions, or is very sour, or is heating, hot, infested with live weevils or other insects injurious to stored grain or is otherwise of distinctly low quality, or contains small, inseparable stones or cinders.

Class II. Durum Wheat.

Grades for durum wheat: The subclasses amber durum, durum, and red durum shall be divided into four grades for each subclass, as follows:

Grade one:

(a) The wheat in grade one shall be cool and sweet;

(b) Shall have a test weight per bushel of at least sixty pounds;

(c) May contain not more than 2 per centum of rye;

(d) May contain not more than 2 per centum of damaged kernels, which may include not more than one-tenth of 1 per centum of heat-damaged kernels;

(e) May contain not more than 5 per centum of wheat other than durum, which 5 per centum may include not more than 2½ per centum of common white and club wheat, either singly or in any combination; and

(f) May contain not more than 5 per centum of durum kernels of distinctively red color in either number one amber durum or number one durum.

Grade two:

(a) The wheat in grade two shall be cool and sweet;

(b) Shall have a test weight per bushel of at least fifty-seven pounds;

(c) May contain not more than 3 per centum of rye;

(d) May contain not more than 4 per centum of damaged kernels, which may include not more than two-tenths of 1 per centum of heat-damaged kernels; and

(e) May contain not more than 10 per centum of wheat other than durum, which 10 per centum may include not more than 3 per centum of common white and club wheat, either singly or in any combination.

Grade three:

(a) The wheat in grade three shall be cool and sweet;

(b) Shall have a test weight per bushel of at least fifty-four pounds;

(c) May contain not more than 4 per centum of rye;

(d) May contain not more than 7 per centum of damaged kernels, which may include not more than five-tenths of 1 per centum of heat-damaged kernels; and

(e) May contain not more than 10 per centum of wheat other than durum, which 10 per centum may include not more than 4 per centum of common white and club wheat, either singly or in any combination.

Grade four:

(a) The wheat in grade four shall be cool and sweet;

(b) Shall have a test weight per bushel of at least fifty-one pounds;

(c) May contain not more than 5 per centum of rye;

(d) May contain not more than 10 per centum of damaged kernels, which may include not more than 1 per centum of heat-damaged kernels; and

(e) May contain not more than 10 per centum of wheat other than durum, which 10 per centum may include not more than 5 per centum of common white and club, wheat, either singly or in any combination.

Sample: Shall be wheat of the subclass amber durum, durum, or mixed wheat, respectively, which does not come within the requirements of any of the grades from number one to number four, inclusive, or which has any commercially objectionable foreign odor except of smut, garlic, or wild onions, or is very sour, or is heating, hot, infested with live weevils or other insects injurious to stored grain, or is otherwise of distinctly low quality, or contains small, inseparable stones or cinders.

That the general rules and definitions applicable to all classes of wheat heretofore promulgated by the Secretary of Agriculture and not inconsistent with the provisions of this Act shall remain in force until changed or modified by the Secretary of Agriculture, pursuant to the United States Grain Standards Act.

Sec. 2. That the standards and grades prescribed in this Act shall take effect thirty days after its passage, and the Secretary shall as soon as may give notice thereof.

The Secretary of Agriculture shall have authority, after a hearing, to alter or modify from time to time said standards or grades whenever the interests of agriculture may, in his opinion, so require: Provided, however, That the grades and standards prescribed in this Act shall not be altered or modified so as to take effect before August 1, 1922.

In promulgating the alterations or modifications of the standards and grades here prescribed, the Secretary shall specify the date or dates when the same shall become effective, and shall give public notice not less than ninety days in advance of such date or dates by such means as he may deem proper.

The bill throws out the moisture test as a grade determining factor, leaving the inspectors to insert the percentage of moisture when in excess of 14½ per cent for the information of the buyer.

The test weight of No. 2 hard red spring and durum is reduced one pound to 56 and 57, the lower grades being reduced 2 and 3 lbs.

Dockage shall not affect the grade, and all but rye foreign material shall be classed as dockage.

Grade No. 5 is abolished.

The argument used is exemplified by the statement of W. C. Palmer, a state grain inspector for North Dakota, at the hearings, that

The federal grades place wheat of widely different values within the same grade. If the grade should do anything, it should classify wheat on the basis of its value. But the fact that this wheat is graded No. 1 out of the country elevator is no indication of what it is going to bring down on the Minneapolis market. It may vary anywhere up to 30 cents.

The grades do not mean anything down on the Minneapolis market. They do mean everything out on the farmer's market, because he is paid on the basis of whatever the wheat grades, and then when they go down on the terminal market they do not pay any attention. They look at the wheat in this pan and they see the grade. It may be No. 3 wheat, and if they want it they may pay more for it than they pay for No. 1 wheat.

If the millers in Minneapolis can not buy by grade, then what is the miller at a distance on the importer to do? He is limited to buying No. 1 and No. 2 wheat, and the rest he will have to let alone. Now, what effect will that have on the price of Nos. 3, 4 and 5 wheat? And the price paid the farmer is on the basis of what the lowest in each of these grades will bring. Then a system that allows the grades to be degraded so that some of them are not fit for buying, what price can the farmer ex-

pect? And what justification is there for grades that make it possible for a large part of our wheat to be put in such condition that it can not be bought on grade, or any other way than sample, and that penalizes the grower?

Representative Haugen's report to the House made it appear that there was no opposition to the changes. The representative of the Buro of Markets was non-committal, merely stating the grades were adopted thus because the Department of Agriculture found them in effect; and Sec'y of Agriculture Wallace saying "I would welcome Congress taking it out of my hands."

Senate Would Investigate Activities of G. D. N. A.

A resolution adopted by the Senate on Aug. 22 provides for the investigation of the activities of the Grain Dealers National Ass'n and other trade organizations alleged to be engaged in combating legislation for the relief of agriculture.

This resolution follows closely the announcement that Attorney General Dougherty and Sec'y of Commerce Hoover were at work on a "Statement of Principles" which would help business organizations avoid conflicting with federal laws.

C. A. McCardle Dead.

Clyde A. McCardle of Indianapolis, died Aug. 18 at a hospital in Detroit, Mich., where he had gone for treatment six weeks ago. He became ill several months ago and in April was forced to give up his work.

Born on a farm in Fountain County 41 years ago Mr. McCardle passed his boyhood at Attica and New Richmond, Ind., and three years at Wabash College and Indiana University. Returning from the University he was associated for a time with his father, John W. McCardle, in the grain business at New Richmond, removing in 1905 to Indianapolis. After spending five years in Georgia he returned to Indiana and engaged in the grain business at Terhune.

He again became a resident of Indianapolis, joining his father, and Bert K. Black in the grain commission business at the McCardle-Black Co., of which he was sec'y-treas. at the time of his death. Interment was at Crown Hill Cemetery, Aug. 2. He is survived by Mrs. McCardle and two adopted children. A portrait of Mr. McCardle is reproduced in the engraving herewith.



Clyde A. McCardle, Indianapolis, Ind., Deceased.

Drying Grain at Country Elevators.

The drying of grain containing so much moisture it will not grade satisfactorily or so much as to make it unsafe for storage is no longer an experimental process of doubtful value. It is an accepted practice and part of the every-day service of the grain dealer whose plant is equipped with modern drying apparatus.

There remains some progress to be made along this line to bring to the business of grain handling the full benefits of engineering progress in the development of more efficient drying equipment. As the equipment is distributed at the present time, practically every terminal and transfer elevator is prepared to dry grain while the percentage of country elevators having the necessary machinery is low. It is true that the total number of country elevators with driers is large, but the majority of these houses are not so equipped. It appears that the country elevator operator has felt he cannot profitably invest the money required to install a drier, and he has preferred to take his chances without it, relying upon his ability to obtain service at some plant in his terminal market when drying offered the only method of saving grain that would otherwise be lost.

In normal years, when grains mature properly and are harvested and placed in storage without becoming loaded with an excessive amount of moisture, the average country elevator has little need for a drier. But in years which occur altogether too frequently, when early frosts catch the corn before it matures or heavy rains at harvest and threshing time cause small grain to carry a high moisture content, a drier offers the only means of avoiding loss on grain going out of condition. It is in these abnormal years that the drier yields a profitable return and justifies the expenditure that was made when it was installed. And, it must be borne in mind that unless the drier is installed *before* it is needed it may be impossible to get it in time to realize the full benefits to be derived from its use.

A drier that will handle a carload a day is approximately the size best suited for the country elevator. It will not cost much to install a drier of this size. The exact amount will depend upon local conditions somewhat and on the arrangement of the elevator where the plant is to be placed.

To analyze the advantages that will be derived from the operation of a drier an example of a carload of corn, 60,000 lbs., grading No. 5 because of the presence of 22½% moisture when it comes from the farmer's wagon, may be used. This corn, grading No. 5, cannot be applied on contract. It must be consigned to the terminal market and sold on its merits as No. 5 corn, or shipped to a drier to be handled unless the country elevator has such a plant. If it is shipped and dried, the chances are it will not apply on contract for technical reasons, so the only available outlet is a sale on the consignment tables. Here, the No. 5 corn will bring several cents less than No. 3, and while the exact discount cannot be determined in a hypothetical case it is certain it would hardly be less than 5c per bushel and it might be much more.

If the corn is shipped in its original condition it is necessary to pay freight at the full rate on the excess moisture, and this will amount to a considerable sum in itself.

In the example given, it would be necessary to dry the corn to 17½% at the country elevator to make it grade No. 3. For the sake of safety it would be better to dry it to 17%, taking out 5½% of the original moisture. Five and one-half per cent of 60,000 lbs. is 3,300 lbs., which would be the approximate loss in the drying process. This is a little over 58 bus. lost, its value at 35c being \$20.65. This price is taken because it is about what the grain will cost from the farmer at present values. The value of this lost weight has a

material effect on the profit or loss result of the drying operation, but if the value is materially higher it is usually accompanied by a corresponding increase in the difference in value between the higher and lower grades, so that this discount helps to offset the greater cost of the weight dried out of the grain.

If the rate of freight from the station to its principal terminal is 21c per cwt. the freight and tax on the moisture removed by drying would be \$7.14.

After drying, there would be 56,700 lbs. of No. 3 corn remaining, or 1,012.28 bus. At 5c per bu., the increase in value of No. 3 over No. 5, this would represent \$50.62.

The actual cost of drying the grain should not exceed 1c per bushel, and it will probably be less if the plant is operated properly and with a view to economy. On the weight of corn used in this example, this cost would be \$10.72.

The result of the operation would thus stand about as follows:

Gain in market value.....	\$50.62
Gain thru freight saved.....	7.14
Total gain.....	\$57.76
Loss in weight.....	\$20.65
Cost of drying.....	10.72
Total loss.....	\$31.37
Net gain.....	\$26.39

In certain cases the results will not be so favorable as this, while in others the gain will be materially greater, so that in the end the use of a drier at country elevators should result in an increased net profit over that realized when the grain is shipped with the water still in it. There is also the value of freedom from the danger that shipments will go out of condition in transit, due to the high moisture content, and thus entail a much greater loss than the one reflected in the mere difference of price in the first place.

Whether a drier can profitably be installed in view of the investment that will at times be useless remains for each dealer to figure in the light of his own circumstances. Some years there will be little or no drying to do, and the plant will return no income, but at any rate it will not then add to operating costs. If the elevator in the example used were to handle 50 cars of corn, on each of which an average of \$25 increased net profit could be realized thru drying, it can easily be seen that this alone would pay much of the cost of drier.

The drier is as serviceable in drying any other grain as with corn. Corn, however, more frequently carries high moisture than does small grain and generally the corn handling elevator will have more need for a drier.

The country elevator seems to be the logical place for the drier, and undoubtedly these plants at country points would save dealers and farmers immense sums of money in the wet years.

THE TOTAL WHEAT production in 16 large wheat producing countries is approximately 1,953,000,000 bus. this year, or about 7,000,000 more than last year, according to the Department of Agriculture.

Adulteration and Misbranding.

Under the Food and Drugs Act the following judgments were recently rendered in the U. S. District Courts for the Buro of Chemistry:

The Globe Elevator Co., Buffalo, N. Y., shipped feed into Massachusetts labeled "protein 13½ to 15% and fat 3%." Analysis showed 11.25% protein and 2.23% fat. Fined \$150.

The Union Seed & Fertilizer Co., England, Ark. labeled cottonseed meal "Ammonia 7%, protein 36%, and crude fiber 12%" whereas an analysis showed 6.54% ammonia, 33.6% protein and 16.6% crude fiber. Pleaded guilty. Fined \$50 and costs.

National Hay Convention at Chicago

A gathering of business men, proceeding in a business like manner to solve the problems of their trade is a fitting description of the 28th annual convention of the National Hay Ass'n, held at Chicago Aug. 23 and 24. The keynote of the convention was conservatism, and that was reflected in every address, every paper and every report.

In welcoming the dealers to the city, Joseph P. Griffin, pres. of the Chicago Board of Trade, showed the similarity of interest of the grain dealer and hay man and asserted that if all business had been subjected to the same bitter attacks as the grain trade in the past few months the whole industrial structure would have collapsed.

Declaring that less government in business and a curb on pernicious agitators alone will bring the return of real prosperity, he asked the questions: "Is the fight on the grain trade a prelude to similar unjust attacks on other trade? Will the proposed grain pool be followed by similar schemes for other lines of business?"

That legislation is organized intimidation by minorities, as it is now functioning, was charged by Mr. Griffin, who stated that these minorities use their voting strength as organized groups to obtain the things they want regardless of the effect on business as a whole.

"This," he said, "is no time to harass business, which is a very sick patient. It is no time for tinkering blacksmiths to interfere with the delicate machinery of trade."

Warren H. Dean, of Auburn, N. Y., who responded to the address of welcome, made the declaration that, "Business men must stand firmly together on the fundamentals of business—not for self, but for the good of all."

In reporting for the board of directors, R. M. White, of Duluth, Minn., pres. of the Ass'n, dealt at length with the incidents of the recent past, which he preferred to style a "period of hard sledding" rather than a "period of readjustment" in the belief that the former term is understood without other explanation. He gave it as his opinion that the best that government can do it to promote liberty and equality of action in business and that it cannot of itself bring prosperity.

On the subject of transportation, D. J. Sims, Auburn, N. Y., traffic mgr. for the Ass'n, said the carriers are not earning the return allowed them by the Transportation Act, even under present rates and that the Interstate Commerce Com'n is forbidden by law to reduce the rates below a point that will give a fair and reasonable return. "The Ass'n," he said, "recognizes that whether fair or unfair the present rates are more than the traffic will bear." He asked the members of the Ass'n to bear in mind the fact that specific information is required when the I. C. C. is asked to reduce rates, and that mere knocks against the carriers are of no avail.

Some instances given by dealers present tended to show that in certain cases freight charges on hay have equalled and even exceeded by double the value of the hay at point of origin.

Government Control and Taxes.

A forceful argument for more business in government and less government in business was delivered by H. A. Wheeler, vice-president of the Union Trust Co., Chicago, when he spoke to the dealers on the subject "Government in Business Insures High Taxes." Mr. Wheeler pointed out that the three forces, Taxation, Tariff Imposts and the Measure of Government Aid, Regulation or Control over Business affect all countries, altho the

effects may differ somewhat in proportion to the errors made in administering the forces. He showed that taxes are absolutely necessary to the protection of life and property rights and that a perfect system would be one under which we know the scheme is helpful to our general affairs, that the money is needed and that it is wisely and decently expended.

None, he said, are exempt from taxation as the only exemption is in that group of securities which have by law been made exempt to the cost of all of us. These securities he charged with making it possible for some individuals to escape a fair share of taxation while making it necessary for others to assume more than their just portion. He summed up his discussion of this phase of his subject by denouncing it as rank folly to exempt large blocks of securities as we have done in this country.

That a democratic government tends gradually to become more complex from the time of its inception was one statement made by Mr. Wheeler. In amplification of this generality he traced some of the activities of our own nation from its beginning to the present day, showing conclusively that as new men have come into office they have added new activities and sought new powers until by the time of our entry into the great war this had moved to a point where it was almost natural for us to confer on governmental and quasi governmental agencies powers and authorities that men had never possessed before in any democracy.

"Because of the power that some men had and loved to exercise under war control," he said, "there exists a strong desire to continue into peace times similar control over business and activities that should be conducted wholly by individual initiative. So they would like to control the live stock and packing industries, fuel, transportation, and finance. The same America which came up to its present position because of the efficiency of its business life under private initiative will sink back because of government control if it is persisted in."

The speaker showed how the cost of railroad control, totaling hundreds of millions, must be paid in double taxation—once out of taxes levied for the expense of the federal government and once more in the lack of efficiency and high rates suffered. He carried the same thought to the operation of the federal reserve bank system and compared the cost of its influence on business to an indirect tax which all help to pay but do not recognize.

The general rule was laid down that every time government enters into the regulation, control or operation of business that is conducted by private enterprise the cost of government, and taxes, are increased.

That a certain amount of properly conceived and applied regulation may be necessary was admitted; but he said that if unlimited control goes on it will reach not only packers and fuel dealers and bankers but it is just as readily applicable to all forms of business endeavor.

"Politics," he declared, "never will simplify government. The public, unorganized, will not do it. Organized business must sweep away the fancies in the public mind and it must demand that men be permitted to do what they know best how to do in order that government itself may be made more simple. Whatever is not good for the public cannot be good for business."

In closing, he made the plea that all organizations of business men study the fundamentals of the situation and perform their duty in guiding correctly the influences that now show a tendency to wrong thinking.

The Transportation Situation.

An address on "Railroad Responsibilities" was delivered Wednesday morning by Wm. H. Finley of Chicago, pres. C. & N. W. Ry. Co. Starting with the statement that transportation means more to the human race than anything else and that the matter of transportation or its lack determines whether a nation is progressive or backward, he outlined some of the steps in the development of American railroads and their assistance in the pioneering of our country. He said that the rate differentials which were disturbed by horizontal increases must be readjusted and will be readjusted, but that it will require time.

In referring to the subject of motor truck transportation, he said that the roads which are provided for the use of trucks are built and maintained by taxes; and that he expects time to prove that transportation cannot be produced so cheaply by trucks as some now think.

The subject of the development of our waterways also received attention from Mr. Finley, and he said: "If the railroads were built and maintained out of the public treasury I could haul your hay and grain cheaper than any boat in the world."

He made it clear, however, that he does not oppose waterways, but expressed the opinion that they should only be developed as the increasing density of population makes their use advisable. His message was largely one designed to arouse the public to a greater interest in a proper study of the railroad's problems.

The cause of the horse versus the tractor and motor truck was presented by Wayne Dinsmore, Chicago, sec'y of the Horse Publicity Ass'n of America. Mr. Dinsmore reviewed the work of his organization and endeavored to impress upon the dealers the necessity for continued activity to show the farmer his advantage in using horses for his work.

New Officers.

Officers elected to serve the Ass'n for the ensuing year are as follows:

Pres., W. I. Blies, Saginaw, Mich.; 1st vice-pres., F. A. Coles, Middletown, Conn.; 2nd vice-pres., Albert E. Nicholls, Los Angeles, Cal. Directors: Robt. M. White, Duluth, Minn.; Rudolph Raabe, Fort Jennings, Ohio; Jos. T. J. Walton, Pittsburgh, Pa.; Geo. F. Brown, Charleston, W. Va.

On Tuesday evening the dealers and their ladies were the guests of the Chicago Board of Trade at a banquet at the Drake Hotel. Joseph P. Griffin acted as toastmaster, and the speakers included J. R. Pickell, Walter Lichtenstein, executive sec'y First National Bank of Chicago; P. E. Goodrich, Winchester, Ind., and pres.-elect W. I. Biles. Following the dinner the guests enjoyed themselves at dancing.

Selecting Seed Corn.

Seed corn should be gathered between Sept. 5 and Sept. 15 advises the University of Minnesota in its Aug. 15 bulletin.

"Corn germinates best and makes the strongest growth when it is well dried out before heavy freezing occurs. It should be allowed to mature on the stalk, but as soon as matured it should be gathered and placed in a shed, outbuilding, or in the dwelling house.

"It is important that there be free circulation of air and that the corn be spread out thinly and frequently turned over. It will cure best on a good drying rack where no two ears touch each other. During a wet cold fall it is often advisable to use a stove to more quickly dry the corn and to stimulate circulation of air.

"The seed ears should be selected before killing frost from the most vigorous stalks in perfect stand hills."

Chokes.

By CAL.

ELEVATOR FIRES, like certain other unwelcome visitors, come mostly at night.

THE DUMP TRIP lever that can be operated by a frightened horse belongs in an elevator that burned in 1897.

THE BEST method of cooping a car with a broken door post is to tell the engine crew to kick it on past and spot another car at the loading spout.

THE LADDER to the cupola should be within reach of a man on the manlift car. He may sometime have to transfer from the automatic to the hand and foot power mode of transportation from top to bottom.

A SMALL bellows like the printer's devil uses for blowing the dust out of type cases is a handy device to have near the automatic scale and motors for removing dust. Ask the printer man up town to show you one of them.

ADVERTISING is not exclusively for those who have some commodity to sell. The country grain dealer can advertise his service to his farmer patrons to their mutual profit and advantage.

WHEN the farmer's granary burns and he is able to salvage some unburned but smoke damaged grain he may offer it to the elevator. It is permissible to buy it—at a discount—but don't mix it with good grain. A wagon load of smoked grain can scent a carload so that all of it will have to suffer a heavy discount at the terminal.

A LINE COMPANY never made a better investment than that of bringing all its station agents together for an experience meeting at least once a year.

A BELT CLAMP should be available for immediate use at every elevator. Never can tell when it may be needed, or how many loads may be waiting at the time.

A FRICTION CLUTCH needs no oil on its contact surfaces, but those surfaces should be kept clean. Scratches on the face of the revolving drum or in the groove mean that particles of foreign material have cut the smooth face.

BUMPING the tester kettle or beam on door jams, wagon boxes and other miscellaneous stationary objects is a discourteous way to treat a good friend. Play fair with the tester so it can play fair with you and your customers.

ENOUGH ENERGY is wasted at the average country elevator each year trying to move cars to pay for several good car movers. In a few years it would cover the cost of a power car puller. But the interest will never be earned on the investment required for a car puller at the elevator which handles only an average of a car a week.

THE TIME is coming when low temperatures will thicken the water of the engine cooling system. If it gets thick enough a cracked cylinder jacket may result. Today is the day to install a draining system that will take the water out of the jacket while it is still thin or to substitute a solution of calcium chloride which will not freeze.

SAVING for a rainy day is generally interpreted by the elevator man to mean the delaying of odd jobs until rain stops deliveries from the farms. The first job for every slack day should be a general cleaning up and examining of the entire plant.

If the average grain dealer knew as much about farming as the ordinary radical agitator of farmers thinks he knows about grain handling the average grain dealer could make seven blades of grass grow where none has grown before. It's queer the agitator is never worried by his failure every time he gets an opportunity to apply his theories.

TOOLS that are scattered in different parts of the elevator are never to be found when small jobs require them. The resultant failure to make repairs while defects are small brings big jobs later. Every elevator needs a closed place for tools, which can be securely locked. Then the operator has some chance of finding the tool needed when he wants it.

How LONG since the boot pulley was oiled? Of all the machines or mechanical contrivances in the elevator, the boot pulley gets the least care. And it is a rather important member of the family, too. Unless the elevator man is one of those who are really efficient, don't be surprised on going to the pit to find the boot pulley in a disgraceful condition.

If ALL persons not authorized to transact business by telephone will refuse flatly to talk when central announces a long distance call the man at the other end will be saved the expense of an unprofitable connection. When there is an instrument and line exclusively for long distance messages, let it ring when the boss is out. The operator can then report nothing but "D. A."

Farmers Losing Interest in U. S. Grain Growers?

During the recent Missouri State Fair held at Sedalia, C. H. Gustafson, pres. of the U. S. Grain Growers, Inc. explained to the farmers the object of the organization.

A St. Louis, Mo. commission merchant who was in Sedalia and attended the fair the day Pres. Gustafson spoke, has the following to say about it:

"I happened to be there and was much surprised at the attendance which was not over one hundred. I was also surprised at the lack of interest shown. It was evident that this talk made no impression on those who heard it."

30,000 Bu. Concrete Elevator at Ypsilanti, Mich.

Gradually the trend in elevator construction, both at country points and in terminal markets, is toward the adoption of more permanent forms and improved designs.

A recent addition to better grain handling equipment of the state of Michigan is shown in the photograph reproduced herewith. It is the new elevator, feed mill, warehouse and office of the Ypsilanti Farm Bureau Ass'n, of Ypsilanti.

The plant, which was designed and built by Ballinger & McAllister, has storage capacity in the elevator for 30,000 bus. of grain and there is 5,000 square feet of warehouse floor space. The elevator is of reinforced concrete and the feed mill building, warehouse and office are of reinforced concrete with brick facing.

The elevator has two legs, a corn sheller, corn crusher, cleaner, Richardson automatic scale, Fairbanks 10-ton truck scale, and power for operating the entire plant is furnished by F-M electric motors. The feed grinding equipment is a Scientific Ball Bearing attrition mill.



Concrete Elevator and Concrete and Brick Warehouse and Office at Ypsilanti, Mich.

Seeds

LISTOWEL, ONT.—J. W. Sangster contemplates rebuilding his seed warehouse and mill.

NEW ALBANY, IND.—The Emery Scott Seed Co. has increased its capital stock from \$10,000 to \$50,000.

PORTER, MINN.—The Farmers Grain & Seed Co. has been incorporated with a capital stock of \$50,000 by F. C. Metcalf.

CRAWFORDSVILLE, IND.—The seed business of W. R. Paxson has been discontinued since the death of Mr. Paxson recently.

COYOTE, CALIF.—A warehouse belonging to the California Seed Growers Ass'n was completely destroyed by fire Aug. 10.

THE REQUE VALLEY SEED FARMS, Inc., has been incorporated with a capital stock of \$50,000 under the laws of Delaware.

LOUISVILLE, KY.—The Powell Seed Co. is the new name of the Card & Powell Seed Co. now in complete charge of Paul Powell.

LOUISVILLE, KY.—The Ross Seed Co. has been incorporated with a capital stock of \$250,000 by George M. G. and Alfred Ches-chier.

GREENE, N. Y.—Charles W. Gray, pres. of a local bank, has been chosen pres. of the Page Seed Co. to succeed E. L. Page, deceased.

LINDSBORG, KAN.—On account of the shortage of the alfalfa crop this year we feel that our orders are much greater than the crop.—The Lindsborg Seed Co.

ROCK LAKE, N. D.—There is no such thing around here as rust proof durum. All durum wheat was hit alike by the rust.—Armourdale Equity Elevator Co.

ROCKY FORD, COLO.—The Blotz-Henneman Seed Co. has been incorporated with a capital stock of \$50,000 with A. A. Henneman as pres., and A. Blotz, sec'y treas.

THE PROPOSED Connecticut seed law is opposed by many of the state's seedsmen. Already several of the seed dealers have sent letters of protest to their legislators.

CLINTON, S. C.—The Rheney Seed Co. of Charleston, S. C., contemplates establishing its headquarters in this city. The company does a large seed business all thru the South.

SIDNEY, NEB.—G. A. Chamber of Lincoln, Neb. has purchased a part interest in the seed business of W. G. Sherman of this city and hereafter will make Sidney his headquarters.

OTTAWA, ONT.—The Dominion Department of agriculture is now maintaining seed laboratories at Ottawa, Winnipeg and Calgary. During the past year these three laboratories made over 28,000 seed tests.

CHICAGO, ILL.—J. Oliver Johnson has registered as his trade marks No. 148,599 to 148,601 inclusive for use on grass seeds the following phrases: "Forest Preserve," "Red—White—Blue" and "Oak Park."

SHERMAN, TEX.—The Sherman Grain & Seed Co., recently organized here will conduct a field and garden seed business as well as engage in the feed and grain business. The manager of the company, J. A. Hughes, has been in the grain business for several years.

WASHINGTON, D. C.—The condition of the various seed crops on Aug. 1 or at harvest was reported by the Bureau of Markets and Crop Estimates as follows: (100% is average) Millet 106.9%; flaxseed 92.5%; beans 98.3%; peas 97.1%; grain sorghums 113% and buckwheat 98.4%. The total production for the season compared with the production in 1920 is estimated at grain sorghums: 90.3%; broom corn, 96.5%; beans, 96.7%; flaxseed, 81.1%, and buckwheat, 93.9%.

CHAUMONT, N. Y.—The Rogers Bros. Seed Co. has increased its capital stock from \$375,000 to \$500,000.

CHICAGO, ILL., Aug. 17.—In Kansas kafir corn, milo and feterita are in good condition.—S. H. Johnson, v. p. C.R.I. & P.

CHICAGO, ILL.—Everette R. Peacock Co.'s new warehouse is rapidly nearing completion. It will be used to care for its rapidly increasing business in onions. The company also handles field and grass seeds on a large scale.

JOHN MATHER LUTON, at one time pres. of the American Seed Trade Ass'n and long interested in the growing and development of various garden seeds passed away July 30 at his home in Mattituck, N. Y. He is survived by a widow and two grown children.

CHICAGO, ILL.—At the Pageant of Progress which just closed at Chicago Mr. Wilson, Illinois Seed Analyst and Mrs. Wilson demonstrated to the visitors the methods of seed testing and grading on Illinois. The same exhibit was maintained that was taken to the seed conventions this spring.

TOLEDO, O.—Timothy continues under pressure, ruling weak and lower. Bulges do not hold. Increased hedging sales against the new crop. Good investment demand and short covering on the scale down. Stocks here not burdensome. The future trend will depend largely upon the fall demand.—Southworth & Co.

CANADA'S EXPORT SEED trade has been on the increase due largely to the assistance given by the Dominion Department of Trade and Commerce and the Dominion Department of Agriculture. The Commerce Commissioners listed the probable foreign purchasers of Canadian seed, communicated with them and passed out what information they received.—B.

TOLEDO, O.—Clover seed has shown little change. Trade has been larger than in some time. Around thirteen dollars the market has met support. Local crowd are mostly bearish. Crop outlook has improved. Is clover too high compared with corn, oats and wheat? Some new mammoth clover has been received here. When the crop moves, will the financial load be too heavy? Shorts do not appear to worry and feel the farmer will be a free seller.—C. A. King & Co.

SALT LAKE CITY, UTAH.—The Western Seed Growers Marketing Co. has new plans for the marketing of seed grown in Utah and Idaho this year. It is reported that the seed marketing company and the western branch of Northrup, King & Co. have reached an agreement whereby Northrup, King & Co. will act as the marketing agency of the growers for much of their clover and alfalfa seed. B. F. Sheehan, seed commissioner of Idaho, has resigned his petition to go with the western marketing company.

From the Seed Trade.

WENDELL, IDA.—There is a large acreage of "Hubam" or annual white sweet clover planted in this district that is under contract to an agricultural ass'n in Illinois. The present outlook for this seed is exceptionally bright. Owing to our long growing season of even dry temperature and with plenty of irrigation water at the present time, this is an ideal seed producing area. In 1920 there was half a million pounds of exceptionally bright colored and plump alfalfa seed grown in this district. Owing to the prevalence of dodder the seed did not grade as it should. This season the growers have made every effort to eradicate the dodder and while the crop will probably not be as large the quality and purity will be greatly improved. There has also been about 50 cars of "Declor" or "Dick Low" wheat shipped from Wendell during the past year and there is probably a 25% increase in the crop this year.—The Wendell Milling & Elevator Co.

Trade Rules of Grass Seed Dealers.

Effective Aug. 1, 1921, the Wholesale Grass Seed Dealers Ass'n, at its recent St. Louis meeting, adopted the following trade rules.

1. **Purchase or Sale Contract**—A contract of sale, except as herein otherwise provided, shall contain the following provisions: (a) Amount of seed sold. (b) Kind of seed. (c) Purchase or sale price. (d) Any conditions or terms of sale, such as time, and place of delivery, time of payment, warranties, etc., which are not included in or which are contrary to provisions contained in these rules.

2. **Confirmation of Sale**—Buyer and seller shall, on the day of making a contract of sale, or as soon thereafter as possible, mail to each other a confirmation of said contract of sale. Any material variation from the original agreement disclosed by this confirmation shall be immediately communicated by telegraph or telephone to the other party and correction effected. Manifest or immaterial errors may be corrected by mail. Failure to confirm by mail shall not in itself invalidate original contract of sale.

3. **Terms of Payment**—Terms of payment shall be sight draft, B/L attached, payable on presentation, unless otherwise specified at time of contract. Seller shall pay all collection charges on drafts, if any.

4. **Definitions**—(1) Where seeds are quoted or sold "Sacks Included" it means sacks are weighed in gross for net, no extra charge being made for sacks. Where goods are quoted to be sold "Bags Extra" tare is allowed and bags shall be charged for at market value at the time of the contract of sale.

(2) "F. A. Q." means fair average quality of the crop and does not permit scalping off the high grade and delivery of the remainder as "F. A. Q."

(3) The terms "Freight Paid" or "Freight Allowed" means that the seller assumes payment of freight to destination simply as an adjustment of the purchase price. The beneficial interest in the goods, carrying with it all risks and responsibilities of loss, damage or delays in transit, shall be in the buyer from the time when the goods are delivered to the transportation carrier.

(4) "Delivered" in connection with the sale of seeds, means that the goods are offered at the sale price delivered on board cars, boat, steamer or on railroad platform, boat wharf, steamer dock, etc., at point of destination ready for delivery by the transportation company.

(5) The term "Dodder Free" shall allow a tolerance of ten (10) dodder seed per pound.

(6) The term "Absolutely Dodder Free" shall mean no dodder found in a 150-gram test.

(7) War tax on freight shall be considered part of freight charges.

5. **Terms of Shipment and Deliveries**—(1) Shipment is defined as the initial delivery of seed by the seller to a transportation company for transporting to the buyer or his order.

(2) Terms of shipment shall be "Immediate"—shipment within three days, and "Prompt"—within ten days from date of sale. The time mentioned above means working days, exclusive of Sundays and holidays. In absence of any provision in the contract of sale as to time of shipment "Immediate" shipment shall be presumed.

(3) A minimum car shall be the weight required by the transportation company to obtain the car load rate of freight. In cases where no specified weight is required by the transportation company to obtain the car load rate of freight, 30,000 pounds shall constitute a minimum car.

(4) In case of sales of seeds f. o. b. shipping point, the seller shall not be subject to any liability whatsoever, after delivery to transportation company for or on account of freight or any other charges, loss, damage or delays. Unless otherwise specified in the contract of sale, all sales are presumed to be made f. o. b. point of shipment.

(5) When seeds are sold "Delivered" the seller shall bear the risks and responsibilities for loss, damage, delays, demurrage or other charges, but said obligation shall cease when the seeds arrive at destination, and are reported to the buyer by the transportation company to be ready for delivery.

(6) These Seed Trade Rules are subject to the following limitations:

(a) The buyer and the seller be relieved from their several obligations under the contract of sale for non-shipment, or for non-delivery in case of sale of seeds "Delivered" as defined by these rules, when the failure of seller to ship or to deliver as aforesaid is occasioned by fire, flood, strikes, embargoes, car shortages, restraint by process of law or war, or by any other causes beyond the seller's control.

(b) Whenever the buyer is unable to obtain possession of the B/L through no fault or failure on his part, the seller shall pay the demurrage charge incurred because of the inability of the buyer to obtain the B/L as aforesaid.

6. **Sale by Samples**—All samples of seed offered for sale shall represent the identical lot unless otherwise specified. In event of a dispute between buyer and seller as to char-

acter, quality or condition of a delivery of seed sold by sample, the buyer may demand arbitration under the rule of the Ass'n of the questions under dispute. The seller shall have five days after such demand for arbitration is made, either to make adjustment to the satisfaction of the buyer or to accept the arbitration and award in settlement of all claims in dispute, including any cost incident to holding or handling such shipment during the period of dispute and arbitration.

7. Analysis—Purity and germination tests shall be made by competent analysts, either in the employ of the party furnishing tests or by competent seed testing stations. When germination tests have not been made within ninety (90) days from date when furnished to a buyer, or when accompanying an offer of sale, the time of making tests shall accompany the test for germination.

All tests shall be made in manner prescribed and with apparatus recommended by this Ass'n of Official Seed Analysts of North America.

When variations occur in tests of buyer and seller, resulting in dispute of quality of seed delivered, some disinterested party preferably a member of the Ass'n. shall draw an average sample from each and every bag of the delivery in the usual manner, and after sealing forward to the United States Department of Agriculture at Washington, D. C.

The test furnished by the Department shall be considered the correct and final test of the seed in dispute.

8. Arbitration—Dispute arising in sales or contracts of sale, which cannot be amicably adjusted by the contracting parties, shall be referred to the Arbitration Com'te of the Wholesale Grass Seed Dealers Ass'n for arbitration and award in accordance with the rules and procedure of said Arbitration Com'te.

A Minnesota Seed House.

In the clover country of Minnesota H. I. Davey has established himself as the Davey Seed Co., in a recently completed office and warehouse which he has dubbed "The Pine River Seed House" at Pine River, Minn., and of which one end is shown in the engraving herewith.

The building is 44x106 ft., has a full basement 10 ft. deep and three full floors, with bins above the third floor. The outside is covered with a white and green marble stucco and the roof with a composition slate shingle, making it practically fireproof as far as sparks from the outside are concerned. The first floor is of cement, and the two floors above are of best grade flooring treated with hot oil. All of the cleaning machinery is installed on the second floor.

Mr. Davey handles clover and field seeds and poultry feed, and has equipped the house with all new machinery. In the office he has a large safe, adding machine and other up-to-date equipment.

Predicts Higher Price for Red Clover.

In reviewing the seed trade in France, Michel Cherot, a seed dealer of Paris, writes as follows:

"The coming crop of red clover will surely not be more than half of the large crop last year. Practically all the stocks of the last crop are still in the hands of the farmers, who do not feel ready to sell at the prevailing low prices. We may expect some very high prices for red clover."

"The yield of crimson clover has turned out much lower than expected. The stocks of yearling seed are still large, but the prices have followed the advance of the new crop. It is feared that the entire crop of new seed will be exhausted early."

Of the shortage of white clover and alsike clover in France, Mr. Cherot says: "It seems almost certain that we will have to import these seeds this year."

Timothy Seed Crop.

Conditions affecting the trade in timothy seed were summarized by the Todolo Produce Exchange as follows: Because of drought during the last half of June and the first week or two of July, many timothy fields or portions of timothy fields, intended for the production of seed were cut for hay in order to provide for a sufficient tonnage of hay.

The inactivity of the timothy seed market since harvest time a year ago, accompanied by sharp declines in price, discouraged growers to save seed this season. On the other hand, there was not much of a disposition to harvest a hay crop instead of a seed crop because of low prices for hay and high freight rates. The stand of timothy plants in most sections was thin and the hot, dry weather was detrimental to the filling out of the seed heads, consequently a smaller yield per acre than usual, estimated at 5% to 10% less than last year, was obtained.

Although considerable timothy seed has been carried over by growers, large country shippers, and by some of the larger seedsmen, the total carryover on July 1 probably was less than on July 1, 1920, because of the reported increased sales during the spring of 1921 and the production last year, which was below normal.

How to Trade in Seed.

For the information of its patrons in the seed and grain trade Southworth & Co., Toledo, O., have sent out the following concise message explaining some of the factors entering into the buying and selling of seed at Toledo:

Clover, alsike and timothy are traded in 50 bag lots or multiples thereof, figuring 2½ bushels to a bag. Dealers usually trade in carload lots. Two hundred bags are equal to 500 bushels, making a minimum carload of either clover or alsike, and 250 bags, or 625 bushels, a minimum carload of timothy. The smallest fluctuations are 2½ cents per bushel, equal to \$3.12½ on each 50 bag lot.

The commission for buying and selling alsike and clover futures is ½ of 1%, and on timothy 1% of the selling price. The commission on cash seeds delivered on contracts or consigned is 1% on clover and alsike; timothy 2% on less than carload lots, and 1½% on carloads. Toledo prices of clover and alsike are based on a bushel of 60 lbs., timothy 45 lbs. We are at present requiring a margin of \$3.00 per bag on clover and alsike and \$1.00 per bag on timothy.

The contract grade is prime seed. To grade prime it must be of fair color, dry, sound, reasonably clean and only slightly mixed with brown and foreign seed.

The buyers of futures must accept delivery when tendered any day during the month the trade was made for. The seller has until the last day of the month to make delivery.

The carrying charges on seeds are as follows: Storage, 7 cents per bag for the first 10 days or fraction thereof, and 1½ cents per bag for each succeeding 10 days. The prevailing interest is 7%, in addition to the insurance charge. The cartage rate has been reduced to 5 cents per bag for 50 bags or over. The approximate carrying charges basis \$13.00 would be about 1%, or 13 cents per bushel per month on clover seed and 4 cents on timothy basis \$3.00 bushel.

French Dealers Predict a Small Seed Crop.

A. Rousset & Fils, Paris, France, large wholesale dealers in field seeds, in a letter written Aug. 5 summarize the condition of the crops in France as follows:

We had practically no rain at all for the last three months, and as the weather was extremely hot, the new crop red clover is expected to be very short. There was little seed left from last year, and it is being sold now either in France or to England around \$26. Prices for the new crop will certainly be very high. Market rises rapidly also in Germany and Hungary.

The new crop of lucerne is expected to be much under the average. There is some seed left from last year. Market has greatly improved owing to the bad reports on the new crop. Choice seed of last year finds buyers at \$18.

Trifolium is an exceedingly small crop. Very little available to export. Quality fine, but prices around \$11. Latewhite is a very poor crop. Prices around \$21.50. A few parcels forwarded to England.

Trefoil crop is expected to be nil. Prices are rising rapidly owing to bad reports. Also rising in England and Luxembourg.

Rape seed crop is under the average. Exportation is prohibited, but some licenses may be granted.

Sainfoin. Prospects for new crop are most unsatisfactory. Not much seed left from last year, which is being sold around \$4.50.

Of white clover and alsike a small crop is expected.

Tall meadow oat grass crop is expected to be very short. No idea about prices yet.

RUSSIA WILL NEED two million tons of grain to supply its starving people and a quarter of a million tons of seed will be required before Sept. 1, according to information sent to the state department on Aug. 8.

MACON, GA.—The Georgia Farm Congress on Aug. 18 unanimously passed a resolution urging that the state take legal steps to stop the production of cotton in Georgia for one year in order to get rid of the boll weevil. Such action would no doubt increase the acreage devoted to grain and hay production.



Seed House of H. I. Davey at Pine River, Minn.

Grain Trade News

Reports of new firms, changes, deaths, casualties and failures; new elevators, improvements, fires and accidents are welcome. Let us hear from you.

CALIFORNIA

Oakland, Cal.—The matter of organizing a grain exchange has been under discussion by a number of grain interests, but has not yet coalesced into a definite organization.

San Francisco, Cal.—Sawyer-Cross Co. and A. H. Hankerson have been merged and now operate as Hankerson, Sawyer & Cross with offices in the Merchants Exchange Building.

San Francisco, Cal.—The state board of harbor commissioners will proceed with its plan to build a big grain warehouse, 800x100 ft., in the harbor. A sea wall will be built and the mud from the future channel and berth will be pumped behind the wall to help fill in.

Patterson, Cal.—The Farm Buro Elvtr. Corp. will conduct a demonstration of the handling of grain in bulk during the annual county fair here and will keep the elvtr., recently built, open and operating all of the time for the benefit of all visitors who care to look it over. Many converts to bulk handling are expected.

Sacramento, Cal.—Moving pictures of the actual handling of bulk grain will be exhibited at the State Fair here by the Farm Buro Federation, in its campaign for bulk handling of all grain. The pictures taken at the recently completed, up-to-date elvtrs. at Patterson and Montpelier, together with the final handling and loading of the grain at the big terminal elvtr. at Oakland, will it is believed convince many that bulk handling is not only practical but profitable.

Oakland, Cal.—The following is a schedule of the grain storage prices now prevailing at the Terminal Elvtr. (Farm Buro Elvtr. Corp. operators). Charges, unless otherwise specified, are on a tonnage basis: Unloading sacked grain, 50c; unloading bulk grain, 25c; storing and insuring, 20c per month first 4 months, 10c per month thereafter; loading out bulk grain, 25c; grading by a licensed federal inspector, \$1.50 per car. Charges for handling storing and insuring for 4 months amount to \$1.80 per ton. Empty sacks will be returned at shipper's expense or sold for their account. The elvtr. corporation will advance freight and storage to shippers until grain is sold. Bonded warehouse receipts are issued which are negotiable.

Los Angeles, Cal.—The Grain Exchange has voted to suspend all board trading in nearby deliveries of carload grain lots. Trading in the future on the call board will only be conducted in 100-ton lots of future grain deliveries. This action was taken by the exchange in the belief that the bidding on nearby grain shipments reflected only forced sales and purchases and that a fictitious market was in that way created. The trading in futures, in August, September, October, December and May shipment contracts, however, is believed to establish a real market, on which growers and brokers can rely. Operations in on track, in transit, and ten, fifteen and twenty-day grain shipments will be conducted from now on through the means of table trading and actual samples, and no figures will be posted on the board.

CANADA

Kerwood, Ont.—The Farmers Co-op. Co., Ltd. has bot the elvtr. of T. F. Mills.

Calgary, Alta.—The Alberta Pacific Grain Co. has sent its supt., H. M. Pearson into the Peace River Valley district to locate sites for elvtrs.

Guernsey, Sask.—S. J. Fox is now buyer here for the State Elvtr. Co., A. E. Buck for the Lake of the Woods Milling Co., and Geo. Oeh for the Western Elvtr. Co., Ltd.

Calgary, Alta.—The Brooks Elvtr. Co. of Minneapolis, Minn., has bot the line of elvtrs. of the McLean Grain Co. and the Davidson & Smith Elvtr. Co. in Saskatchewan and Alberta, and will operate the 42 houses as the Brooks Elvtr. Co., Inc.

Saskatoon, Sask.—The Interprovincial Flour Mills, Ltd., has moved into a new and modern office in the Cole Building. It has a laboratory attached under the management of Fred Loomis, formerly with the Wichita Milling & Elvtr. Co., Wichita Falls, Tex., U. S. A.

Moose Jaw, Sask.—The Board of Grain Commissioners has been requested to investigate the handling of grain and the practice of some of the elvtr. companies in this vicinity, by the order of Grain Buyers of Canada, of which G. F. Sullivan is sec'y. Several charges against the companies operating in the west have been made, especially in regard to the system whereby Mr. Sullivan alleges grain buyers are forced to protect themselves against the shortages at Fort William.

Fort William, Ont.—The Gillespie Terminal Grain Co., Ltd., has bot Elvtrs. "A" and "C" of the Canadian Pacific Ry. Co. at this port and the Horn Elvtr. at Port Arthur. The latter elvtr. has been closed for the last 3 years. It was built by Jos. King and later owned by David Horn. The Gillespie company now has 4 big elvtrs. with a combined capacity of 3,250,000 bus., made up as follows: Horn Elvtr., 750,000 bus.; "A" and "C" Elvtrs., 2,250,000 bus. and Gillespie Elvtr., 250,000 bus.

WINNIPEG LETTER.

The final meeting of the Canadian Wheat Board was held in this city, Aug. 11.

The Security Elvtr. Co., Ltd., has doubled its capital stock, making it now \$1,000,000.—B.

Hedley Shaw, managing director of the Maple Leaf Milling Co., who has been ill for almost a year, is reported to be steadily regaining his health in Western Canada.

The Board of Grain Examiners will soon hold examinations for inspectors of grain. The sec'y of the Board of Trade has applications blanks and all information needed.

J. A. Robb has opened a general grain brokerage office in this city. He was formerly mgr. for the Northland Milling Co., Ltd., and was also for some time connected with the Norris Grain Co., Ltd.

The United Grain Growers operate 95 of the 100 government elvtrs. in this province, under lease. All of the elvtrs. have been thoroughly overhauled and repaired and are in first class condition for the coming season.

Tariff of charges at public terminal elvtrs, tariff of charges at public country elvtrs. and rules and regulations for public country elvtrs. for the season of 1921-1922 were considered at a hearing of the Board of Grain Commissioners here, Aug. 23.

The old Board of Trade Building has been completely abandoned by the exchange which moved some time ago into the Royal Bank Building. The city has taken the old building over for municipal offices and the few grain firms left in it will be compelled to seek other quarters.

The Royal Inquiry Commission has disbanded, following the suspension of its activities by a recent injunction. No appeal can be made from the injunction before October or later. The offices occupied by the commission have been leased to others, the furniture will be sold and the staff of employees has been dismissed.

COLORADO

Denver, Colo.—J. W. Cook is now mgr. for the Westman Commission Co.

Denver, Colo.—The Colorado Mill & Elvtr. Co. will build a new office here.

Laird, Colo.—We have enlarged and improved our elvtr. this summer.—Farmers Co-op. Elvtr. Co.

Kit Carson, Colo.—We opened shipping stations at the following locations: Firstview, Ascolon (Cheyenne Wells, p. o.), Cheyenne Wells, Salas, Arapaho, all in Colorado; and at Weskan, Kan.—Plains Grain & Produce Co.

Sterling, Colo.—Fiefield & Stevens are out of business here and the office is closed.—S. J. White, White Milling & Elvtr. Co.

Manila (Watkins p. o.), Colo.—Colorado Mlg. & Elvtr. Co. will install a Hall Signaling Grain Distributor in its elvtr. at this station.

Severence, Colo.—The Severence Elvtr. Co. is under the management of the Model Mills, but I am mgr., not J. Rothschild.—E. C. Page.

Paoli, Colo.—We bot the elvtr. of Mitchell & Barkley and took possession Aug. 1. Chas. Reimer is mgr.—Renner, Smith Grain Co., Holyoke.

Denver, Colo.—J. K. Mullen, pres. of the Colorado Milling & Elvtr. Co., has been named a Knight of St. Gregory, one of the highest honors given by the Pope of Rome.

Carr, Colo.—Harry Cline will build an elvtr. here next year. At present he is buying at track. He has a 12,000-bu. elvtr. at Nunn.—Farmers Co-op. Elvtr. Co., Nunn.

Haxtum, Colo.—My initials are H. L. instead of R., as given in your Rocky Mountain List. Otherwise the list is fine.—H. L. Ballinger, mgr., Longmont Farmers' Milling & Elvtr. Co.

Calumet, Colo.—This is not a post office in this state altho listed in the new Rocky Mountain List. The Farmers Grain Co. listed at that station is located at Calumet, Okla.

Omar (Wiggins p. o.), Colo.—I am in the grain business here and operate as the Omar Grain Co. I have about 400 bus. of storage just now but will build an elvtr. later.—W. E. Conn.

Holyoke, Colo.—The new elvtr. of the Holyoke Grain Co. has been completed and is now in operation. Louis Speltz is mgr. The company later will build a number of warehouses.

Branson, Colo.—The Great West Mill & Elvtr. Co. of Amarillo, Tex., has secured a site and will start work on an elvtr. at once. A. F. Brown of Booker, Tex., J. S. Booker and Luther Grieg are interested.

Nunn, Colo.—The Model Mills Co. of Greeley, is building a 40,000-bu. elvtr. here in the place of the warehouse formerly run for them by Turner Bros. Harry Cline has a 12,000-bu. elvtr. here in connection with the W. L. Cline elvtr. but does not buy at Nunn.—Farmers Co-op. Elvtr. Co.

IDAHO

Payette, Ida.—The Payette Mills Co. has been overhauling and repairing its plant.

Kemp, Ill.—The elvtr. of Munson & Moss was slightly damaged by fire recently.

Aberdeen, Ida.—C. E. Ramsay has succeeded Gus. Lofgren as mgr. for the Midland Elvtr.

Kuna, Ida.—I have succeeded Bell Bros., formerly props. of the Kuna Milling and Elvtr. Co.—A. J. Petrie.

Midvale, Ida.—Geo. A. Urquhart, formerly mgr. for the Tri-State Terminal Co., is now buying independently.

Pocatello, Ida.—E. C. Enrich, vice-pres. of the Pocatello Milling & Elvtr. Co. and supt. of the local plant, has resigned.

St. Maries, Ida.—We are building our new elvtr. to replace the house damaged by fire June 4. Will also build warehouse and store. We have a frontage of 60 ft. and trackage of 112 ft. on the back of the site. We expect to be ready to install machinery by Oct. 1.—Geo. O'Dwyer, Inc.

American Falls, Ida.—"We have leased our elvtrs. to the Globe Milling Co. for a period of 1 year, the lease to expire June 1, 1922. We were unable to finance a grain business this year, due to the stringency of the money market. We feel that we are doing the very best thing for our members in leasing the property until conditions have bettered themselves. By another year we will probably be in a position to resume our grain trading. Had we not leased, we would have had our entire investment in elevators lying idle. The present solution is much better. Our brokerage business here, in Pocatello and at other points will be maintained and operated as usual as the terms of our lease are for the elvtrs. only."—A. Y. Satterfield, gen. mgr. Intermountain Farmers Equity Co. (The company leased 9 elvtrs. to the Globe Grain & Milling Co., of Ogden, Utah, which will warehouse grain for the Idaho Wheat-growers Ass'n. The elvtrs. are located at Rexburg, Rigby, Inkorn, McCammon, American Falls, Schiller, Virginia, Arimo and Downey.)

Weiser, Ida.—John White, employed by the Weiser Milling & Elevator Co., had his hand badly cut and bruised when he caught it in some moving machinery.

Priest River, Ida.—H. A. Dart is mgr. for the Priest River Valley Grain Co., which will build a warehouse on the Gt. Nor. Ry. The company is a branch of the Coeur d'Alene Grain & Mfg. Co.

ILLINOIS

Edwardsville, Ill.—C. E. Geihur is now mgr. for the Farmers Co-op. Co.

Arcola, Ill.—We have bot the business of J. M. Ernst & Son.—Glen F. Smith, W. H. Smith & Son.

Rushville, Ill.—The 25,000-bu. elvtr. of H. V. Teel is now completed. It is 101 ft. high and 27 ft. in diameter.

Spires (Minonk p. o.), Ill.—We have installed a new 15-h. p. Fairbanks Engine in our elvtr.—Spires Elvtr. Co.

Peoria, Ill.—A. V. Laszloffy and Arthur S. Jackson have been admitted to membership in the Board of Trade.

Joliet, Ill.—R. T. O'Neill is now mgr. for Philip H. Schifflin & Co., Inc., Chicago, with offices in the Heggie Building.

Williamsfield, Ill.—I am not mgr. at this station, as has been reported and am out of the grain business at present.—Verne L. Marks.

Covel, Ill.—Our recent fire loss amounted to only \$69.30. The repairs required have been made.—A. L. Nicol, mgr. Farmers Grain Co.

St David, Ill.—The material for our new warehouse has arrived and work is now going forward without delay.—E. G. Carver, Farmers Co-op. Grain & Supply Co.

Berwick, Ill.—I have sold my elvtr. to Mahoney & Underwood, who will operate it under the name of the Berwick Grain Co. Gale K. Young is mgr.—W. M. Simmons.

Kewanee, Ill.—The adding machine in the elvtr. office of W. B. Cavanaugh, was stolen recently. The thieves broke a window to get into the elvtr. Nothing else was disturbed.

Minier, Ill.—The Little Mackinaw Grain Co. incorporated for \$50,000 by F. S. Freitag, J. Haning and others. For particulars in reference to this company see elsewhere in the Journal.

Argenta, Ill.—Chas. Likens, who has been mgr. for the Argenta Grain & Milling Co. since the company has been operating here, has resigned and will be succeeded by C. H. Luckenbill.

Berdan, Ill.—The Farmer Elvtr. Co. made an attempt to organize but never completed its organization. The Advance Mill & Elvtr. Co. still own and operate the only elvtr. here.—J. F. Pinkerton.

El Paso, Ill.—The fire which destroyed my elvtr. and 5,000 bus. of grain recently was caused by a spark from a train. The insurance does not cover the loss but I am going to rebuild.—John Kinsella.

Bloomington, Ill.—J. A. McCreery & Son, of Mason City, have opened an office in the Griesheim Building and will be correspondents for the J. Rosenbaum Grain Co. W. H. Barnes will be mgr. with Earl Fenton as ass't.

Newman, Ill.—The new elvtr. of the Farmers Grain & Lumber Co. has been completed. It replaces the house burned last fall and is now in operation. It is of concrete construction and has a 50,000 bus. capacity. There are 8 tile storage tanks in connection. Oscar Anderson, of Broadlands, is reported to be slated as mgr.

Byron, Ill.—The grain in an upper bin in the elvtr., of the Farmers Elvtr. Co., was found in flames about 9 a. m., Aug. 13. It is believed that lightning struck the elvtr. the night before during a heavy storm and that the grain had been smoldering for some time. Quick work put out the blaze and most of the damage was due to water.

New Canton, Ill.—The elvtr. of the King Milling Co., containing 12,000 bus. of grain, burned Aug. 12 and is a total loss. The fire started in the cob house at 8 p. m. The books and safe were all that were saved. J. R. Kendrick was local mgr. Difficulty was experienced in preventing the conflagration from extending to the elvtr. of Heidloff & Rose, about 300 ft. distant. Both elvtrs. are ¾ of a mile from the village.

Decatur, Ind.—The loss on our elvtr. burned July 1, amounted to \$7,000 and \$2,000 on stock, making the total \$9,000. It is fully covered by insurance. The adjusters think the blaze was started by a spark from a switch engine.—Shellabarger Elvtr. Co.

CHICAGO NOTES.

Board of Trade memberships are quoted at \$6,600 net to buyer.

Jinosuki Minami has applied for membership in the Board of Trade.

John H. Herron and R. L. Sunderling have been admitted to membership in the Board of Trade.

Gov. Small has appointed Charles J. Peters as chief grain inspector to succeed Walter H. Schmidt, effective Sept. 1.

The offices of the Grain Inspection Division of the Illinois Department of Commerce have been moved to the Imperial Building.

Members admitted to the Board of Trade Aug. 23 were: Samuel Tilden Aber, Moses J. Sleppeck, G. P. Harbison and W. D. Haggerty.

Wilbur Wait, at one time active in the grain trade, died at his home in Evanston, Aug. 20, at the age of 89. He retired some years ago.

The proposed amendment to the rules of the Board of Trade making No. 3 rye deliverable on contracts for futures delivery at a penalty of 5c a bu. has been approved by the directors.

Receivers can handle cars for which the B/L is not available by filing a bond with the Board of Trade joint agency, under an arrangement made with most of the railroads, effective Aug. 15.

Fred Payne is now on the road for C. H. Thayer & Co., covering his old territory in Nebraska, Iowa, South Dakota and southern Minnesota. His headquarters are at Sioux Falls, S. D.

Corn in South Chicago Elvtr. C and Annex has gone out of condition and on Aug. 12 the South Chicago Elvtr. Co. posted notice on the Board of Trade to the holders of the warehouse receipts aggregating several hundred thousand bus. Most of the corn was taken in during the month of June and none of the receipts already posted bears date later than July 18.

INDIANA

Greenwood, Ind.—The Greenwood Grain Co. is owned by the Suckow Milling Co., of Franklin.

Crete, Ind.—The Crete Elvtr. Co. incorporated for \$20,000 by James B. Jordan, Fred Elliott and others.

Kempton, Ind.—Ward Lynn, employed by Cohee & Clark, at their elvtr., recently had his hand caught in the machinery and badly crushed.

Ossian, Ind.—The Melcher Grain Co. suffered a loss of \$1,200 Aug. 11, when a large storage barn containing several tons of hay and 600 bus. of oats, burned.

Brownstown, Ind.—We have not sold out here.—Ewing Mill Co. (Reported some time ago that Farmers would organize and buy property of the above company.)

Shelby, Ind.—J. E. Caster, who has been a member of the National Hay Ass'n for 10 or 12 years, has been suspended from membership for refusing to arbitrate.

Indianapolis, Ind.—Carl D. Menzie Grain & Brokerage Co. (established in 1919), incorporated for \$20,000 by Carl D. Menzie, J. C. Consoidine and I. G. Holdaway.

Indianapolis, Ind., Aug. 23.—I am celebrating a long wedding anniversary today, 28 years, not bald and fairly active in business. It is remarkable what some people can stand.—Bert A. Boyd.

Hartford City, Ind.—Edwin C. Wright has bot the property of the Hartford City Grain & Milling Co. and is in possession. M. V. Groves, who has been mgr. since 1896, will remain with the new firm.

Rockfield, Ind.—Frank Darrow, of Wallcott, will be mgr. for the Farmers Co-op. Elvtr. Co. after Sept. 15. John W. Kerlin and Chas. Lane have been temporarily handling the elvtr. since the death of Mgr. Edmon Roach.

Coatesville, Ind.—Altho the Sutherland Bros. sold their interests in the Coatesville Elvtr. & Feed Co. to A. J. "Dick" Smith, F. A. Rouse still owns his third interest in the company. Mr. Smith will be mgr. and is now in charge.

Valparaiso, Ind.—We took over the plant here July 25, and are in good running order. The plant was repaired by the Reliance Constr. Co. and is up-to-date. Will have a full line of side lines within 30 days.—S. J. Brown, mgr., Chatfield Grain Co.

Emporia, Ind.—Pierre B. Aiman, mgr. for the Emporia Grain Co., died Aug. 3, in a hospital in Indianapolis following an operation performed July 29. He is survived by his wife and 2 children. He was 27 years old and very popular with all who came in contact with him.

Indianapolis, Ind.—The National Elvtr. Co. has not retired from business, as rumored, but has made a change in the organization whereby the company will be handled separately from the American Hominy Co. The business is to continue as heretofore.—H. J. Berry, mgr. National Elvtrs. Co.

North Manchester, Ind.—The stock of the Farmers Elvtr. Co. is being sold out and the elvtr. and other property will be sold after Sept. 12.—P. Vickery, mgr. (The Farmers Elvtr. Co. was placed in the hands of receivers by Judge Hunter of Wabash. The Union Trust Co. has been appointed to look after the affairs of the company. The company owes \$1,110.23 for coal according to reports. The town is North Manchester, not Manchester as reported Aug. 10.)

IOWA

Pierson, Ia.—R. L. Staben is now mgr. for the McCall Elvtr. Co.

Stanhope, Ia.—The Farmers Elvtr. Co. has installed a truck dump.

Rock Falls, Ia.—Evert Sleeper is our agt. at this station.—Hubbard Grain Co.

Atlantic, Ia.—I am not active in the grain trade at present.—L. F. Spangler.

Randolph, Ia.—T. A. Rohde is now mgr. for the Farmers Elvtr. Co. succeeding L. R. Irons.

Bremer, Ia.—W. A. Smay of Waverly, has succeeded Frank Lynes as mgr. for the Farmers Elvtr. Co.

Conrad, Ia.—C. E. Beall has resigned as mgr. for the Farmers Elvtr. Co. here and will move to Williams.

Sioux City, Ia.—The Van Dusen-Harrington Co. has opened an office here with Arthur J. Lariff as mgr.

Cedar Falls, Ia.—The elvtr. of the Farmers Co-op. Co. was recently damaged by fire.—The loss was small.

Algona, Ia.—The Algona Flour & Feed Co. is the new name for the Bowles-Kessler Grain Co. W. A. Dutton is mgr.

Ireton, Ia.—The Taylor Grain Co. has bot the grain and coal business of H. L. Staben, who is now located at Pierson.

Inwood, Ia.—We have leased the elvtr. of the Farmers Elvtr. Co., now owned by the Inwood Elvtr. Co.—T. E. Moen & Son.

Williams, Ia.—C. E. Beall, former mgr. of the Farmers Grain Co. at Conrad, has bot the elvtr. of E. J. Funk at this station.

Terrill, Ia.—L. R. Irons, formerly mgr. at Randolph, is now mgr. for the Farmers Co-op. Elvtr. Co., here, succeeding W. F. Mitts.

Rock Rapids, Ia.—The Farmers' Co-op. Co. has drawn plans for a new cement block office and sales room, 25x60 ft. and one story high.

Malvern, Ia.—I have taken over my father's business and operate in my own name. The old name was P. Kilmartin.—A. P. Kilmartin.

Inwood, Ia.—I have leased the elvtr. of T. E. Moen & Son for a year and will operate it myself. We have done quite a bit of repairing on the house.—G. A. Lyon.

Atlantic, Ia.—Wm. Henshaw, who in 1844 formed a partnership with M. Ringle and operated a grain business as Henshaw & Ringle, died Aug. 11 at the age of 79.

Woodburn, Ia.—Wm. Bell and son Ralph, have leased the elvtrs., seed warehouses and coal sheds of F. M. Keeney & Sons. The new company will operate as Wm. Bell & Son.

Denhart (Kanawha p. o.), Ia.—I am now mgr. for Davis Bros. & Arnold but have not bot an interest yet.—Glen McCarel, formerly mgr. for the Farmers Elvtr. Co. at Albion.

Davenport, Ia.—We have closed our Minneapolis, Minn., office and moved to Davenport, where our terminal elvtr. is located. We desired to be nearer our country elvtrs.—H. D. McCord, pres. of the Merchants' Elvtr. Co.

Cedar Rapids, Ia.—Mgr. Jack Hill of Lamson Bros. & Co. here, recently fractured his leg and Ass't Mgr. James Robbins of the Milwaukee office, is now managing the office.

Rock Falls, Ia.—I am out of the grain business.—H. W. Hill. (Mr. Hill formerly leased the elvtr. of the Hunting Elvtr. Co. which was recently bot by the Hubbard Grain Co.)

Goldfield, Ia.—The cupola of the old elvtr. of the Farmers Elvtr. Co. was discovered in flames recently, but the quick work of the fire department put it out with little damage to the elvtr.

Albia, Ia.—We have completed our 12,000 bu. elvtr. and it is now running in fine shape. The equipment includes Richardson Automatic Scale and Boss Car Loader.—Wilkins Grain Co.

Norway, Ia.—Geo. Miller, formerly mgr. for the Mowbray Elvtr. Co., at Malta, Ill., has succeeded Leo Nimback as mgr. for the Quaker Oats Co. here. Mr. Nimback has been transferred to Renwick by the company, where E. Sterner is mgr.

Tingley, Ia.—The elvtr. and office building of the Farmers Union Elvtr. Co. burned Aug. 10, at 2 a. m., with a loss of \$11,000, covered by \$6,000 insurance. The fire was beyond control when discovered and the elvtr. and office, which were iron clad, are a total loss.

Dawson, Ia.—R. S. Witter, who retired from the grain business a few months ago, is again in the game. He has bot the elvtr. of the Farmers' Elvtr. Co. and will operate it personally. He is repairing and remodeling the house and will put it in No. 1 shape.

Gladbrook, Ia.—I am receiver for the Farmers' Elvtr. Co. here and will sell the plant and the business just as soon as the court orders it done. The capital stock of the company, \$54,550, is wiped out.—Wm. Lorenz, receiver. (Total liabilities amount to \$130,046.13 and assets to \$108,447.54.)

Sioux City, Ia.—W. M. Bell and F. H. Huntington, who combined their offices here about 2 years ago and operated as the Bell-Huntington Grain Co., have dissolved the firm and Mr. Huntington will continue the business alone. The W. M. Bell Co. has offices at Sioux Falls, S. D. and at Algona, Ia.

Sherdahl, Ia.—Entrance thru the sliding doors of the drive way and thence thru the office door by removing a panel in the door, was gained by thieves recently in an attempt to rob the office of the Farmers Grain & Coal Co. The attempt to open the safe was unsuccessful and the burglars secured nothing.

Sheldon, Ia.—The elvtr. of J. H. Wolf was struck by lightning during the night of the 16th and a hole was torn in the roof large enuf for a man to crawl thru but no blaze was seen. The next morning when Mr. Wolf opened the elvtr. a draft was created and some of the grain in the elvtr. began to blaze. The fire was promptly put out, the damage amounting to about \$100.

Des Moines, Ia.—The new concrete elvtr. of the Iowa Corn Products Co. has been completed and is in operation. The elvtr. is 50 x 50 ft. on the ground and 201 ft. high, and has in connection 2 storage tanks, 21 ft. in diameter. The house is equipped with the latest machinery and has a capacity of 120,000 bus. For the present only the elvtr. will be in operation by the company, which expects to do a transfer business in grain. A. N. Heggen is vice-pres. of the company.

Liscomb, Ia.—B. F. Froning, former mgr. of the Farmers Elvtr. Co. at Eldora, has bot the elvtr. and the coal, lumber and building material yards of B. F. Vorhes, who has conducted the business here for the last 9 years and has been in the grain trade for the last 35. Mr. Vorhes sold his house to Mr. Froning and will probably retire from active business, moving to Des Moines. J. A. Beaman, who has "been sold" with the business a number of times, will remain with the new owner.

KANSAS

Leoti, Kan.—Ed. Chase has resigned as mgr. for the Equity Co-op. Mill Elvtr. Co.

Russell, Kan.—The K. Foss Feed Co. has bot the elvtr. of the Farmers Union.

Hartford, Kan.—O'Connor & Co. will install a truck dump and scale in their elvtr. soon.

Protection, Kan.—C. D. Eubanks, is now mgr. of the elvtr. of the Arkansas City Milling Co.

Marysville, Kan.—H. D. Harding of the Topeka Grain Co. is now mgr. of the company's office here.

Ensign, Kan.—L. F. Henry of Dodge City, has leased an elvtr. of the Farmers Grain & Supply Co.

Hoxie, Kan.—E. E. McClassen will operate the elvtr. of C. F. Crofoot for the next year under lease.

Atchison, Kan.—Dilts & Morgan now have an office here with Donald F. Priest, of Kansas City, as mgr.

Wichita, Kan.—The F. O. Jones Brokerage Co. has opened an office here in the Sedge-wick Building.

Jennings, Kan.—J. Cumming has succeeded J. A. McKinney as mgr. for the C. E. Robinson Grain Co.

Hugoton, Kan.—Geo. Stockstill has bot the site of the Dodge City Alfalfa Mill & Elvtr. Co. and will build an elvtr.

Topeka, Kan.—A. Conner has succeeded H. D. Harding as mgr. for the Topeka Grain Co. here. Mr. Harding is at Marysville.

Spring Hill, Kan.—I have leased the elvtr. of J. S. Null.—H. E. Mossman. (It was reported that he bot the house.)

Hoxie, Kan.—The Robinson Grain Co., of Salina, has bot the elvtr. of the Farmers Elvtr. Co. here. R. G. Van Horn is mgr.

Esbon, Kan.—Dan Feldcamp has bot an elvtr. and will operate it himself, resigning as mgr. for the Farmers Union Elvtr. Co.

Wichita, Kans.—Pres. E. F. Beyer, of the Board of Trade, is again at his desk after an illness of a week or more duration.

Clifton, Kan.—J. A. Steber will be mgr. for Keith Goodwin who recently leased the elvtr. of the Associated Mill & Elvtr. Co. here.

Sylvia, Kan.—The plant of the Sylvia Milling Co. is again in operation after a shut down of several months. W. E. Bowman is prop.

Belpre, Kan.—A. M. Long, of Belpre, has bot the Belpre Co-op. Equity Union and the elvtrs. of the same company at Hawes and Malloy.

Abilene, Kan.—The elvtr. of the Farmers Co-op. Elvtr. Co. was discovered on fire recently. Prompt attention to the blaze left a small loss.

Fontana, Kan.—Andrew Bluffington has succeeded C. E. Conley as mgr. for Blaker Lumber & Grain Co. Mr. Conley is now at Paola, Kan.

Alton, Kan.—L. O. Duer has succeeded me as mgr. for the Osborne County Co-op. Ass'n.—W. E. Conn, owner and mgr. of the Omar Grain Co., Omar, Colo.

Holton, Kan.—We do not expect to go out of business and are not now offering our elvtr. for sale.—W. E. Beightel, the Jackson Co. Grange Co-op. Ass'n.

Castleton, Kan.—D. E. Krehbiel and I have leased the elvtr. of the Farmers Elvtr. Co. and are operating it as Krehbiel & McCaffery. I am mgr.—John McCaffery.

Colby, Kan.—The E. T. Rickel Grain Co. is installing a conveyor from the elvtr. to the track, a distance of about 150 ft. Hal B. McDougal, mgr. Morrison Grain Co.

Denton, Kan.—Mail addressed to F. W. Fanon who was reported to have traded his farm for the elvtr. of the Denton Grain Co., is returned with the notation "Not Here."

Centerview, Kan.—Jos. D. Zook is now mgr. for the Centerview Co-op. Exchange, succeeding B. J. Allen who resigned some time ago. Mr. Zook was formerly at Nettleton.

Hutchinson, Kan.—Mill "B" of the Wm. Kelly Milling Co., who bot the property last October from the Reno Flour Mills Co., has been overhauled and put into excellent repair.

Wichita, Kan.—Harry J. Williams and W. J. Coleman have been admitted to membership in the Board of Trade. Z. M. Evans has applied for membership on transfer from H. C. Rice.

Udall, Kan.—The report that the Farmers Mill & Elvtr. Co. had bot the Shoup & Throckmorton elvtr. here was erroneous. The elvtr. was at Mulvane, not here and was bot by the Mulvane Co-op. Union.

Almena, Kan.—We bot elvtr. of the the Central Granaries Co. last July and operate it. The elvtr. of Ira Reeves is only used for storage.—J. H. Gailey, mgr. Roller & Jennings. (Main office, Wray, Colo.)

Jeffmore, Kan.—A. H. Ling, elvtr. owner here, is reported to have bot 4 grain cars in order to move the surplus grain which he had piled on the ground around his elvtr. It is said that he has 7,000 bus. or more in the pile.

Salina, Kan.—The Robinson Wyatt Grain Co. is successor to the C. E. Robinson Grain Co. and the Robinson Grain Co. There are now the four distinct and separate firms here, as follows: Robinson-Wyatt Grain Co., Robinson Elvtr. Co., Robinson Milling Co., and the Son Grain Co.

Belleville, Kan.—The Belleville Mill & Elvtr. Co. is increasing its storage capacity to the extent of 25,000 bus. A new storage tank, 21 ft. in diameter and 35 ft. high has been added to the plant. It has a capacity of 10,500 bus. The new flour warehouse will have a capacity of 10,000 bus. of ground feed, meal, etc.

Cawker City, Kan.—The elvtr. at this station, formerly operated by the Jones-Roger Grain Co., was operated under lease and when the lease expired, July 1, it was not renewed. The company still operates houses at Quinter, Voda and Seguin, with headquarters at Hays City, where it is doing a wholesale grain merchandizing business.

Glen Elder, Kan.—The Kansas City Milling Co., of Kansas City, Mo., which is the reorganized company that took over the interests of the Kaul Milling Co. which recently failed, has notified Libby & Goudy, who have been operating the plant under lease since April to vacate as the new company will now operate the plant. L. E. Wellman will be mgr. of the elvtr. and Paul Workman will be in charge of the mill.

KENTUCKY

Louisville, Ky.—The grain traffic department of the Board of Trade will enlarge its traffic department.

Morganfield, Ky.—Thos. B. Young, for many years in the grain trade in this vicinity, died Aug. 15 in a hospital at Evansville following an operation. He was 77 years old and is survived by 5 children.

MARYLAND

Baltimore, Md.—The Western Md. Ry. Co. has announced that it will build a twin elvtr. to the house now owned and operated by it at Port Covington. It will cost in the neighborhood of \$1,000,000. The company has asked permission of the Interstate Commerce Commission to borrow \$1,000,000, to be secured by first re-funding mortgage bonds.

MICHIGAN

Kawkawlin, Mich.—The Kawkawlin Bean & Grain Co., will build an elvtr. here.

Fowler, Mich.—W. T. Cloverdale has resigned as mgr. for the Fowler Co-op. Elvtr. Co.

Escanaba, Mich.—The elvtr. of Hewitt Grain & Provision Co. burned recently with a loss of \$5,000.

Hastings, Mich.—We have completed our new elvtr. at this point.—Hastings Co-op. Elvtr. Ass'n.

North Branch, Mich.—The North Branch Producers Ass'n has succeeded the North Branch Grain Co.

Detroit, Mich.—The Carson Grain Co. has bot a site on the river and will build a 50,000-bu. elvtr. of the latest and most up-to-date type. It is hoped to have the house completed in time to handle this year's corn.

SEEDBUERO QUALITY GRADING EQUIPMENT
 IS THE BEST AT LOWEST PRICE,
 IT WILL PAY YOU TO USE THE
 OFFICIAL BROWN-DUVEL MOISTURE
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Goblesville, Mich.—The Hutchinson interests in the Goblesville Milling Co., were recently sold to Frank Fairfield.

Burlington, Mich.—We have sold our elvtr. here and C. O. Strong, formerly our agt., is not now employed by us.—Stockbridge Elvtr. Co.

Burlington, Mich.—We have bot the elvtr. of the Stockbridge Elvtr. Co. and I am running the house.—C. O. Strong, Farm Buro Marketing Ass'n.

Freesoil, Mich.—The Farmers Elvtr. Co. has installed a new spout and is rushing work on the elvtr. Grain is being handled thru the house now.

MINNESOTA

Altura, Minn.—The Altura Elvtr. Co. will build a new elvtr. this fall.

Browns Valley, Minn.—Wm. Rixe is now agt. for the Cargill Elvtr. Co. here.

Doran, Minn.—J. H. Donaldson is now mgr. for the Farmers Elvtr. Co. here.

Duluth, Minn.—G. W. Walker has applied for membership in the Board of Trade.

Florence, Minn.—The Atlas Elvtr. Co. has bot the elvtr. of the Northwestern Elvtr. Co.

Doty Sta. (Eyota p. o.), Minn.—H. W. Solnigs will build an elvtr., warehouse, etc., here.

Dundas, Minn.—Louis Hansen has bot the elvtr. here and will take personal charge of it.

Grove City, Minn.—Walter Peterson, Lake Elizabeth, is now mgr. for the Farmers Elvtr. Co.

Deer Creek, Minn.—Otto F. Nelson is mgr. for K. B. Wilson, who recently bot the elvtr. here.

Madison, Minn.—The elvtr. of the Atlas Elvtr. Co. has been closed on account of short crops.

Taunton, Minn.—C. J. Inhofer has succeeded F. B. Hartwick as mgr. for the Marshall Milling Co.

Harbor Beach, Minn.—The Farmers Co-op. Co. is installing new machinery including a cleaner.

Fairmont, Minn.—E. W. Stafford, of Sioux City, is now mgr. for the Farmers Co-op. Elvtr. Co.

Worthington, Minn.—J. A. Seward has succeeded T. C. Ager as mgr. for us.—Farmers Co-op. Co.

Wolverton, Minn.—The elvtr. of the Monarch Elvtr. Co. will be closed for the season of 1921-22.

Climax, Minn.—The elvtr. of the Northwestern Elvtr. Co. has been closed for an indefinite period.

Frost, Minn.—The Farmers Co-op. Co., incorporated for \$100,000 by A. E. Amundson and E. O. Dybevic.

Alpha, Minn.—Frank Matson, formerly mgr. for the Farmers Elvtr. Co. here, has bot an elvtr. at Triumph.

La Fayette, Minn.—Henning W. Johnson is ass't mgr. for the Farmers Elvtr. Co. here. F. J. Goblirsch is mgr.

Pelican Rapids, Minn.—I have succeeded C. J. Swanson as mgr. for the Co-op. Warehouse Ass'n.—J. R. Quamme.

Burtrum, Minn.—The Tanner Elvtr. Co. recently bot the elvtrs. of the Erwin Elvtr. Co. here and at Grey Eagle.

Middle River, Minn.—E. O. Ramstead is now our mgr.—Middle River Co-op. Elvtr. Co. (M. T. Bakke was former mgr.)

Elrosa, Minn.—Lightning struck our elvtr. and tore up the cupola but there was no fire.—H. Sieben, agt. Belgrade Flour Mill Co.

Park Rapids, Minn.—K. B. Wilson, former mgr. for the Farmers Produce Co., recently resigned, having bot an elvtr. at Deer Creek.

Sauk Center, Minn.—A. E. Erwin, who has been in the grain business for 47 years, has sold his elvtr. here to the Equity Grain Co., of St. Paul.

Marshall, Minn.—The Farmers Elvtr. & Milling Co. has decided to change to the co-op. plan of business and the company will be re-organized.

Sleepy Eye, Minn.—The new 500,000-bu. elvtr. of the Farmers Elvtr. Co. has been completed and is now in operation. A full description of the elvtr. with plans, was given in the April 10 number of the Journal.

Doland, Minn.—J. S. Foster, ass't mgr. for the Farmers Elvtr. Co. for the past 7 years, is now mgr. of the Sully County Farmers Ass'n at Oneida, S. D.

Lismore, Minn.—The Farmers Elvtr. Co. has been reorganized and the elvtr. taken over and put into operation. J. C. Webster is pres. and J. F. Comiskey, sec'y.

Clements, Minn.—M. J. Fowler has succeeded B. F. Bertrand and operates the elvtr. here.—B. J. Bertrand, now mgr. for the Farmers Elvtr. Co. at Morgan.

St. Paul, Minn.—C. C. Chambers was elected pres. of the Grain Exchange at its recent annual election. W. E. Coles, Jr., vice-pres. and James F. O'Meara, sec'y.

West Concord, Minn.—Since Mr. Wheeler retired from the firm the elvtr. is being operated as the A. J. Wilson Elvtr. I have installed a new cleaner.—A. J. Wilson.

Twin Valley, Minn.—Henry Vehle for many years agt. for the Thorpe Elvtr. Co. here, has bot. the elvtr. of the Twin Valley Grain Co., later operated by M. A. Heiberg.

Motley, Minn.—There is no Farmers Elvtr. Co. at this station as reported. There is a Farmers Shipping Ass'n but it does not handle Grain.—C. E. Seeley, agt. Monarch Elvtr. Co.

Madelia, Minn.—The C. S. Christensen Co., Inc., has taken over the plant of C. S. Christensen & Co. which was declared bankrupt some time ago and will start to operate it, Sept. 1.

Triumph, Minn.—Frank Matson of Alpha, has bot the elvtr. of the Triumph Grain Co., which has been operated by the W. C. Mitchell Co., of Minneapolis, for a number of years. P. S. Hanson, mgr. for the former owners, will move to Minneapolis.

Duluth, Minn.—T. L. Gibson, F. S. Lewis, R. M. Knox and A. S. Riches, have been elected to membership in the Board of Trade and the membership of E. S. Jennings, H. F. Crofton, B. B. Gunderson and E. W. Myron have been posted for transfer.

Truman, Minn.—We have completely overhauled both of our elvtrs., installing a man-lift and truck dump in Elvtr. No. 2, and wagon scales at the office for coal weighing. Both houses now have galvanized roofs and siding.—Farmers Elvtr. Co.

Waseca, Minn.—L. E. Olmstead and W. L. McPeak have bot the property of the defunct Waseca Milling Co. and will engage in the grain business. The mill will not be opened. The new company will be known as the McPeak Elvtr. Co. and Mr. Olmstead will be mgr.

Lake City, Minn.—The elvtr. of the Hunting Elvtr. Co. was threatened with destruction by fire when sparks from an engine set fire to the depot and the flames spread to the roof of the elvtr. The blaze was quickly put out but the wheat and other grain in the house was scorched and soaked.

St. Cloud, Minn.—Our new elvtr. will be of cribbed, iron clad construction, 5 stories high; with a capacity of 40,000 bus. We are working hard on it and expect to have it completed in time to receive the first movement of the spring wheat crop around here.—Great Northern Flour Mills Co.

Cray (Lake Crystal, R. F. D.), Minn.—We have taken over the elvtr. here and will operate it in conjunction with our elvtr. at Lake Crystal. There has nothing been done toward organizing a farmers elvtr. company here of late and there will not likely be one.—C. W. Clark, mgr. Hubbard & Palmer Co.

MINNEAPOLIS LETTER.

C. J. Marboe has been elected to membership in the Chamber of Commerce.

The "Marquette" Elvtr. has been declared regular by the Chamber of Commerce.

J. R. Stewart is reported to have made an offer of \$400,00 for all of the assets of the Clax Milling Co., now in the hands of a receiver.

I have resigned from the Washburn Crosby Co. and have accepted a position with the Barber Milling Co., as a representative in Iowa, Illinois and Indiana.—R. R. Bond, Cedar Rapids, Ia.

The Brooks Elvtr. Co. has taken over the 42 elvtrs. of the McLean Grain Co. and the Davidson & Smith Elvtr. Co. in Saskatchewan and Alberta, Canada, and will operate them as the Brooks Elvtr. Co., Ltd.

Geo. Sanborn, who traveled for Nye-Jenks & Co. until the company sold out, is now on the road for Quinn-Shephardson Co., covering his old territory.

The Monarch Elvtr. Co. has bot the 58 elvtrs. of the Northwestern Elvtr. Co., making a total of 168 line elvtrs. for the Monarch company, which is a subsidiary of F. H. Peavy & Co.

We have closed our Minneapolis office and moved to Davenport, where our terminal elvtr. is located and near many of our country elvtrs.—H. D. McCord, pres., Merchants Elvtr. Co.

Fire was discovered on the 6th floor of the elvtr. of the Erie Elvtr., operated by the Fleischmann Yeast Co., Aug. 11 and while the blaze did little or no damage, \$20,000 worth of wheat was wet down and practically rendered worthless by the firemen. A large amount of machinery was also damaged.

The annual golf contest of the Chamber of Commerce was held Aug. 2 at the White Bear Yacht Club, White Bear. Geo. K. Labatt, of the St. Anthony & Dakota Elvtr. Co. was declared the champion, his score being 76, Harry Van Every was a close second who made the winner look to his laurels, his score being only 77. The team match was won by the bulls who defeated the bears by a score of 20 to 17. B. F. Benson was captain and Walter G. Hudson, ass't captain of the bulls while J. H. MacMillan and Walter Brittan, held the same positions with the bears.

MISSOURI

Turney, Mo.—The elvtr. of the Turney Co-op. Co. is completed.—H. F. Brenner.

Hunnewell, Mo.—L. Calvert is now mgr. for the Farmers Elvtr. & Exchange Co.

Gainsville, Mo.—The Alley Grain Co., of Mercer, has bot the coal business of R. Kaughenberry here.

Madison, Mo.—L. G. Abel, of Monroe City, has succeeded H. B. Atterbury as mgr. for the Madison Elvtr. Exchange.

Sikeston, Mo.—The C. L. Cook Grain Co. has moved into larger quarters on the ground floor of the McCoy-Tanner Building.

St. Joseph, Mo.—The Donahue-Fahey-Stratton Corp. will take over the Elwood Elvtr. here and will do a general grain business.

Kidder, Mo.—H. E. Brenner has resigned as mgr. for the Talbott Grain Co. here and is now mgr. of the Turney Elvtr. Co. at Turney.

Pierce City, Mo.—I have not bot. the elvtr. here but am operating it for the John F. Meyer Milling Co. of Springfield.—N. A. Kelly.

Carthage, Mo.—Our officers are S. O. Morrow, pres.; S. R. Morrow, sec'y and mgr. and E. W. Kidder, vice-pres. and sales mgr.—Morrow Milling Co.

Waverly, Mo.—The Farmers Elvtr. Co. incorporated for \$10,000 by T. Landrum and others. The company had been reported as a purchaser of the elvtr. of W. A. Guenther.

Canton, Mo.—The Canton Co-op. Co. has completed its new 20,000-bu. elvtr., replacing the house burned Feb. 10. The new house is 23x35 ft. on the ground and 60 ft. high and has 8 bins. Glenn Rogers is mgr.

Independence, Mo.—The present officers of our company are W. H. Waggoner, pres.; A. B. Waggoner, vice-pres.; T. O. Cunningham, sec'y-treas., and W. I. Heddens, sales mgr.—Waggoner-Gates Milling Co.

La Grange, Mo.—I did not sell my elvtr. here, nor at Durham, as has been reported. I am running both houses and buying grain.—W. E. Mitchell. (It was reported some time ago that farmers would take over the plants.)

KANSAS CITY LETTER.

Frank O. Everts has decided to enter the grain trade here and will be associated with his cousin, Ralph Orthwein.

Mail addressed to the Langenberg Bros. Grain & Hay Co. at its office in the Gibraltar Building is returned marked, "Removed."

The Knight Brokerage Co. is now settled in its new quarters in the Massachusetts Building, having moved from the Gibraltar Building.

The new Kansas City Milling Co., which succeeded the defunct Kaull Milling Co., has enlarged its offices in the New York Life Building.

Donald F. Priest, who has been connected with Dilts & Morgan has been sent to Atchison, Kan. to manage the office of the company there.

Frank O. Everts has been admitted to membership on transfer from H. A. Merrill. The membership was sold at \$11,500, including transfer fee.

L. B. McBride has applied for membership in the Board of Trade on transfer from Thos. R. Smith and E. O. Moffatt has been elected to membership.

The J. C. Lysle Milling Co. expects to have its new plant in operation Oct. 1. The building has been completed and the machinery is now being installed.

Members of the Board of Trade voted Aug. 15, to rescind the rule permitting trades in bids and offers, formerly called "puts" and "calls." No regular trading in these privileges has been carried on here for two or three years.

C. W. Avery, formerly with the Armour Grain Co., whose membership in the Board of Trade was to be turned over to John F. Sugrue, who is now with the same company, has decided to retain it and is now connected with the Shannon Grain Co. Mr. Sugrue has now applied for membership on transfer from T. P. Gordon.

Attacking the constitutionality of the statute under which Governor Hyde, dismissed him and installed a new chief grain inspector, James T. Bradshaw, former commissioner has applied for a writ of quo warranto before the supreme court to oust T. J. Hedrick, his successor. He asks to be permitted to serve out his term which expires in April, 1923.

E. O. Moffatt, who only a month ago announced his retirement from business and from the Moffatt Grain Co., just can't stay out of the game. He sold his membership but he is back again, having bot the membership of his son, J. D. Moffatt. Moffatt Jr. will go on the road as consignment solicitor and "father" will run the business. He has again been admitted to membership.

The entire plant of the Kelly Milling Co., including the mill, head house, elvtr., office and warehouses, burned at 12 midnight, Aug. 17. Lightning is said to have struck the mill but the fire did not break out at once and when discovered it was too late to save the plant. The elvtr. storage amounted to 300,000 bus. and the mill has a capacity for 2,500-bbls. The elvtr. was of cribbed type, of wood with metal covered and collapsed 15 minutes after the fire was discovered. The mill, of brick, also collapsed at the same time. The fire loss, including 30,000 bus. of wheat in the elvtr., 3,000 bbls. of flour in the warehouse and 3,000 sacks of feed, amounts to from \$400,000 to \$500,000. A blanket insurance policy is reported to completely cover the amount. The Acme Elvtr., operated by the Moore-Seaver Grain Co., and 200 ft. west of the burning plant was damaged to the extent of \$1,500, most of the loss being due to the constant stream of water poured over the house to save it. All that is left of the plant is a few smoldering pillars and twisted steel.

ST. LOUIS LETTER.

Philip H. Postel has applied for membership in the Merchants Exchange.

Fred M. Brown is now ass't gen. mgr. for the Geo. P. Plant Milling Co.

Arthur A. Holthaus, for the last 3 years ass't sec'y of the Merchants Exchange and, for 10 years connected with the secretary's office, has resigned and is now connected with an investment security company.

MONTANA

Hobson, Mont.—The Judith Milling Co. is repairing its plant here.

Sidney, Mont.—The Farmers Merc. Elvtr. Co. has not made repairs as reported.

Glendive, Mont.—A. J. Foss, formerly mgr. for the Farmers Grain Co., has resigned.

Raymond, Mont.—We have let the contract for our new elvtr.—Equity Exchange Co.

Baker, Mont.—I am now mgr. for the Geo. C. Bagley Elvtr. Co. here.—Chas. R. McClosky.

Glendive, Mont.—Our elvtr. here has a capacity of 35,000 bus., not 25,000.—Glendive Milling Co.

Three Forks, Mont.—The elvtr. of the Grain Growers Grain Co. burned recently and is a total loss.

Big Sandy, Mont.—Fire did serious damage to the elvtr. owned by the Farmers Produce Co. on Aug. 12.

Boyd, Mont.—A. W. Anderson is agt for the Occident Elvtr. Co. here, not T. V. McPherson, who is at Rapelge.

Ryegate, Mont.—We will not install a new dump as has been reported.—C. B. Smith, agt. Monarch Elvtr. Co.

Scobey, Mont.—The Scobey Grain Co. incorporated for \$15,000 by Otto B. Eglund, Wm. A. Lannon and H. J. Christenson.

Hamilton, Mont.—Farmers Co-op. Elvtr. Co., incorporated for \$50,000 by Peter Duns, J. Nordheim, and P. E. Lundquist.

Big Timber, Mont.—We have bot the elvtr. of the Montana Grain Growers and P. A. Miller is mgr.—Park County Milling Co., Livingston.

Coffee Creek, Mont.—The P. F. Brown Grain Co. has leased the elvtr. of the Farmers Elvtr. Co. for this season. Geo. Hayes will be mgr.

Richey, Mont.—A. J. Foss has succeeded A. D. Floyd as mgr. for the Farmers Co-op. Elvtr. Co. Mr. Foss was formerly at Glendive.

Rapelge, Mont.—I am agt. at this station and A. W. Anderson, listed as agt. here in the Rocky Mountain List, is at Boyd, where the list places me.—T. V. McPherson, agt. Occident Elvtr. Co.

Willow Creek, Mont.—The elvtr. of the Montana Grain Growers burned recently and is a total loss. There was no grain in the building at the time of the fire. It is believed that the losses are covered by insurance.

Great Falls, Mont.—The milling interests of the State Milling Co., at Manhattan, and the Cascade Milling & Elvtr. Co., have been consolidated. The latter has been operating the mill under lease. It also is the owner of the line of elvtrs. known as the State Elvtr. Co. New officers of the company are August Schwachheim, pres. and treas.; M. M. Moore, vice-pres.; and W. C. Boeke, sec'y and sales mgr., Hardy Jones, W. B. Taylor and Henry Mattson of this city with Ford Johnson of Helena, form the directors together with the officers, who are all of this city. The company will now operate as the Cascade Milling & Elvtr. Co.

NEBRASKA

Sterling, Neb.—A. Kusler is mgr. for the Farmers Equity Elvtr. Co.

Osceola, Neb.—J. E. Valk is now in possession of the Osceola Mill & Elvtr.

Lincoln, Neb.—We are liquidating our affairs.—M. T. Cummings Grain Co.

Stromsburg, Neb.—J. E. Pelton is now agt. for the Wright-Leet Grain Co.

Petersburg, Neb.—Geo. Markus has succeeded A. Brinkman as mgr. for the Farmers Elvtr. Co.

Elwood, Neb.—T. J. Hartman has resigned as mgr. for the Farmers Elvtr. Co., effective Sept. 1.

DeWeese, Neb.—Mail addressed to the DeWeese Grain Co. is returned marked "unclaimed."

Stamford, Neb.—Carl Berg, mgr. and F. Woodruff, ass't mgr. for the Equity Exchange, have resigned.

Johnson City, Neb.—E. E. Wirick, formerly agt. for the Duff Grain Co. here is now at Hillsdale, Wyo.

Trenton, Neb.—The Trenton Equity Exchange has moved its old building and will build a new one on the site.

Farwell, Neb.—The elvtr. of E. G. Taylor is being overhauled and put into shape to handle the wheat crop.

Steinauer, Neb.—Clifford Schroff of the Steinauer Milling Co., was recently married to Miss Joan Whalen of Lincoln.

Maxwell, Neb.—We have sold our elvtr. here to the Farmers Co-op. Ass'n.—Leypoldt & Pennington Co., North Platte.

Eddyville, Neb.—The elvtr. of D. Wort has been sold to H. S. and R. I. Clouse and W. A. Summers. H. S. Clouse is mgr.

Clearwater, Neb.—Homer McClellan, formerly mgr. for the Nye-Schneider-Fowler Co., has resigned and is now at Kimball.

Gresham, Neb.—P. Christenson has succeeded C. L. Gilbert as mgr. for the Farmers Elvtr. Co. Mr. Gilbert is at Bradshaw.

Amherst, Neb.—D. Wort is reported to have been compelled to close his elvtr. as it was filled to capacity and no cars are available.

Bradshaw, Neb.—C. L. Gilbert, formerly mgr. for the Farmers Elvtr. Co. at Gresham, has bot an elvtr. here and will operate it himself.

North Platte, Neb.—The Leypoldt & Pennington Co. has been completely reorganized. L. P. Japon is pres. and S. M. Souder, sec'y-treas.

Hastings, Neb.—T. E. Jackson has sold his interest in the Jackson Grain Co. and has moved to Salina, where he will enter the milling trade.

Edgar, Neb.—H. B. Storrs is now mgr. for the Shannon Grain Co. here. He was formerly mgr. for the Farmers Shipping Ass'n at Davenport.

Norman, Neb.—The Farmers Union Co-op. Grain & Supply Co. which was recently incorporated, has bot out the Farmers Business Ass'n.

Friend, Neb.—The mill of the Acme Mill & Elvtr. Co., owned by the Peter Stepanek estate, was recently struck by lightning but the damage was slight.

Sutherland, Neb.—We have just finished putting in an 8-in. steel loading spout and a Hall Distributor. Jas. Beveridge is mgr.—Farmers Union Co-op. Ass'n.

Homer, Neb.—The elvtr. of the Omadi Co-op. Grain Co. has been overhauled and repaired. A new foundation and new concrete floors were among the improvements.

Anselmo, Neb.—Wm. Dickson has bot the elvtr. of the Farmers Elvtr. Co. The company closed its elvtr. last March and it has been on the market ever since.

Ogallala, Neb.—We have sold our elvtr. here to the Combined Elvtr. Co.—Leypoldt & Pennington Co. (Chas. Snoddy, former mgr. for the old company, has been retained as mgr. for the new.)

Stella, Neb.—The elvtr. that we bot at McCandless (Nemaha, p. o.), was purchased from the trustee in bankruptcy. It will be managed thru this office.—Ralph Garvillis, mgr., Harry Brenner.

Fremont, Neb.—It is reported that the Nye-Schneider-Fowler Co., which has been in financial difficulties, but which was recently re-organized, will soon be in possession of the \$2,000,000 loan it asked of the banks of Omaha and Chicago.

Fremont, Neb.—W. R. Wilson, founder in 1880 of the Nye-Wilson Morehouse Grain Co. which later became the Nye-Schneider-Fowler Grain Co., died Aug. 3, at his home here. He came to this city and entered the grain business in 1870. He was actively interested in the trade for many years.

Beverly, Neb.—Wm. Sharp is now mgr. for the Beverly Equity Exchange, succeeding me, Aug. 1. The new elvtr. of the company now being built is of cribbed, metal clad construction and has a capacity of 12,000 bus. It will be equipped with the first Bird Scale ever installed in this vicinity. Other equipment includes Beall Cleaner, dump, manlift and 15-h. p. F.-M., type "Z" Engine.—Irving Magorian.

NEW ENGLAND

Hartford, Conn.—We have not as yet sold our plant to the Conn. Agri. Exchange, contrary to all reports.—David Garber, treas. Garber-Northam Grain Co.

Hamden, Conn.—Thos. E. Hadden of New Haven, is said to be the head of a new company which has bot a site here and will build a grain elvtr. and coal sheds.

New Haven, Conn.—We have the same old officers altho we have incorporated our partnership. R. G. Davis is pres., H. H. Davis, vice-pres. and mgr., and L. L. Davis, sec'y and treas.—R. G. Davis & Sons, Inc.

Boston, Mass.—Henry B. Moore, who recently retired from active business and went to California to live, is back east again but has nothing to say as to business. He was a member of J. P. Soper & Co. for years.

Easthampton, Mass.—J. A. Sturgis, Miss F. M. Curran and Merton S. Clark have formed J. A. Sturgis & Co., a partnership which combines the business of J. A. Sturgis and that of Prentiss Brooks & Co. The business of the latter firm will be discontinued and headquarters will be in the old Sturgis company's offices. The company will operate the B. & M. mill and warehouse as heretofore.

NEW MEXICO

Clayton, N. M.—The Union Grain & Elvtr. Co. was dissolved several months ago.

San Jon, N. M.—We expect to install a cleaner and loader very soon.—R. C. Mundell, San Jon Elvtr. Co.

Clayton, N. M.—The Four States Seed Co. is the only regular buyer at this market, as the Blair Grain Co. has been out of business for the last 6 months.

NEW YORK

Freeville, N. Y.—The Farmers Syndicate Inc. has been organized with a capital stock of \$200,000 to build a 50,000-bu. elvtr. feed mill and mixing plant.

Buffalo, N. Y.—The affairs of the Mutual Elvtr. & Mutual Terminal Co. have been settled and the office of the company in the Chamber of Commerce Building is now closed.

Rochester, N. Y.—Albert H. Case, formerly treas. of the Van Vechten Milling Co., died Aug. 6, after an illness of 2 years. He was 52 years old and was widely known in the grain and milling circles.

Oswego, N. Y.—The recently organized Ontario Milling Co. will open its offices in the Downey Building Sept. 1. The building has been fitted up and is ready for operation. The company will take over all the patent rights of the defunct Oswego Milling Co.

NEW YORK LETTER.

Robert M. Morgan has resigned his associate membership in the Produce Exchange and is now a full fledged member.

Geo. Rossen has succeeded J. B. Smull as vice-pres. of the Produce Exchange, Mr. Smull retiring on account of the pressure of other business.

Applicants for membership in the Produce Exchange are: Julius C. Friedrich, Fred E. Andrews, Chas. LaDue, P. J. McCulloch, and Jinosuke Minami.

M. M. Marks is now mgr. for the National Grain Corp. in this city and represents it on the exchange. He was formerly in business on his own account.

Garrit Spillenaar, ass't to Joseph M. Nugent now mgr. for the W. V. Waverens Grain & Import Co. of Holland at its New York office, has been elected to membership in the Produce Exchange.

Woolsey A. Moran, Chas. E. Deming, Aaron D. Braham, Andrew C. Ely, Nicholas J. Campion, Arthur T. Smith, S. Grann-Myer and Theo. F. Ismert are new members of the Produce Exchange.

Chas. W. Chilcotte is now acting solicitor for the J. Rosenbaum Grain Co. on the exchange here. He was formerly with Knight & Co. and later with E. F. Leland & Co. at this market. He is an applicant for membership in the exchange.

NORTH DAKOTA

Binford, N. D.—I am mgr. for the Binford Grain Co.—Ludwig Amundsen.

Beulah, N. D.—Our mgr.'s name is Fred, not Frank Klein.—Farmers Elvtr. Co.

Nome, N. D.—The Nome Co-op. Grain Co. has been incorporated for \$25,000.

Barton, N. D.—Fire completely destroyed the elvtr. of Langum & Simonson, Aug. 4.

Deep, N. D.—It is reported that the elvtr. of the Dodge Elvtr. Co. burned recently.

Zap, N. D.—L. A. Jackson has succeeded Geo. Riesland as mgr. for the Farmers Co-op. Co.

Cummings, N. D.—The Cummings Equity Elvtr. Co. will increase its capital stock \$10,000.

Halliday, N. D.—Ernest Ingvald, formerly at Taylor, is now mgr. for the Farmers Elvtr. Co. here.

Rolla, N. D.—The Farmers Grain Co. is reported as incorporated here with a capital stock of \$15,000.

McVie, N. D.—M. F. Swanston has bot the elvtr. of the H. E. Zeeveld Grain Co. and is operating it.

Balta, N. D.—I am now located at Sunburst, Mont.—Wm. Halter, formerly mgr. for the Balta Grain Co.

Taylor, N. D.—E. Ingvald, formerly mgr. for the Farmers Elvtr. Co., has resigned and is now at Halliday.

Berthold, N. D.—F. E. Carlson of Ambrose, has succeeded N. E. Norman as mgr. for the Farmers Elvtr. Co.

Ransom (Cayuga p. o.), N. D.—Mail addressed to C. K. Thorstad, formerly mgr. for the Farmers Elvtr. & Trading Co., is returned marked "removed."

Lehr, N. D.—The Eichhorn Elvtr. Co. experienced a small loss when lightning struck the building recently.

Velva, N. D.—The Farmers Elvtr. Co. has increased its capital stock from \$10,000 to \$50,000. R. Feige is mgr.

Binford, N. D.—I am now agt. for the Monarch Elvtr. Co. here.—A. T. Larson. (L. Amundsen was formerly mgr.)

Portal, N. D.—The elvtr. of the Standard Grain Co., reported bankrupt last December, burned recently with a loss of \$5,000.

Richardton, N. D.—F. A. Weist, formerly mgr. of the Equity Exchange at New England, is now mgr. for the Farmers Equity Co. here.

Stirum, N. D.—The Farmers Elvtr. Co., whose elvtr. was recently damaged by a cyclone, now has a new elvtr. built by the T. E. Ibberson Co.

La Moure, N. D.—The Andrews Grain Co. opened its elvtr. here, which has been closed for the last 3 years. R. Theiring of Valley City is mgr.

Rutland, N. D.—I am still mgr. here and we have never heard of H. A. Stageland.—E. J. Hoel, mgr. Farmers Co-op. Elvtr. Co. (Mr. Stageland was reported to have succeeded Mr. Hoel.)

Kathryn, N. D.—The elvtr. and mill of the Farmers Mutual Elvtr. Co. are fireproof and operated by electricity. The 50-bbl. mill is operated in conjunction with the 50,000-bu. elvtr.

Ludden, N. D.—Both of our elvtrs. were destroyed in the tornado of July 2. Our new 35,000-bu. elvtr. is nearly completed now. D. F. Hoag & Co. had the contract.—J. M. Bleibaum, mgr. Ludden Co-op. Co.

Powers Lake, N. D.—I have been transferred to the elvtr. at this station, as our company will not rebuild the elvtr. at Langford, S. D., which burned recently and of which I was mgr.—R. E. Shoemaker, agt. National Elvtr. Co.

Mannhaven, N. D.—The Farmers Elvtr. Ass'n which recently bot the elvtr. of the Bismarck Elvtr. & Grain Co., will move the elvtr. about 15 ft. from its present site as it is on the river's edge and sudden rises, etc., might cause a large loss.

Fargo, N. D.—We are opening our branch office here for the convenience of country shippers around here who handle barley and other grains. D. H. Stuhr will be in charge of the office, which will specialize in barley.—R. J. Johnstone, Federal Grain Co.

Hazen, N. D.—M. J. Pridt is mgr. for the Hazen Grain Co., not the Farmers Elvtr. Co., of which I am mgr. The former company has bot the elvtr. of the Knife River Grain Co., which has been closed for 2 years and is operating it.—L. G. Smith, mgr. Farmers Elvtr. Co.

De Lamere, N. D.—The new 40,000-bu. elvtr. of the Equity Elvtr. & Trading Co., replacing the house burned recently, is now under construction by the T. E. Ibberson Co., which has the contract. The equipment includes 2 legs, cleaner and truck dump. The plans also call for a fireproof engine room and office.

Eckman, N. D.—The elvtrs. of the Canton Grain Co. and the Northland Elvtr. Co. burned Aug. 10 with a loss of \$25,000. The depot also burned. The fire is thot to have been started by tramps near the Canton Elvtr., as it burned first and the flames spread to the other buildings. A car of wheat, one of coal and 5 empties were also destroyed.

Turtle Lake, N. D.—The Farmers Elvtr. & Trading Co. has let contract to the T. E. Ibberson Co., for a 40,000-bu. elvtr. to replace the house burned July 26. It will be equipped with the most modern machinery and will be on a concrete slag foundation. It is also reported that the elvtrs. of the Occident Elvtr. Co. and the Andrews Grain Co. which burned at the same time will be rebuilt.

OHIO

Portsmouth, O.—The Strittmatter Grain Co. has discontinued here.

Hamilton, O.—Robt. A. Campbell has bot a site for an elvtr. here. He is from Richmond, Ind.

Lancaster, O.—The Mondhank & Raab Co. has nearly completed its 6,000-bu. tile elvtr. here.

Camden, O.—The Farmers Grain & Supply Co. has doubled its capital stock making it \$40,000.

Genoa, O.—The C. A. Powers Grain Co. is in financial difficulties and is in the hands of a trustee.

Columbus, O.—The Early & Daniel Co., of Cincinnati, has opened an office here with Ellis Early in charge.

West Sonora, O.—The elvtr. formerly owned by H. A. Klepinger, is now operated by the recently incorporated Klepinger Grain Co.

Whitetail (Metamora p. o.), O.—Farmers are organizing to build an elvtr. or buy that of H. Buseb, who contemplates selling or exchanging his house.

Bellevue, O.—The A. M. Tousley Milling Co. has leased the elvtr. and mill of the defunct Bellevue Flour Mills Co. from H. E. Young, trustee.

Delphos, O.—The Garman Grain Co. has been reincorporated for \$40,000 by A. L. Garman, Harry M. Viel, Wm. Deuch, G. G. McCoy and Ed. E. Truesdale.

Rising Sun, O.—The Farmers Commercial Grain & Seed Co. has completed its 18,000-bu. concrete elvtr. Equipment includes: 15 and 35-h. p. motors and Richardson Scale.

Toledo, O.—Frank Moorman of the Crumbaugh-Kuehn Co., wears a perpetual smile these days. Moorman, Jr., arrived a few days ago and mother and baby all doing well.

Ashville, O.—We expect to cover the elvtr. with galvanized iron next month so that it will be weatherproof. It will also reduce fire risk.—G. C. Cline, mgr. Ashville Grain Co.

Marysville, O.—Our mill was recently struck by lightning but hand extinguishers put out the blaze and our only damage was to the roof and amounted to about \$50.—Spurrier Bros.

Le Moyne, O.—The Lucky Farmers Elvtr. Co. has completed its concrete elvtr. and is installing the machinery. The elvtr. is all concrete with the exception of the cupola, which is of wood.

Enon, O.—We took over the elvtr. of the Miami Conservancy Co. recently. We now have a storeroom and cement storeroom and block office to replace the wood office still standing.—Beard and Beard.

Berkey, O.—The Berkey Elvtr. Co. is occupying its new 21x29 ft. office. A new addition on the lumber shed has given additional 336 sq. ft. A 20x24 garage cares for our two trucks.—C. G. Gormley, mgr.

Malinta, O.—The Gleaners Clearing House Ass'n of Grand Rapids, Mich., has nearly completed its 30,000 bu. elvtr. It will be equipped with cleaner, Fairbanks 100-bu. Hopper Scale, sheller, wagon scale, etc.

Goodwin, O.—Axel and Catherine White have sold their interests in John Wickenheiser & Co.'s company to the other stockholders. The company will continue to operate here as in the past, under the old name.

Waverly, O.—It is reported that the Waverly Farmers Co-op. Co. has given up its charter because of its inability to sell stock to the required amount among the farmers. It was recently reported incorporated for \$10,000.

Melvin, O.—Custes & Barrett now operate the former Channel Elvtr., having leased same July 1st from Jim Anderson of Washington C. H. Mr. Curtis sold his interest in the firm of Ewing & Curtis the 1st of the year.—W. A. Ewing.

Circleville, O.—We recently bot the elvtr. at Kinderhook, O., and will build a new 10,000-bu. house at New Holland. We also bot the elvtr. of Rife & Morris here and have put up a new house at Island Road.—H. M. Crites & Co.

Xenia, O.—C. H. Ervin has bot the interest of Herbert Davis in Ervin Bros. & Davis, including the interest in what was formerly known as the "Smart" elvtr. Mr. Davis is retiring on account of ill health after 25 years in the trade.

Wapakoneta, O.—Elmer Sheets, prop. of the Wapakoneta Grain Co., has added to his line of elvtrs. and now operates houses at this station, Waynefield, Botkins and Gutman and Geyer, both St. Johns p. o. The two last he recently bot from C. M. Dobie.

Akron, O.—Chas. J. True, for 14 years connected with the Quaker Oats Co. here and at Chicago, etc., has entered the grain feed and flour brokerage business on his own account. Mr. True writes: I have entered the feed, flour and grain business here on a brokerage and jobbing basis, covering Northeastern Ohio. We do a carlot business only and do not have a warehouse.

Columbus, O.—The Columbus Grain & Hay Co. has succeeded O. W. Cook & Co. After Mr. Cook's death, July 11, Mrs. Mabel Stevenson who has been a partner in the business since its organization and Omer Snyder, who formerly operated an elvtr. at Tremont City, O., formed the above firm which will continue the policies of the old. The old offices will be retained.—M. C. Stevenson.

Grove City, O.—The Grove City Farmers Elvtr. Co. has nearly completed its new 21,000-bu. concrete elvtr. It is equipped with four F.-M. Motors, McMillan Dump, manlift, U. S. Sheller, Howe Wagon Scale, Richardson Auto Scale and ear corn band buckets. It has two stands of elvtr. legs, 12x7 and 8x5, with V-shaped buckets. The doors are of wood, covered with crimped steel. It will be completed in about 10 days. Ballinger & McAllister did the work. The Farmers Exchange expects to build a mill also.

Chillicothe, O.—The Farm Buro which has been organizing a company to operate the Camp Sherman Elvtr. better known as the "Weidinger Elvtr." has repudiated its lease from the government for the elvtr. at an annual rental of \$201 and will refuse to sign the final papers leasing the elvtr. unless the government installs the alternating current motors required by the light and power company before it will furnish power to run the elvtr. The directors of the new company have decided that they will not go to the expense of furnishing the new required motors. The estimated cost of them is about \$2,000. The lease, it is stated, calls for an elvtr. fully "equipped" and one that has no motors for which power can be secured is not regarded by the local company as being so equipped.

Toledo, O.—The following changes in the public elvtr. charges became effective Aug. 15: Rule 31, Sec. 2, Elevation from vessels, including 10 days free storage, 1/2c per bu. was 3/4c per bu. Reduced 33 1/3%; storage charge on grain per bu. 1/30c per day was 1/20c per day. Reduced 33 1/3%. On grain that remains in store from December 1 to May 1, a winter storage rate of 1c will be allowed, equal to an additional 30 days. Charge for making direct transfer, same grain, same car; wheat, corn, oats, rye, and barley, 1/2c per bu. plus loading charge, was 3/4c per bu. Reduced 33 1/3%. The allowance for grain screenings is nothing at present as there is absolutely no demand and the elevators are trying to get rid of them to anybody who will haul them away, but can't do it. There may be a little inquiry later. —A. A. Gassaway, sec'y Produce Exchange.

Deshler, O.—A state-wide Farmers' Elvtr. Picnic will be held under the auspices of the State Mgrs.' Club on Labor Day, Sept. 5th. The program is in part as follows: Registration, 10 a. m.; business session of mgrs., 11 a. m.; picnic dinner, noon; debate, "Resolved, That the plan of marketing grain as proposed by the U. S. Grain Growers Inc. will be more beneficial to the grain growers than the present method as now in operation by the Chicago Board of Trade." Affirmative, C. S. Latchaw, Defiance; H. W. Robinson, Cleveland. Negative, H. O. Barnhouse, representative of Chicago Board of Trade; also another representative of Chicago Board of Trade, 1 p. m.; Ball Game, State Mgrs.' Club vs. Toledo Produce Exchange, 3 p. m. A prize of \$10 will be paid to the mgr. attending this picnic from the farthest point from Deshler. A prize of a box of cigars will be given to the winning ball team.

CINCINNATI LETTER.

The McQuillan & Riley Hay & Grain Co. has succeeded the McQuillan Co.

Thos. Dugan, for many years in the grain trade here, has engaged in the grain and hay business on his own account and will operate as the Thos. Dugan Grain Co. with offices in the Rawson Building.

Frank Maguire, who has been ill is steadily improving at his summer home in Biltmore, N. C., will never forget his 50th birthday. Over fifty of his friends and associates on the exchange remembered him with a letter of good wishes on that day.

Chas. S. Maguire is the oldest active member of the grain trade at this city and his many friends and associates recently remembered his 80th birthday by presenting him with a beautiful basket of flowers. Mr. Maguire thanked the members of the exchange and then passed cigars.

B. H. Wess, of the Wess Hay & Grain Co., was elected mayor of St. Bernard, a suburb, for the 5th time. He won by a vote of 655 to 403.

OKLAHOMA

Kiowa, Okla.—H. G. Hancock is a member of the Service Grain Co. with C. E. Warner.

Kingfisher, Okla.—The Farmers Elvtr. Co. is installing mill machinery.—Oklahoma Mill Co.

Jefferson, Okla.—R. M. Wharry has succeeded D. H. Gerhardt as mgr. for the Grange Co-op. Ass'n.

Kingfisher, Okla.—I bot the Pennington Elvtr. here and Dale Johnston is in charge.—W. B. Johnston.

Amarillo, Tex.—We have changed our headquarters to this city, not to Oklahoma City, as reported.—D. C. Ewing Grain Co.

Enid, Okla.—We have moved from the Commerce Building to the Enid Nat'l Bank Building.—P. P. Miller, mgr. Miller & Grain Co.

Ninnekah, Okla.—We have bot the elvtr. of the Moore Grain Co. and will not rebuild our house, burned July 12.—J. F. Bird, mgr. Ninnekah Grain & Lbr. Co.

OKLAHOMA CITY LETTER.

Our headquarters are at Amarillo, Tex., not in this city, as reported.—D. C. Ewing Grain Co.

We buy for the Sun Grain & Export Co., Guthrie, Okla. Also for Clement Grain Co. of Waco, Tex. We are extensive handlers of wheat, oats and corn, but specialize on the two first mentioned grains.—Alexander-Jacobson Co.

Oklahoma City, Okla.—This is to advise that on account of the heavy excess profits tax charged against corporations, the Cherokee Grain Co. is being transferred from a corporation to a co-partnership, there being no change whatever in the personnel of its business or otherwise.—Willis G. Sautbine, pres.

OREGON

Yamhill, Ore.—The Yamhill Milling Co. filed an involuntary petition in bankruptcy on Aug. 13. Liabilities are placed at \$45,000 and assets at \$35,000.

PENNSYLVANIA

Highspire, Pa.—The Highspire Milling Co., Inc., is located here and not at Harrisburg, as reported.

Pittsburgh, Pa.—C. C. Cole, formerly with the Jesse C. Stewart Co. of this city, is conducting a grain brokerage business at Charleston, W. Va.

Harrisburg, Pa.—We have a mill, feed mill and grain elvtr. in this city on the P. & R. At Mechanicsburg we are putting up a mill, feed mill and grain elvtr. of 15,000 bus. capacity and at Williams Grove, we operate a 10,000-bu. elvtr. We are incorporated for \$300,000.—Hoffer & Garman, inc. (It is reported that H. H. Greybille has bot a part of the interest of Henry R. Garman in the firm.)

PHILADELPHIA LETTER.

Geo. A. Magee & Co. have new offices in the Bourse Building.

Geo. A. Stuart, who has been in charge of the grain inspection at the Bourse, has been appointed grain expert for the state Buro of Markets.

Wm. B. Stiles, Geo. G. Omerly and Philip R. Markey have been appointed as the settlement com'te of the Commercial Exchange, on off grade corn.

The oats settlement com'te of the Commercial Exchange has changed the settling differences on off grade oats and at present they are 2 to 3c for No. 3 white oats and 4 to 6c on No. 4 white oats.

SOUTHEAST

Beaufort, S. C.—Central Terminal Co. will equip its elvtr. with a Hall Signaling Grain Distributor.

Charleston, W. Va.—C. C. Cole, formerly with the Jesse C. Stewart Co. of Pittsburgh, Pa., is selling grain here for the account of shippers and conducting a brokerage business.

SOUTH DAKOTA

Chester, S. D.—Jerry Ryan has purchased an elvtr. at this station.

Tyndall, S. D.—E. W. Giedd of Tripp, S. D., has bot an elvtr. here.

Langford, S. D.—The National Elvtr. Co. will not rebuild its elvtr. here this year.

Irene, S. D.—The elvtr. of C. E. Gunderson was recently badly damaged by fire.

Huron, S. D.—The elvtr. of the Davis Grain Co. has been sold to Sheldon F. Reese who is now in charge.

Webster, S. D.—The new elvtr. of the Miller Grain Co., replacing the house burned in May, is now under construction.

Sinai, S. D.—E. E. Clement is the mgr. of the Sinai Elvtr. Co. which is a combination of the two old companies here.

Langford, S. D.—R. E. Shoemaker, formerly mgr. for the National Elvtr. Co. here, which burned recently is now at Powers Lake, N. D.

Davis, S. D.—C. P. Enright of Elk Point, S. D., is now mgr. of the Farmers Elvtr. Co. at this station. J. R. Ellis held the position of mgr. for several years.

Wallace, S. D.—Andrew Faehn, mgr. for the Farmers Elvtr. Co. here, died recently at a hospital in Watertown, Minn. He is survived by a wife and 5 children.

Yankton, S. D.—Because the Missouri River endangers the operation of trains over tracks leading up to its elvtr., the Western Terminal Elvtr. Co. has suspended operations for a time.

Badger, S. D.—Farmers Co-op. Grain Co. incorporated for \$50,000 by Andrew Norgaard and E. F. Glaive, of Arlington, O., O. A. Stordahl of this city and D. H. Carlson, of Lake Preston.

Aberdeen, S. D.—An overheated motor, coming on one of its busy days, for a time threatened the elvtr. of the McCaull-Webster Co. The prompt arrival of the Aberdeen fire department is all that saved the plant.

Volga, S. D.—J. C. Sundet, who bot the elvtr. of Dyball & Cotton, some time ago has sold a half interest in the business to Geo. O. Cotton, one of the members of the old firm. Mr. Cotton will take an active interest in the work.

Chelsea, S. D.—We have just built a new \$6,000 office and engine room. We have also installed a dump scale and an outside coal scale. The recently incorporated Chelsea Grain Co. has bot the elvtr. of the Pacific Elvtr. Co.—Geo. A. Hurd, Farmers Elvtr. Co.

Seneca, S. D.—We have repaired our elvtr. and put on a new cupola, replacing the one blown off, July 2. Have also installed new cup belt and other improvements. G. W. Van Dusen & Co. have also repaired their elvtr. damaged at the same time.—Farmers Elvtr. Co.

Sioux Falls, S. D.—I am entirely out of the grain business and have been since the elvtr. of J. E. Arnold burned May 10. I was operating the elvtr. at the time. Mr. Arnold will not rebuild this year but it is his intention, I believe, to build another house next year.—W. A. Mills.

TENNESSEE

Union City, Tenn.—The mill of the Walker-Dahnke Milling Co. will be sold at public sale, Sept. 12 by the trustee, H. C. Stanfield.

TEXAS

Waco, Tex.—The elvtr. of B. E. Clement burned recently.

Lark, Tex.—The 250,000-bu. elvtr. of Lester Stone & Co. has been completed.

Tulia, Tex.—The Farmers Grain Co. has increased its capital stock to \$75,000.

Amarillo, Tex.—Mail addressed to the C. B. Cozart Grain Co. here is returned "unclaimed."

Finney Switch (Planview p. o.), Tex.—My 10,000-bu. elvtr. has been completed.—L. J. Halbert.

Houston, Tex.—The Beatty-Archer Co. incorporated for \$25,000 by W. R. Archer and E. J. Christman.

Cooper, Tex.—S. H. Nelson and Will Anderson, operating as the Nelson-Anderson Co., are engaged in the grain business here.

Fort Worth, Tex.—C. M. Carter will operate here as the C. M. Carter Grain Co. He is pres. of the Carter Grain Co. of Bay City.

Clarendon, Tex.—The hay sheds of the Clarendon Grain Co. burned at 8:30 p. m., Aug. 12, with a loss of 75 tons of hay. It was partially insured.

Houston, Tex.—The elvtr. of the Houston Mill & Elvtr. Co. at this point has been completed. It has a capacity of 500,000 bus. and is in operation daily.

Knox City, Tex.—We are out of the grain business and have been succeeded by the Knox City Grain Elvtr. Co.—J. H. Davis, of J. H. Davis Elvtr. Co.

Whitewright, Tex.—We bot the property of the Phoenix Elvtrs. at Sherman and have moved our headquarters to that city. We still operate here and at Wolfe City.—Kimbell Milling Co.

Wichita Falls, Tex.—Fred Loomis, formerly with the Wichita Milling & Elvtr. Co. here, is at present in charge of the laboratory of the Interprovincial Flour Mills Co., Ltd., at Saskatoon, Can.

Ft. Worth, Tex.—G. C. Mountcastle, senior member of the Mountcastle-Merrill Grain Co., passed away at his home in this city on Aug. 14 at the age of 73. Mr. Mountcastle had long been identified with the grain trade in this section of the country, having been there off and on for the past 51 years. Mr. Mountcastle also had the honor of being one of the oldest members of the Kansas City Board of Trade. For several years he was manager of C. F. Orthwein Sons Co. Kansas City office and later built and operated their plant at Fort Worth. When the Orthwein company discontinued business, Mr. Mountcastle became southwestern manager of the J. Rosenbaum Grain Co. He held this position until 1915, when he engaged in the grain business with R. I. Merrill at Fort Worth as the Mountcastle-Merrill Grain Co. Mr. Mountcastle was the first pres. of the Fort Worth Grain & Cotton Exchange, the organizer of the Fort Worth Freight Bureau and was prominent for many years in all matters pertaining to the grain business in the Southwest. His death will be keenly felt by his many friends throughout the country. The business of the Mountcastle-Merrill Grain Co., which has been very successful thru the efforts of Mr. Mountcastle and Mr. Merrill, will be continued as formerly, with R. I. Merrill actively in charge.

UTAH

Millville, Utah—The plant of the So. Cache Milling Co. was totally destroyed by fire on Aug. 2.

Kaysville, Utah—The Utah Milling Co. has succeeded the Kaysville Milling Co. and the Layton Milling & Elvtr. Co. The company now plans the rebuilding of the conveyors destroyed when the Kaysville mill burned last year.

Tremonton, Utah—The Mutual Fuel & Feed Co., whose incorporation was recently announced, says of its plans: We do not anticipate the erection of an elevator or the installation of machinery and for the present are shipping in carlots only.

WASHINGTON

Harrington, Wash.—J. P. Hosenbaugh is mgr. for the Northern Grain & Warehouse Co. here.

Molson, Wash.—Wm. McCrory, has succeeded W. R. Messamore as mgr. for the Molson Union Elvtr. Co.

Oakesdale, Wash.—The Palmerton-Moore Grain Co. was the lessee of the elvtr. burned July 23 at this station.

Wilbur, Wash.—One of the bins of an elvtr. here was burned when fire was started by sparks of a threshing machine. Nearly 1,000 bus. of wheat were destroyed.

Spokane, Wash.—M. B. Mikkelsen for many years mgr. for the Lewiston Mlg. Co. and T. A. Riggs, mgr. for the same company at Portland, have formed the Mikkelsen & Riggs Grain Co. with offices here and at Portland, Ore.

Endicott, Wash.—The Neil Bros. Grain Co., of Spokane, has leased the elvtr. and warehouse of the Community Milling Co., recently reported as in the hands of receivers, for a year, the grain company to pay 25c a ton for storing grain in the house.

Olympia, Wash.—As the office of the chief grain inspector has been abolished in this state, and under the new law the work must be taken care of by the Agricultural department, R. R. White, of Sumner, chief clerk of the dept. has assumed the duties of the position.

Davenport, Wash.—Bain Smith has succeeded Almer McCurtain as mgr. for the Farmers Union Warehouse Co., Mr. McCurtain resigning after 9 years of service.

WISCONSIN

Beaver Dam, Wis.—We will operate as Lange & Hartzheim.—L. & H.

Wausaukee, Wis.—I am out of the grain business.—H. C. Christ.

Cylon, Wis.—We operate the elvtr. of the New Richmond Roller Mills Co., giving us 2 elvtrs. here.—Art Martin, mgr. Cylon Elvtr. Co.

Racine, Wis.—We operate a grain elvtr. and will engage in the grain business in addition to the feed and flour business. We also have a feed mill and warehouses.—Bell City Milling Co.

Manitowoc, Wis.—The Bartlett-Frazier Co. is repairing Elvtr. "A" and putting it into first class condition for the immediate handling of grain. The house has been closed for about a year.

Random Lake, Wis.—The Random Lake Co-op. Ass'n decided to expend from \$7,000 to \$9,000 in remodeling the company's plant. A grain elvtr. and coal shed will be built and the storage building remodeled, to take care of the company's fast growing business.

Oconto, Wis.—The Alphonse Pierre Corp., grain dealers, filed a voluntary petition in bankruptcy in federal court Aug. 10. Mr. Pierre, pres., also filed a voluntary petition in his own behalf. The corporation lists liabilities of \$106,763.26, with assets of \$26,117.87. Mr. Pierre lists personal liabilities of \$111,376.24, with no assets except memberships in clubs and associations.

MILWAUKEE LETTER.

R. H. MacMasters is a new member of the Chamber of Commerce.

P. P. Donahue is now chairman of the supervisors of grain inspection and weighing of the Chamber of Commerce.

Samuel Hazelhurst, for several years a member of the Chamber of Commerce, passed away in the east, following an operation.

Milwaukee, Wis.—Bernhard Stern & Sons' mill was threatened by fire when a spark, supposedly from a passing locomotive, started a small fire on the roof.

The replacement of Elvtr. A by the C. M. & St. P. Ry. Co. is now reported under consideration. The old elvtr. was destroyed by fire in January and was under lease to the Donahue-Stratton Co.

Clement Bournique, the 14 year old son of Lyman Bournique of the Taylor & Bournique Co., died in Paris, France, recently of acute appendicitis. The body will be cremated and brot home for burial by Mr. Bournique, who was traveling with his son in Europe.

By resolution adopted by the Chamber of Commerce, Aug. 25, 200,000 bus. storage in Rialto Elvtr., operated by Donahue-Stratton Co., and 300,000 bus. storage in Elvtr. "E," operated by Taylor & Bournique Co., are declared "regular" warehouses for the storage of grain and flax seed under the rules of the Chamber of Commerce for the period ending Aug. 1, 1922.

The elvtr. of the Ladish Milling Co. burned Aug. 20, at 6 p. m., with a loss of \$150,000. The rest of the plant, containing 750,000 bus. of grain, was saved, but both grain and buildings were damaged by water. According to officials, who estimated the loss, a great share of the damage was to the building. Seven carloads of grain owned by the Donahue-Stratton Co., stored in the elevator, were destroyed. The elvtr. was built 25 years ago and was formerly the Bay View plant of the American Malting Co.

THE CONTINUED BUYING of hard winter wheat by the governments of Brazil and Uruguay for shipment to South America is a new element of considerable significance. It is reported that so far six cargoes of this wheat has been booked for South American shipment. As evidenced by the exceptionally light exports from Argentine, Canadian and American wheat now appears to have no serious foreign competition and sales are restricted only by the difficulties in arranging satisfactory credits abroad.—Quinn-Shepherdson Co.

Points on Railroad Claims.

By OWEN L. COON.

The actual and not an estimated loading weight should always be placed on the B/L and for the following reasons: If a shortage claim arises it may be necessary to start a suit to enforce collection. If the trial of a suit on such a shortage claim becomes necessary, the shipper must introduce evidence as to the amount of grain loaded into the car. To make a trip to the place where the suit has been started is often inadvisable because of the expense involved and the frequently small size of the claim. One way that such can be avoided and the suit be more easily prosecuted is to obtain an order of court previous to trial (such can be obtained at least in Illinois) whereby the defendant is ordered to produce at the trial the original bill of lading. The weight shown on the bill of lading is always taken as true and correct in the first instance until one side or the other proves the contrary.

If the actual loading weight is placed on the B/L when the car is shipped all that need be done under the above plan to prove the loading weight is obtain a court order previous to trial ordering the railroad to produce the original B/L. When the railroad produces the document the actual loading weight automatically is introduced in evidence.

A little care in placing the actual weight on the B/L will make it far easier for shipper's attorney to successfully handle any claim that arises where trial of the suit on the claim becomes necessary and will enable him to have his claim prosecuted without going a number of miles to personally introduce in court evidence as to what the loading weight actually was.

Inspector Fowler Suspended.

Walter F. Fowler, assistant chief grain inspector at Omaha, Neb., after a hearing by Supervisor Fred S. Smith in the directors' room of the Grain Exchange Aug. 10, was placed under suspension of license pending a decision by the Sec'y of Agriculture.

Mr. Fowler is nearly 65 years of age, was chief grain inspector at West Superior, Wis., and thereafter for several years one of the federal grain supervisors at Chicago. The formal charges against him were that he had "improperly graded 6 cars of wheat," and that he had "issued instructions to several deputy inspectors resulting in misgrading of wheat."

His defense was that it was done in the rush of business, but the record showed that practically all of the errors were in the one direction of grading yellow wheat as hard winter. The deputy inspectors testified that Fowler had instructed them to be lenient in grading yellow berry wheat.

Nebraska on this crop has very much yellow berry wheat and 47 cars erroneously graded went from Omaha in one movement to another market.

Mr. Fowler resigned the office of assistant chief at the request of the directors of the Grain Exchange Aug. 17. Chief Inspector George Powell is not involved, and the deputies excused their misgrading by fear of losing their jobs if they did not follow Mr. Fowler's instructions.

Mr. Fowler is thoroly well posted in the grain business and learned the grain inspection profession before federal supervision was heard of, in the days when the chief at each market was the king who could do no wrong and interpreted the rules to suit himself. All the old-time grain inspectors were forced to do this by the vague phraseology then in vogue such as "reasonable" and "not good enough for."

ITALY HAS RAISED a bumper crop of wheat this year amounting to 188,125,000 bus., compared with a yield last year of but 141,336,000 bus.

Feedstuffs

CAMERON, WIS.—The Cameron Flour & Feed Mill is now in charge of Christ Holman.

ASHER, OKLA.—The feed mill of Thomas Jackson has been purchased by H. F. Henderson.

BELLAIRE, O.—The buildings of the Eastern Feed & Supply Co. were damaged \$25,000 by fire on Aug. 2.

KING, N. C.—The King Milling Co. contemplates the installation of new feed grinding machinery.

CLEVELAND, O.—The recently incorporated Murphy Roberts Co. will do a feed and flour brokerage business.

PORT DOVER, ONT.—The Regal Poultry Feed Co., Ltd., has been incorporated with a capital stock of \$40,000.

NASHVILLE, TENN.—The feed manufacturing plant of the Ritter-Hennings Co. was recently damaged \$25,000 by fire.

NORWALK, CONN.—Nicholas Sarantos & Louis Maskalens have bought the feed business of Samuel Roodner for \$35,000.

OLYMPIA, WASH.—Brewer & Co., in the wholesale feed business here, have increased their capital stock from \$15,000 to \$20,000.

NEOSHO FALLS, KAN.—F. M. Denney who recently completed a new feed mill contemplates the installation of a flour mill at a later date.

STANWOOD, WASH.—The Stanwood Grain Co. has equipped its feed mill with additional storage bins which will accommodate several carloads of grain.

MINNEAPOLIS, MINN.—Shipments of oil cake from this market during July amounted to 15,541,311 lbs., compared with 15,607,665 lbs. shipped during July 1920.

SOME DEALERS feel that the dry weather in the eastern and northeastern states cutting the hay crop will materially increase the demand for their feeds this winter.

JANESVILLE, WIS.—F. H. Greene & Co. have sold the retail section of their business to Graham & Farley, a firm which operates retail feed stores at many Wisconsin towns.

GALVESTON, TEX.—The Humphreys-Godwin Co. contemplates the erection of a large grinding and sacking plant for the handling of cottonseed meal and cottonseed cake for export.

COLUMBUS, O.—O. L. Carlson is to be the Ohio representative of Hales & Hunter, of Chicago, and will maintain an office in this city. C. B. Henderson is the local representative of the company.

COLUMBUS, WIS.—Guy V. Dering, long in the feed, grain and coal business in this city, has taken over the produce business of R. J. Lueders and will conduct it in conjunction with his other interests.

EVANSVILLE, IND.—Ziliak & Schaefer are constructing a feed mill at a cost of \$20,000. George Lamey, manager, says that when the plant is finally completed it will be one of the largest plants this side of Chicago.—C.

ITALY'S customs officials on Aug. 18 were given orders to permit the exportation of residues from the manufacture of corn, dried packinghouse residues and various meals, all of which may be used for cattle feeding.

THE FEED INDUSTRY in Canada is being investigated by Will Hill of Vancouver, B. C., a representative of the Dominion Government. Mr. Hill will endeavor to learn the extent to which Canadian mills use screenings and similar substances in the manufacture of their feeds. The investigation grew out of a recent conference held between the Canadian Millers Ass'n and representatives of the Dominion government.

AUBURN, WASH.—Of the nature of the business it will conduct from its new warehouses, the Smith Feed Co. writes as follows: "We expect to do a lot of grain shipping and will operate a feed warehouse in connection."

JAMESVILLE, N. Y.—Alleging an attempt to cancel a contract for alfalfa at \$30 when the price fell to \$14 L. Sevier a nearby farmer has filed suit against the Farmers Alfalfa Products Co. of this city for \$2,755 damages.

CEDAR RAPIDS, IA.—Alex Siniako & Son of Madison, Wis., have taken over the Jackson Grain & Feed Co. in this city and will conduct it as the Iowa Milling Co. specializing in the manufacture of chicken feeds and corn meal.

MEMPHIS, TENN.—Feed dealers here evidently are using but little barley chaff in their products. Three cars of barley chaff on track at Chicago were recently offered free to any dealer in Memphis who would pay the freight with no takers.

CRESTON, IA.—The warehouse of the Creston Flour & Feed Co. is now in charge of A. J. Nord and his brother. Mr. Nord comes from Minneapolis where he has been connected with the Interstate Flour & Feed Co. and the Bergmann Millfeed Co.

MISSISSIPPI FEED DEALERS have been warned by the Mississippi Department of Agriculture that they should be careful not to place the tax stamp across the analysis label. It also asks that the dealers use more care in preparing their statements of analysis.

E. C. EIKENBERRY, of Camden, O., is to be the new pres. of the reorganized Nutritia Company. Mr. Eikenberry has for a number of years been in the grain business at Camden with his brothers and was also at one time pres. of the Grain Dealers National Ass'n.

CINCINNATI, O.—The property we acquired here adjoins our grain elevator and hay house and we expect to improve it with an additional warehouse and to enlarge our plant for the manufacture of sweet feeds for horses, mules, cattle and poultry.—The Cincinnati Grain & Hay Co. A. L. Heile, pres.

LITTLE ROCK, ARK.—Fire which originated in a near by lumber yard recently spread to an elevator used as a feed warehouse by the Hayes Grain & Commission Co., the Cunningham Commission Co., and the J. F. Weinmann Milling Co. The damage to the feed, amounting to \$5000, was fully covered by insurance.

TENNESSEE manufacturers of feed were visited during July by three representatives of the state department of agriculture. The department has a scheme of card indexing every dealer in the state. The report of the investigators was that there was a general disposition on the part of the feed men to obey the law.

KANSAS CITY, MO.—The equipment of the new plant of the Quisenberry Feed Manufacturing Co. now under construction will include one Allis Feed Mill, two Allis Scalpers, two Allis Dust Collectors, a 500-bu. Fairbanks Hopper Scale and a Richardson Automatic Scale for weighing poultry feeds and a Eureka Separator. The Lehrack Construction Co. is doing the work.

WISCONSIN'S NEW FEED LAW which will go into effect Dec. 1. contains many parts that are objectionable to the Wisconsin feed dealer. Some of the provisions of the law follow: A flour mill having a capacity of 200 bbls. or less shall pay \$5 for each brand of flour mill offal manufactured, the larger mills to pay more according to their size; mills other than flour mills that manufacture feed shall, when of less than 50 tons capacity pay \$5 for each brand of feed manufactured, the larger mills also to pay more; once each year the feed manufacturer must report the amount of feed made and the name of his agents and shall pay a tax of 5 cents on each ton of feed manufactured.

CHICAGO, ILL.—Chapin & Co. have withdrawn from membership in the American Feed Manufacturers Ass'n and R. W. Chapin has handed in his resignation as pres. of the organization. Mr. Chapin gave the following reason for his action: "Chapin & Co. will not belong to any ass'n that is dominated and controlled by any special interests nor have I desire to be pres. of a trade organization that sacrifices its educational program for politics." H. G. Atwood, vice pres. of the Ass'n therefore automatically became pres. Mr. Atwood is connected with the American Milling Co., a large Peoria feed manufacturer. Prof. F. Minkler, who has been director of publicity for the ass'n and who has done much to spread useful feed information, is no longer with the Ass'n, having handed in his resignation after the directors announced that not sufficient money was available for such work.

IN BELGIUM the control of the importation of wheat will be discontinued on Sept. 1. Grain dealers will, however, have to report their transactions to the Minister of Commerce and Supplies. The restrictions on the importation of flour will also be removed but the Minister announces that the government still reserves the right to control flour imports in the case of dumping or other harmful practices.

Want Mill Oats As a Trade Name.

Several representatives of the milling and grain trade have appealed to the Sec'y of Agriculture to ask that the words "mill oats" be continued in use as a trade name under the regulations of the Bureau of Chemistry.

It seems that a difference has arisen between the Bureau of Chemistry and the Bureau of Standards regarding the use of the term "mill oats" for "volunteer oats."

Sec'y Husband of the Millers National Federation, contends that the phrase "mill oats" had been in use for 30 years and that a change now would impair the value of the product because the dealers would ask a larger profit for the handling of an unfamiliar product.

Underweight Sacks Subject to Seizure.

The Western Grain Co., of Birmingham, Ala., delivered to Chas. A. Jones & Co., of the same place, 22 sacks of "Daisy Horse and Mule Feed," one of which contained 109 lbs., while 21 sacks varied in weight from 88 to 98 lbs., all under the 100 lbs. called for by the label, the total weight being 2,084 lbs.

A representative of the Commissioner of Agriculture and Industries undertook to forfeit the entire lot as being underweight. In reversing a decision of the lower court the Supreme Court of Alabama said:

If one sack was 9 pounds under weight, and 21 of proper weight or practically correct, it would be a strong, if not conclusive, circumstance of no intent to have an underweight and no intent to deceive or defraud.

In this case there is one sack out of 22 over weight, and 21 out of 22 under weight, reducing it per ton about 100 pounds. This, with no evidence to the contrary, impresses the court that the weight of the 21 sacks is false, that it was done to deceive or defraud, and, it being for sale or offered for sale, should be condemned and forfeited to the state of Alabama.

The design of the act is to make the label on a sack of feed speak the truth as to its contents. If it does not, and the proof shows it was done to deceive or defraud, and it was kept for sale or offered for sale, it is subject to condemnation and forfeiture to the state.

The court below ordered the 22 sacks to be delivered to Charles A. Jones & Co., the claimants, from whose place of business it was seized.

The court should have ordered the sack weighing 109 pounds delivered to Charles A. Jones & Co., and the other 21 sacks forfeited to the state of Alabama and provided for their delivery to the Commissioner of Agriculture and Industries as the law directs. Acts 1919, p. 1069, § 11; Acts 1919, p. 88, § 2.—88 South. 422.

Supply Trade

Sharon, Pa.—Business in the Sharon-Youngs-town steel districts is rapidly improving, a few of the mills running 80% capacity.

Minneapolis, Minn.—C. A. Weaver, in charge of the Sprout, Waldron & Co. office in this city, has had Wisconsin added to his territory.

New York, N. Y.—Judge Gary on Aug. 23d announced that the Steel Corporation would meet the lowered prices on sheet and tin plate of the independent companies.

New York, N. Y.—The new building of the Dodge Sales & Engineering Co., located at 49 Park Place, is practically completed. The basement of the building and the first three floors will be used by the company as a warehouse. The remaining 9 floors will be rented.

No. Kansas City, Mo.—The Standard Steel Works have just completed a new plant at 16th and Holmes St., and will move into it this month. The new plant is a modern, daylight factory building of steel and brick construction, and will be devoted to the manufacture of a complete line of mill and elevator equipment.

New York, N. Y.—A general wage cut in all the plants of the U. S. Steel Corporation was put in operation on Aug. 23. The low selling price of steel as compared with the high cost of production was assigned as a reason for the wage cut. The general rates of day labor will be cut to 30c an hour and other wages and salaries will be reduced in proportion.

Chicago, Ill.—The ability of wood working factories outside this city to get ample help at a lower wage scale has shut out competition from Chicago factories, with the result that the factories belonging to the Woodworkers Employers' Ass'n, together with some members of the Cabinet Mfrs.' Ass'n and some from the Millmen's Ass'n have, after a strike of nearly three months, elected to run their factories on the open shop, or American plan, paying a somewhat higher scale of wages and running nine hours per day, fifty hours per week, and have joined together under the name of the Chicago Ass'n of Wood Industries.

STATISTICS show that during the past 10½ months U. S. mills have produced about 27 million barrels of flour less than for the corresponding period last year. Our exports decreased 9 million barrels. This leaves us 18 million barrels shy on disposition of flour at home. Canada has taken from us about 1 million barrels of our trade, leaving 17 million short of last year's consumption. This shows clearly why milling business has been "rotten" for so many months. Our wheat exports since July 1, 1920, were 440,501,000 bus. vs. 279,736,000 the preceding year, including flour. —Frank H. Tanner, secy., Ohio Millers Ass'n.

Big Order for Grain Cleaners.

Repeat orders bring great satisfaction to manufacturers and salesmen; and they are the best of all testimonials. These facts, if there were nothing more, would be cause for a feeling of gratification on the part of the officers of the Huntley Mfg. Co. on the recent receipt of an order for 20 No. 11 Monitor Oat Clippers and 12 No. 11 Monitor Warehouse Separators for installation in the C. & N-W. Ry. Co.'s Calumet Terminal Elevator at Chicago to replace those damaged by the dust explosion March 19th.

Machines of the same type were placed in the plant when it was built a few years ago, and the reorder is convincing evidence that they rendered satisfactory service. The purchase of the equipment represents the largest single order for grain cleaning machinery placed in recent years. The sale was made by A. D. McPherson, Chicago representative for the manufacturer, and he is to be congratulated for his success in bringing to his company for the second time the order.

Failure of Crusher to Give Guaranteed Output.

The Williams Patent Crusher & Pulverizer Co. sold to J. W. Lyon of Leesburg, Ga., a machine guaranteed to crush approximately per hour thru a ¾-inch cage 2,000 to 5,000 lbs. of corn, cob, velvet beans, etc.

Lyon alleged that the machine would not and did not crush more than 500 lbs. of velvet beans or 600 lbs. of corn per hour. On complaint a different cage was sent but still there was no increase in the capacity. The builder of the machine also failed to send, as promised, a representative to Leesburg to put the machine in order to perform the guaranteed capacity.

Lyon paid one draft for \$570 and on his refusal to pay the balance the builder brot suit and was given judgment because Lyon had failed to place the machine on board car, forward the B/L to the seller and pay freight charges both ways, as provided in the contract. Lyon's defense that he signed the contracts without reading them and was given no duplicate copy did not excuse him from liability on the contract, the Court of Appeals of Georgia held, in 107 S. E. 590.

FRANCE is slowly returning to her normal position as a wheat producer. The crop for 1921 is expected to total far above the crop of 1920 when 229,880,000 bus. were raised. In 1913, the year before the war, France raised 316,645,000 bus. of wheat and the average production for the previous ten years was 322,608,000 bus.

Patents Granted

1,386,158. Conveyor. Charles Candlot, Paris, France. This is an apparatus for the shaking of screens and similar devices. The shaking is done by means of a silently operating crank connection driven by a driving rod or shaft.

1,385,870. Pneumatic Conveyor. George Gieseler, Cleveland, O. In this conveyor the receiver for the material conveyed has an outlet which is fitted with an air lock. A spring at the lock co-operates with the lock so as to handle any irregular sized materials.

1,386,546. Automatic Scale. Richard Zanone, New Albany, Ind., assignor to the Weighing Machine Manufacturing Co., Louisville, Ky. In this weighing device a shaft is journaled within hopper. The shaft co-operates with the beam in weighing material automatically.

1,385,848. Air Separator. Thomas Sturtevant, assignor to the Sturtevant Mill Co., Boston, Mass. In this apparatus material is directed into air paths which move thru inner and outer chambers. The movement of the air thru the chambers is controlled by a system of valves provided.

1,385,718. Weight Indicator for Scales. Harry Schaper, St. Louis, Mo., assignor to the General Scale Co., St. Louis, Mo. The weighing device has a graduated scale for showing weights. A means is provided so the readings on the scale may be projected on a screen in a greatly enlarged form.

1,376,505. Means for Operating Reciprocating Screens. Richard Jacobsen, assignor to Jacobsen & Richards, Chicago, Ill. The screens in this device are given their reciprocating motion by the revolving of a pulley stationed below. The pulley is set off center and is revolved by a belt attached to power.

1,385,964. Seed Separator and Grader. William Van Etten, Bellingham, Wash. A mechanism to control the volume of seed flow, a vibrating seed sieve and a brush screen comprise the principal parts of this device. The sieve and brush screens are so attached that their respective movements may be readily adjusted.

1,385,847. Air Separator. Thomas Sturtevant, Wellesley, Mass., assignor to the Sturtevant, Mill Co. This double chambered separator permits a current of air to pass up an inner chamber and down an outer chamber. The air current is provided by an overhead fan and materials separated are discharged thru separate spouts.

1,386,506. Self Weighing and Sacking Machine. James Jewell, Kansas City, Mo. This is a device which comprises a hopper within which is contained a weighing device. The hopper has a discharge opening controlled by a sliding door. A connection is maintained between the weighing device and the sliding door by means of a system of rods.

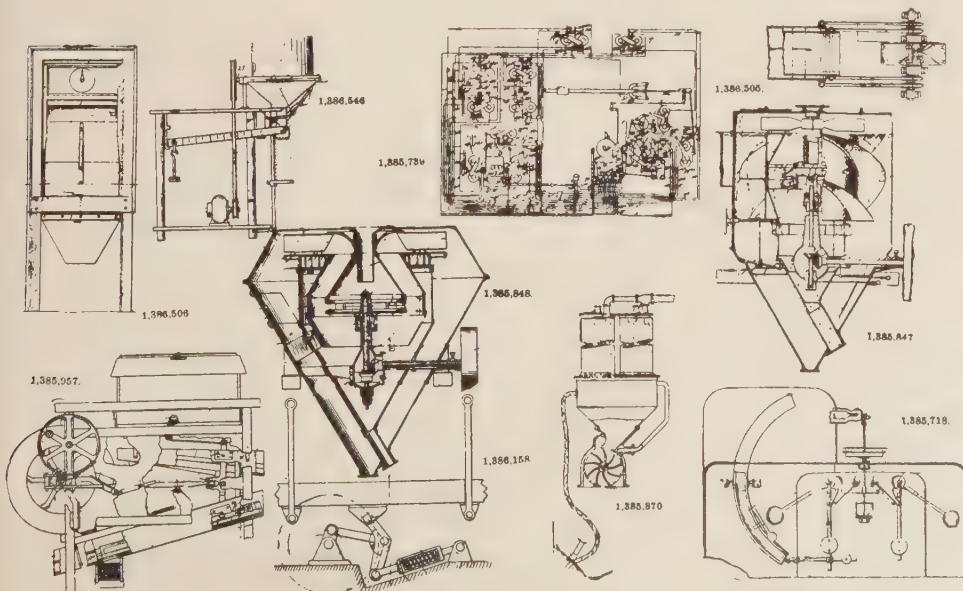
1,385,739. Weighing Machine. Arthur Adams, Brookline, Mass., and Frank Emery, Ashmont, Mass., assignor to the Boston Scale and Machinery Co. A beam, a poise, a weight indicator and the mechanism for their operation are controlled by an electric key or connection. The connection cannot be broken until the weight shown on the indicator is recorded.

The Erie Fools the Arbitrary Unions.

The domination of the union representatives over the shop affairs of the various carriers seems to have been solved by the Erie. When the Erie rearranged its car shop working schedules at Marion, O., recently the workers all walked out.

Consequently the Erie leased its shops at Marion to a private concern which in the future will handle the car shop work of that road. The carrier now expects to have its work done with dispatch and without interference from labor agitators and without bother from the U. S. Labor Board.

As the private concern which has taken over the shops will not be under the jurisdiction of the Labor Board which regulates the pay of rail workers, the Erie will no doubt effect a material saving in money as well as time and worry.



Grain Carriers

THE ERIE Barge Canal is having one of the busiest grain handling seasons in its history.

FREIGHT CARS temporarily out of service on the American railways on Aug. 8 numbered 13,137 cars fewer than were out of service on July 31.

NUMEROUS CANCELLATIONS of orders for cars have been reported by the division freight agent of the Missouri Pacific who is stationed at Hutchinson.

LOADED WITH WHEAT destined for Montreal from a Lake Superior port, the steamer Beaverton ran aground in the St. Lawrence River near Alexandria Bay.

METAMORA, O.—The electric freight line here gives the best service of any line in the United States, steam lines included.—S. L. Rice, Metamora Elevator Co.

A REDUCTION of 7½ cents per hundred on grain moving for export from Minneapolis has been made in line with the reductions recently announced from central markets.

NEW ORLEANS, LA.—All export carload freight moving through New Orleans via the I. C. is now in charge of A. E. Scalfie, import and export agent, Stuyvesant Docks.

BOSTON, MASS.—The steamer Lady of Gaspe is ashore on the Thrum Cap Shoals and vessel operators believe her cargo of feed, flour and other merchandise will be a total loss.

PERMISSION to reduce its rates on grain from Missouri River crossings and the middle west to the Gulf ports has been asked of the Interstate Commerce Commission by the Missouri Pacific.

THE NEW ORLEANS joint tariff bureau recently predicted that the 7½ cent reduction in rates on grain to the eastern ports would put the southern ports out of business as far as grain exporting was concerned.

EASTERN CARRIERS are now giving consideration to the matter of a reduction in domestic rates according to an announcement made Aug. 22 by J. S. Brown, mgr. Transportation Department, Chicago Board of Trade.

RATES ON RICE moving from Louisiana producing sections to milling points and then to points in other states have been declared unreasonable by Examiner Kephart in a tentative report to the Interstate Commerce Commission.

ST. LOUIS, Mo.—The recently established local office of the U. S. Shipping Board has sent a message to the trade asking them to ship their products in American vessels at the same rate foreign vessel owners are charging.

GRAIN LOADINGS on American railways for the week ending July 23 amounting to 64,919 cars were nearly 100% more than the loadings during the corresponding week in 1920 and established a record for cars loaded with grain in any one week.

TO DEMONSTRATE the shipping advantages of Philadelphia to the trade ass'ns in various parts of the country the Philadelphia Department of Wharves, Docks and Ferries is having a film made which shows how Philadelphia cares for its exports.

THE HIGH RATES for the shipping of wheat from the states of Utah and Idaho were recently condemned by H. W. Prickett, manager of the Traffic Bureau at Salt Lake City. He claimed that the present rate of 55 cents for shipping a bushel of wheat from Logan, Utah to Galveston, is nearly as much as the producer receives for it. He says the many uncultivated Utah wheat farms are a result of the high freights.

IN A MESSAGE TO CARRIERS dated Aug. 12 the American Railway Ass'n says of the movement of grain: In loading to agricultural territory, select cars as far as possible that are suitable for grain or which can be made suitable for grain with light repairs.

LOWER OCEAN FREIGHT rates on grain and other products moving from the Argentine to this country will no doubt be in effect if the predicted rate war between the Stinnes ship interests and the alliance of the United American and Hamburg alliance comes to a head.

TO MEET THE COMPETITION of the water carriers the Southern Pacific has asked permission to lower its rates on rice and several other commodities moving between San Francisco and Los Angeles to less than the rate charged on the same products between intermediate points.

PERMISSION to reduce its grain rates from Missouri River crossings and middle west territory to the Gulf ports 7½ cents per hundred has been asked of the Interstate Commerce Commission by the Missouri Pacific Ry. The M. P. says that it wants the new rate so that it will be able to compete with the eastern roads in handling export freight.

THE RECORD CARGO of 415,000 bus. of wheat was carried from South Chicago on Aug. 19 by the steamer Westmount. It was loaded by the Armour Grain Co. from South Chicago Elevator C. and was destined for Depot Harbor on Georgian Bay. The largest previous cargo of wheat carried was 404,000 bus. by the Russell C. Hubbard.

THE ANNOUNCEMENT OF REDUCTIONS in rates on grain for export has started other industries to ask the same thing. In a petition presented to the chairman of the Eastern Trunk Line Ass'n, the Institute of American Meat Packers points out that since the rates on grain and steel have been reduced the rates on export meat should be similarly reduced.

WITHDRAW THE REQUEST made recently for an equalization of the grain rates to New Orleans and Galveston is the suggestion sent to the Panhandle Plains Chamber of Commerce by the Galveston Commercial Ass'n. The plea for the equalization sent to the Interstate Commerce Commission appeared on page 221 of the Aug. 10 number of the Journal.

THE APPLICATION of the carriers to reduce their rates to the east 7½ cents per hundred and to reduce their rates ex-lake from Buffalo 5 cents per hundred was officially approved by the Interstate Commerce Commission on Aug. 18 in spite of the strenuous opposition of various western and southwestern roads and will probably become effective about Aug. 29.

WHETHER OR NOT a carrier has the right to abandon a line of railway and thereby make worthless the property of a grain elevator located thereon is soon to be threshed out in the courts. Because the Big Four contemplates the abandonment of a line which serves their elevator and mill at Zionsville, Ind., M. A. Roby, C. W. Cook, L. M. Miedlinger and R. P. Bundy have filed suit for damages against the Big Four because of the loss they will suffer by losing their shipping facilities.

GRAIN LOADINGS of lake vessels have been steady of late and vessel owners report several charters for grain loading during late August and early September at rates ranging from 1¾ to 2½ cents. Vessel men who have been handling grain between various ports during the past season say grain vessels are getting good dispatch at Buffalo and that the elevators are in good shape and could take care of a much heavier volume of business. Of the facilities at Port Colborne they say they are encountering some delay on account of the slowness of the grain moving out because of the shortage of Welland canal tonnage.

OCEAN SHIPMENTS of wheat and flour during the autumn and the early winter will aggregate 150,000 tons according to L. L. Bates of Seattle, Wash., foreign agent of the Admiral line which operates a large number of shipping board vessels. Mr. Bates attributes the revival of the market for these two commodities to the application of the freight rate of \$6 per short ton which became effective July 7.

EVANSVILLE, IND.—The steamer Jo Horton Fall has entered the lower Ohio river trade and will make two trips a week between Evansville and Cairo, Ill. At Cairo the steamer will connect with the government controlled barge line that operates between St. Louis and New Orleans. It is the first time in years that shippers along the lower Ohio river have had thru connection to New Orleans by water.—C.

A BILL introduced into the Senate by Senator Pittman of Nevada would take away from the Interstate Commerce Commission its right to impose long and short haul rates under the Esch act. The Senate Committee on Interstate Commerce previously had refused to include the suggestion of Mr. Pittman in the proposed refunding bill. Senator Pittman says the passage of his bill will end the present discrimination in railroad rates against the middle west.

GRAIN MOVEMENT thru the St. Lawrence is on the increase. The Lachine canal which carries vessels around rapids in the river near Montreal accommodated more grain vessels during July than during any other month in its history. The July movement follows with comparisons: Wheat, 2,536,453 bus.; corn, 7,313,000 bus.; oats, 869,000 bus. compared with a movement during July, 1920, of wheat, 1,485,000 bus.; corn, 85,000 bus., and oats 18,000 bus.

GULF VESSEL OPERATORS on Aug. 12 announced a reduction in their rates on grain. The rate on wheat and corn to western European ports will be 22½ cents, compared with 25 cents charged heretofore. The rates on oats, barley and flour were correspondingly reduced. To Hamburg the rates will be 2½ cents per hundred higher than the rates to other ports in western Europe. Rice rates from the Gulf to ports in the neighborhood of Marseilles, France, have also been reduced. The new rate will be 50 cents or 15 cents less than before.

ALL the wild ideas of unbalanced agitators the world over, in their ignorant and pitiable quest for happiness thru revolution, confiscation of property, and crime, cannot overthrow the eternal truth that the one route to happiness thru property or government is over the broad and open highway of service. And service always means industry, thrift, respect for authority, and recognition of the rights of others.—W. G. Sibley.

Idaho's Commission Decides on Car Capacity.

"Unreasonable and unlawful" is the decision of the Idaho Commission on the practice of Idaho carriers supplying 100,000 capacity cars on a 60,000 capacity order and charging the shipper for the 100,000 capacity car.

The Gooding Milling & Elevator Co., Gooding, Ida., shipped two carloads of bulk barley from Gannett, Ida., to Gooding, Ida., also a carload of sacked wheat from Tuttle, Ida., to Gooding. Complaint was made to the Commission to learn if the carrier could charge on the basis of the 100,000 capacity car furnished when a 60,000 capacity car was ordered.

The Commission said, Carriers furnishing a car of larger capacity than ordered at their own convenience should not be permitted to do so at the expense of the shipper.

Passing of a Pioneer Firm.

The fact that an individual or an organization has been continuously engaged in a single line of business activity for a period of time passing the half century mark is interesting in any land. In America it takes on added significance when we consider that it means that the beginning was made when much of our territory was wilderness with little or none of its present day advancement. America itself is so young, comparatively, that the business institution of fifty years standing is rare, and it is for this reason if for none other that the passing of a 53-year old grain firm holds something of interest for every man who is engaged in handling the grain crops of this country.

It was in 1867 that N. P. Williams, R. J. Mitchell and J. E. Railsback organized the firm of Williams, Railsback & Co. at Minier, Ill. A few years later they were joined in their enterprise by John F. and James F. Quigg. This company built the first building for handling grain in the section, a warehouse thru which grain was handled by scooping.

The first elevator at the station was built on the C. & A. right of way by Samuel Smith about 1869. This was later sold to the firm of Williams & Quigg, which had been formed by Mr. Williams and the brothers Quigg when they withdrew from the original organization some time previously.

The second elevator at Minier, built in 1874 by W. E. Verry, was purchased a short time thereafter by the firm of Railsback & Mitchell, which had succeeded Williams, Railsback & Co. upon the withdrawal of Williams and the Quiggs.

For a number of years there existed the two firms, in keen business competition. In 1885 Mr. Williams died and S. S. Tanner came into the firm of Williams & Quigg, which then changed its name to Quigg Bros. & Tanner, under which it was operated until 1906.

In the latter year Mr. Tanner retired from the grain business and a reorganization of the two firms at Minier was effected. The new concern took the form of a corporation, Quigg, Railsback & Co., which has continued to do business up to the present time. G. J., J. C. and J. E. Railsback, John F. and B. F. Quigg and R. J. Mitchell were the persons active in this reorganization. John F. Quigg was active manager until his death in 1915, when his son, B. F. Quigg, succeeded him.

The early elevators were operated by horse power, this being long before the beginning of the successful application of internal combustion engines as prime movers for country elevators. When the old Illinois Midland, now the Vandalia, entered Minier, an elevator was built on its line by the partners; about 1878 the present elevator was erected on the C. & A.; and about this time, also, a plant was built at Bradley Station, now Tazewell, on the completion of the Illinois Midland thru that point. More recently this was replaced by the company's present elevator at Tazewell.

Quigg, Railsback & Co. has now retired from the grain business and the corporation has been dissolved. The elevators at Minier and Tazewell were sold to the Little Mackinaw Grain Co., an organization of farmers of the two communities. This company has been incorporated with capital stock of \$50,000.

This marks the passing of the oldest business firm in Minier, one of the oldest grain firms in Central Illinois, and one that has been owned and controlled by practically the same interests for 53 years, altho all of the organizers are now dead. It was started at a time when the country was emerging from the raw prairie of the Indians. It has for over half a century carried on the grain business successfully, always serving its com-

munity in an efficient and capable manner. It is said that the individuals did not lay up great stores of material possessions—few grain dealers do—but they left a community, and a nation the better for their having lived therein; a business better because they had left their stamp of service where other grain dealers who follow can read it and profit by the story of their work.

Protests Method of Car Distribution in Oklahoma.

In a complaint filed with the Oklahoma Corporation Commission the Buffalo Farmers Co-operative Elevator Co. of Buffalo, Okla., has protested against the method of car distribution in practice on the Santa Fe.

The complaint says that on one occasion the Wallingford Bros. Elevator Co. had 18,000 bus. of grain ready for shipment and the farmers elevator had 32,000 bus. ready. The Santa Fe is alleged to have given the Wallingford Co. 9 cars and the farmers 7 cars.

Railroad Presidents Refuse Demands of Rail Unions.

Thirty-three presidents of American railroads in session in New York City on Aug. 11 refused the plea of the Railroad Brotherhoods that the roads re-establish the wage scale in effect prior to June 30 and withdraw their pleas for wage reductions.

The following statement was made public by the executives: "After careful consideration of the questions at issue, we have decided that the secretaries should notify their brotherhoods that conditions make it impossible to meet their requests.

Achieved Co-operative Ideal Only to Have It Shattered.

Theodore Wade of the Bureau of Markets U. S. Dept. of Agriculture, addressed the Kansas State Board of Agriculture some time ago on "Co-operative Marketing." As an example of the most successful co-operation he recited the history of the raisin combination, not even omitting to mention its knock-out by the law. He said:

Raisins were selling for 7½ cents a pound. The raisin growers were mortgaging their farms, and the men that were going to buy would say, "What is the particular land worth?" At that time the growers met, and under the leadership of some bankers they said, "We will organize the growers when we get at least 75 per cent of them signed up that they will market their stuff FIVE YEARS thru this organization." The first year they sent and got an expert salesman, paid him a high salary, and spent \$75,000 in advertising their product. They were raising 6,000,000 pounds of raisins, and today they are raising 180,000,000. Last year they spent \$440,000 in advertising and this year \$600,000. The price has gone on advancing until it is now 10 cents a pound.

Any wholesale grocer or other person that wanted to handle their raisins had to give a written order for the amount he expected to use, that he agreed to accept and pay for, at the price left blank. The board of directors would sit around the table and fill in the price, and at that time had contracts for over fifteen million dollars' worth of raisins at any price they saw fit to fill in the contract. The Federal Trade Commission found they had guaranteed any purchaser against any loss whatever. They formed as tight a trust as the Standard Oil ever did, in my judgment. The Federal Trade Commission requested them to desist. They were a monopoly and wouldn't sell to the speculators. The Federal Trade Commission said, "We have no objection to you refusing to sell to the speculators, but we have objections against the method you are using in selling, whereby the other party has nothing to say about the price." Mr. Palmer, the head of the Department of Justice, got out an injunction, and they were obliged to agree to desist from their methods.

Now the Farm Bureaus want their combinations exempted from operation of law, reminding us of the old saw: "No scoundrel ever felt the halter draw with a good opinion of the law."

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Grain Dealers Journal
305 So. LaSalle St., Chicago, Ill.

Supreme Court Decisions

No Right to Inspection before Payment of Draft.—Buyer who agreed in order that payment for goods should be made by honoring draft attached to B/L had no right of inspection before payment unless entitled thereto by the custom of the business.—*Southwestern Milling Co. v. Niemeier*. Appellate Court of Indiana. 131 N. W. 831.

Replevin of Hay forming Part of Stack.—Where one purchased and paid for a certain number of tons of hay out of a certain stack, segregation of such amount of hay from the stack was not necessary to an actual delivery thereof, and purchaser could bring action in replevin to recover the same.—*Logan v. Cross*. Supreme Court of Oregon. 198 Pac. 1097.

Future Trade Not Gambling when One Party Intends Delivery.—By the law of Louisiana, a contract for the purchase or sale of cotton for future delivery, though void if both parties intended the transaction as a wager on the market movements, to be settled by differences, is not void if only one of them has that intention.—*Browne v. Thorn*. U. S. Circuit Court of Appeals. 272 Fed. 950.

Burden on Carrier to Explain Delay.—When evidence of unusual delay is adduced, a prima facie case of negligence is made out, and the burden then devolves on the carrier to explain the delay and to show it arose from some other cause than the defendant's negligence, or that of its agents or servants.—*N. Y., P. & N. R. Co. vs. Chandler*. Supreme Court of Appeals of Virginia. 106 S. E. 684.

Technical Language may be Explained by Parol Testimony.—Provision of contract for sale of sacks reading, "Apply usual quantity of differentials," could be explained by parol testimony that the prices stated were for lots of 1,000, but that, if sacks were shipped in greater quantities, a less price should be paid for them.—*Taylor Milling Co. v. American Bag Co.* Court of Civil Appeals of Texas. 230 S. W. 782.

Burden of Proof on Carrier after Non-Delivery.—When holders of Bs/L in an interstate shipment prove nondelivery of the goods to consignee, it is presumed that they have been lost by negligence of the carrier or its agents, and the burden of showing that loss resulted from some cause for which the initial carrier was not responsible is on it.—*Ostroff v. Eustis*. Supreme Court of New Hampshire. 114 Atl. 27.

Carrier Not Liable for Deterioration Due to Inherent Vice.—A carrier is not liable for injury to a shipment of meal because of the inherent vice of the article, which became heated merely as a result of transportation. In an action for damages to a shipment of meal, evidence held in view of war conditions insufficient to show negligent delay so as to render the carrier liable for its becoming heated.—*Hines, Director-General, v. First Guaranty State Bank of Aubrey*. Court of Civil Appeals of Texas. 230 S. W. 764.

Insurance by Mail.—Where the circumstances indicated that the parties contemplated that the acceptance of an application for insurance should be transmitted through the mails, the mailing of the policy, duly stamped and addressed to insured, was an acceptance of the offer contained in the application, which makes the contract binding on both parties, so that the insured could not defeat recovery of the premium on the ground that there was a failure of consideration, even though he never received the policy.—*State Ins. Co. v. Lock*. Supreme Court of Iowa. 183 N. W. 311.

Auditing Required Under Fidelity Bond.—Where a contract of insurance against losses through the embezzlement of an employee contains a warranty on the part of the employer that the books shall be inspected, audited, and verified at least once in three months, this requirement is met by an examination conducted by the officers of the employing company, and does not involve the employment of an expert accountant for the purpose; nor does the fact that the examinations that were made failed to disclose shortages that were afterwards found to have existed necessarily show any breach of the warranty.—*Mitchell Grain & Supply Co. v. Maryland Casualty Co.* Supreme Court of Kansas. 195 Pac. 978.

No Contract unless Confirmation Agreed To.—The appellant, claiming that he bought from the appellee 5,000 barrels of flour delivered in Chicago, and that only 4,000 barrels were shipped, bought 1,000 barrels at an advanced price, and sued to recover the difference. Held, that the evidence is sufficient to sustain findings to the effect that he had no contract with the appellee, because the minds of the parties never met on the terms of a contract, by reason of appellant's failure to agree to the terms of a written confirmation sent him.—*Clark v. Topeka Flour Mills Co.* Supreme Court of Kansas. 198 Pac. 935.

Title to Crop.—Where a contract for the sale of land by implication gave the purchaser right of possession, he was entitled to growing crops, cut and harvested before forfeiture, tho he had only an equitable title. Where vendor removed grain from land, his act in taking an elevator receipt in the name of himself and purchaser, who had been in possession, was such an admission of title in the purchaser as will carry to the jury an action of conversion by a third person claiming under a chattel mortgage given by the purchaser.—*First Nat. Bank of Galata v. Montana Emporium Co.* Supreme Court of Montana. 197 Pac. 997.

Suit for Excessive Charges Maintained during Federal Control.—Under Federal Control Act, § 10 (Comp. St. 1918, Comp. St. Ann. Supp. 1919, § 3115½j), allowing actions at law against carriers without defense that the carrier is an instrumentality of the federal government, an action to enforce an order by the Interstate Commerce Commission, requiring the carrier to repay to the shipper excessive charges collected before the government took control, can be maintained against the company during the period of government control.—*Vicksburg, S. & P. Ry. Co. v. Anderson-Tully Co.* Supreme Court of the United States. 41 Sup. Ct. Rep. 524.

Buyer's Right to Extend after Contract Time.—A contract for sale of grain to be delivered during a stated month, which provided: "If this contract, or any part of it, expires without being filled, take up disposition of balance with buyer by wire, phone, or letter. Buyer reserves right to accept or reject cars shipped after contract expires"—gave the buyer the right at his election to waive a breach by failure to deliver within the month, and an agreement made after expiration of the month extended the time for deliveries held not without consideration. Where, in an action by the purchaser for breach of a contract for the sale and delivery of grain, defendant pleaded as an excuse for failure to deliver or tender delivery that plaintiff had repudiated the contract, such defense raised an issue of fact for the jury.—*J. C. Shaffer & Co. v. West Tennessee Grain Co.* U. S. Circuit Court of Appeals. 271 Fed. 820.

Burden of Proof of Leakage in Transit.—Proof of weights of grain before and after shipment, when shown to have been carefully made and with proper apparatus, is presumptively correct, but, where the railroad company introduces evidence of mistakes, or other evidence tending to impeach the accuracy or reliability of the weights, or of the record of the weights made, the question of the correctness of the weights is for the jury and is a fact which the shipper must prove by a preponderance of the evidence. Where evidence is introduced to show that grain will shrink in weight during shipment, owing to loss of moisture content, it at once appears that the mere discrepancy in weight before and after shipment cannot be relied on alone to prove the actual loss of grain from the car, and the burden, therefore, of proving such shrinkage, or the reasonable limit of such shrinkage, and of making allowance therefor, is upon the shipper.—*Nye, Schneider, Fowler Co. v. C. N-W. Ry. Co.* Supreme Court of Nebraska. 182 N. W. 967.

Sunday Counts in Three Days Allowed for Diversion.

Childress Grain Co., Temple, Tex., plaintiff, v. Roach Grain & Elevator Co., Frisco, Tex., defendant, before the Arbitration Com'te of the Texas Grain Dealers Ass'n, composed of E. W. Crouch, W. H. Killingsworth and Joe E. Lawther.

The evidence shows that diversion on the car of oats was accomplished four days after arrival at Greenville. The car was ordered diverted on the seventeenth. B/L shows that diversion was actually accomplished on the twenty-first of the month. Excluding the seventeenth we construe that four days delay were occasioned by diversion.

The defendant contends that since Sunday was included as one of these four days, that diversion was accomplished within contract time as per our trade rules. The Arbitration Com'te holds a different view.

Based on Rule 7, which states "Any given number of days shall mean calendar days excluding date of sale." This rule further reads: "Cars diverted shall apply on contracts in accordance with date original B/L regardless of date of diversion, provided that more than three days' delay in transit has not been occasioned by such diversion."

The Com'te's decision is that this car was delayed more than three days, therefore not applicable on contract.

The plaintiff and defendant both agreed that price at which these oats were taken into account by Childress Grain Co., was in line with market price on that date.

Therefore we find for the plaintiff for amount claimed, \$277.78, and hereby order Roach Grain Co., to pay to Secretary H. B. Dorsey at Ft. Worth, Texas, promptly, the sum of \$277.78.

No Obligation on Buyer to Call Reinspection.

Kansas Milling Co., Wichita, Kan., plaintiff, v. Maney Export Co., Oklahoma City, Okla., defendant, before the Arbitration Appeals Com'te of the Grain Dealers National Ass'n, composed of Elmer Hutchinson, E. M. Combs, W. W. Manning, John S. Green and Joy M. Hackler.

Two cars involved in this case, No. 15905 and No. 17491, were shipped by the defendant to apply on sales to the plaintiff on destination weights and inspection. In invoicing the cars, however, the defendant attached Oklahoma City inspection certificates based on samples "furnished and represented by Maney Export Co. as having been taken from" these cars. On these certificates the following clause was stamped with rubber stamp:

"Please note our grade and test on this wheat. If this is not upheld on official inspection, please call reinspection and if necessary, appeal to sustain the grade.

Maney Export Co." Car No. 15905 inspected No. 4 dark hard 55 lbs. at St. Louis and was so applied on contract without reinspection or appeal. The Oklahoma City certificate on the car was No. 3 hard wheat 56 lbs. Car No. 17491 inspected at Wichita, Kan., No. 3 hard winter 57.5 lbs. and the same on reinspection and not appealed, against the Oklahoma City certificate of No. 2 hard winter 58 lbs.

The only question involved on these two cars is to what extent the rubber stamp notation is binding on the buyer, the plaintiff in this case. The Arbitration Com'te held that there is no obligation on the part of the buyer to call reinspection or appeal except as an act of courtesy to his seller, and that if the seller desires this service performed he should insure its performance through a properly appointed agent and instruct the buyer that cars grading below invoice must not be unloaded without explicit instructions. In this we fully concur as we do not believe the placing of this obligation upon the buyer is contemplated in any of our Trade Rules. He can assume it if he wishes.

On car No. 3175 on first inspection graded sample hard winter wheat 56 lbs., which information was promptly wired to the defendant by the plaintiff. It was later reinspected and graded No. 3 hard winter 56 lbs., which information was also promptly wired to the defendant with the statement that the plaintiff could not use it. The next day, with both messages in hand, the defendant wired instructions to divert to Kansas City. Late in the afternoon the same day he sent another wire to apply same on contract as it was contract grade. The plaintiff claims that the car was not applicable on contract and also that the car had been diverted on the previous instructions of the plaintiff. Without passing on the question of the applicability of the car, the defendant in effect accepted the rejection of the plaintiff on the car when he ordered it diverted to Kansas City.

We, therefore, affirm the decision of the Arbitration Com'te No. 1, in this case and assess the cost of this appeal against the Maney Export Company.

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Changes in Rates

As shown by tariffs recently filed with the Interstate Commerce Com'n the carriers have made the following changes in rates:

Western Trunk Lines, E. B. Boyd, agt., in Sup. 2 to 1P, a vacating order, gives the rules, regulations and exceptions to classifications. Issued on July 22.

C. & A. in tariff 2-H gives the switching and other terminal charges, also rules governing the absorption of switching, drayage and transfer charges applying at C. & A. stations, effective Sept. 19.

Illinois Traction System in 480-B gives the local, joint and proportional rates on grain and grain products between stations on the I. T. S. and Alton, East St. Louis, Venice, Madison, Granite City, Ill., and St. Louis, Mo., effective Sept. 7.

A. T. & S. F. in Sup. 5 to 5655-Z gives the joint proportional rates on grain, grain products and seeds from points in Mo., Kan., Okla., N. M. and Tex., also Superior, Neb., to Galveston, Houston, Texas City, Beaumont, Orange and Port Arthur, Tex., when for export, effective Sept. 10.

C. R. I. & P. in Sup. 5 to 27537-F gives the joint and proportional rates on grain, grain products, seeds, hay and broom corn from stations on the Rock Island in Illinois, Iowa, Kansas, Missouri and Nebraska to stations in Ill., Ind., Ky., Mich., N. Y., O., Pa., effective Sept. 9.

A. T. & S. F. in Sup. 12 to 5588-L gives the local, joint and proportional rates on grain, grain products, etc., between points in Kansas, Colo., Mo., and Okla., also Superior, Neb., and Joplin, Mo., and points in Ill., Ind., Ia., Kan., Mich., Minn., Mo., Neb., Okla., and Wis., effective Sept. 8.

C. R. I. & P. in Sup. 1 to 13207 gives the joint proportional rates on grain, grain products and seeds from Albright, Neb., Armourdale, Atchison, Kan., Council Bluffs, Ia., Kansas City, Mo., Leavenworth, Kan., Omaha, Neb., Omaha, Neb., and St. Joseph, Mo., to stations in Ill., Ind., Ia., Mich., and Wis., and connecting lines effective Sept. 14.

C. R. I. & P. in Sup. 13 to 29329-D gives the local, joint and proportional rates on grain, grain products and seeds between Albright, Neb., Armourdale, Atchison, Kan., Council Bluffs, Ia., Kansas City, Mo., Omaha, Neb., St. Joseph, Mo., So. Omaha, Neb., Sugar Creek, Mo., and stations in Ia., Kan., Mo., and Neb., on the Rock Island, Burlington, and the M. P. effective Sept. 8.

L. A. Lowrey, agent, in Sup. 12 to 20-K gives the local and joint terminal charges, rules and regulations from or to points within the Chicago district on inbound and outbound traffic, also the rules governing the intermediate service in freight traffic passing thru the Chicago district, effective Sept. 8.

C. R. I. & P. in Sup. 2 to 13207-I gives the joint rates on grain, grain products and seeds from Albright, Neb., Armourdale, Kan., Atchison, Kan., Council Bluffs, Ia., Kansas City, Mo., Leavenworth, Kan., Omaha, S. Omaha, Neb., and St. Joseph, Mo., to stations in Ill., Ind., Ia., Mich., and Wis., on connecting lines, effective Sept. 15.

C. R. I. & P., Sup. 6 to 19687-L naming joint and proportional rates on grain, grain products, seeds, hay, broom corn, from Mississippi River and other stations in Illinois, Iowa, Minnesota, etc., to Mississippi Valley points and other stations in Alabama, Arkansas, Florida, Louisiana, etc., has been rejected by the Interstate Commerce Commission. Pending reissue of this supplement, rates named in original tariff and Sup. 4 and 5 will remain in effect.—H. A. Tribel, Chief of Tariff Bureau, C. R. I. & P.

Would Equalize Rates to Galveston.

The T. & P., one of the large Texas carriers recently appealed to the Interstate Commerce Commission for permission to establish on five days' notice a rate of 32½ cents to New Orleans, La. This rate, if granted, would have had the effect of equalizing the rates on grain to Galveston and New Orleans.

However, the T. & P., in connection with the A. & S., the G. T. & W., the K. C. M. & O. and the R. S. & P. has filed a tariff which establishes the 32½ cent rates effective Sept. 15. Only on the complaint of some interested party will the I. C. C. suspend the operation of the new tariff.

Grain Rates From South Dakota Unjust.

The South Dakota Board of Railway Commissioners in a complaint filed with the Interstate Commerce Commission on Aug 16, condemns as unjust the rates and charges for the shipping of grain and grain products from points in South Dakota, compared with the rates charged to the same points on grain and grain products having their origin in Iowa, Nebraska, Minnesota and other western states.

The complaint specifically names the rates charged by the C. & N. W., the C. B. & Q., and the C. St. P. M. & O. Railways.

Reduce Ex-Lake Rates to Montreal.

In line with the recent reduction in rates on grain for export moving ex-lake to eastern U. S. ports the rates on grain for export moving ex-lake from ports on Lake Huron and Georgian Bay to Montreal have been correspondingly reduced.

According to J. S. Brown, manager of the Transportation Department of the Chicago Board of Trade, the rates became effective Aug. 9. The new rates, including cost of elevating from lake steamers to elevators and 15 days' storage in the elevators at Montreal in cents per hundred, were as follows:

On wheat 14.34; on corn and rye, 13.9; on oats, 15.07; and on barley 16.04.

Suggests Flat Rate for Demurrage.

The National Industrial Traffic League in conjunction with the American Railway Ass'n is advocating a flat rate on demurrage of \$3 per day instead of the present schedule of \$2 per day for the first four days and \$5 per day for each succeeding day thereafter. The matter has been presented to the Interstate Commerce Commission for a decision.

The proponents of the flat \$3 per day charge point out that during the severe car shortage last winter excessive demurrage rates of \$3 per day for the first four days, \$6 per day for the next three days and \$10 per day for each day thereafter were temporarily established. They think the establishment of a stationary \$3 per day charge which will not be increased during a shortage will greatly benefit the shippers and will lose the railroads nothing.

Protests Rate Reductions to the East.

Traffic representatives of New Orleans, Louisville, Cairo, St. Louis and Cincinnati recently protested to the I. C. C. against the publication of the reduced rates to the east under less than the statutory notice of 30 days.

The objections were summarized by Carl Giessow, general manager of the New Orleans Traffic Bureau as follows:

"If the Illinois Central and Missouri Pacific meet the cuts made by the other roads to the East, as they did two months ago, the situation will be relieved. Ocean freight rates from New Orleans to the United Kingdom and Continental Europe are 2½ to 5 cents higher than from Atlantic ports.

"The new rates will have the effect of benefiting the producers of one section and not all the country alike. It also will disturb the regular flow of grain that comes to the Gulf ports. All of the middle western territory, except Southern Kansas, Oklahoma and Texas will be made tributary to the Atlantic ports. The Texas and Oklahoma grain, by reason of an advance of rates, will go to Galveston, as will that from southern Texas, because of the Santa Fe railroad which serves that section."

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Universal Grain Code, the most complete code published for the use of grain dealers and millers. Its use will reduce your tolls one-half. Its 150 pages contain 14,910 code words for grain trade terms and expressions; no two near enough alike to cause confusion. Bond paper, flexible leather, \$3.00; Book paper, board cover, \$1.50.

Robinson Cipher Code, revised, with 1912 and 1917 Supplements for domestic grain business. Bound in flexible leather, price \$2.25; cloth, \$1.75.

A. B. C. Improved 5th Edition, contains a complete set of five letter code words for every expression in the former edition. Any two of these words may be combined and sent as one word, reducing telegraph tolls 50 per cent. Price in English, \$18.00.

Miller's Code (1917), for milling and flour trades, 3¼x6 inches, 77 pages, \$2.00.

Riverside Code, fifth edition, for millers and flour dealers. Bound in flexible leather, 228 pages, \$3.00.

Baltimore Export Cable Code, most popular and simplest code for export trade. Bound in red leather, third grain edition, 413 pages, size 7x9¼ inches, \$15.00; fourth edition, for grain, stocks, bonds and general merchandise, 160 pages, 6¼x9 inches, \$15.00.

Cross Telegraphic Cipher Code, for provision and grain trade. 145 pages, bound in flexible leather, \$2.00.

Your Name in gilt letters stamped on front cover of any of the above books for 35 cents extra. Any code upon short notice.

Grain Dealers Journal

305 S. La Salle Street,

Chicago, Ill.

New Automatic Hot Journal Alarm.

Many of the grain elevator fires reported "Cause unknown," are due to overheated bearings that were not under the observation of the operator. Many other mysterious fires have their origin in bearings that were hot when the plant was shut down for the night, and when the operator did not inspect the machinery before leaving.

In its annual report the Grain Dealers National Insurance Co. says "43 of the company's 80 fires which occurred during 1920 happened between 7 p. m. and 7 a. m., while 37 occurred during daylight. This is perfectly natural because many of the fires in grain elevators break out after the machinery is stopped and the operators go home. If every plant was carefully inspected for hot bearings each night before locking up the number of night fires would surely be reduced."

Bearings literally running in oil have been known to start fires, some of the principal reasons beside lack of lubrication are dust mixing with the oil, shaft getting out of line, box too light for the work it is doing, box not installed correctly, top half screwed down too tight.

Settling of the elevator building unequally due to the shifting load of grain, as bins on opposite sides are emptied and filled, will throw shafting out of line so much that the journals will heat.

The value of an automatic device that would set off an alarm when a bearing becomes heated was long ago recognized, but these devices are likely to get out of order and are expensive to install. They may be found in some of the larger terminal elevators where the cost was not considered, although these big plants have men employed whose regular duty it is to oil and go over machinery.

Until the invention of the bearing alarm shown in the engravings herewith these devices have been considered too much of a luxury for the country grain elevator, which needed them more than did the terminal houses. The diagram reproduced herewith shows at a glance the principle employed, the expansion of mercury in a cup to close an electric circuit and ring a gong. The manufacturer seals the alarm so that it can not be tampered with. It is screwed into the bearing as one piece and sealed to the bearing with a wire seal, preventing its removal except by competent authority, making it impossible to remove any of the connections without detection.

The average elevator is divided into three independent circuits, one circuit for the journals in the cupola and on the bin floor? One circuit for the work floor, this includes all

bearings on cleaners, motors or other machinery, and another circuit for the line shaft and boots.

Three finding buttons are provided on the switchboard which is installed in the office for these circuits. Should an alarm sound the operator has but to step to the switchboard and press these buttons one at a time. The one that stops the gong indicates the circuit that the heated journal is to be found in. These finding buttons are so constructed that the instant the operator releases the pressure of the button the gongs continue to ring. There is but one way to stop their ringing, this is by removing the cause of the alarm, by cooling with water, oiling or whatever attention it may require. There is no shutting the system off and forgetting about it, the gongs will stay on the job until the temperature of the bearing is reduced.

Opposite these three finding buttons there are three test buttons as no system could be devised that would be so entirely automatic that one would not want to check up on it. Pressing any of these buttons will cause the gongs to ring indicating that the circuit opposite is entirely in working order. Therefore it is possible to test every inch of wire and every connection from the switchboard.

In the base of the switchboard the battery

is located, of the auxiliary car lighting type. There are but two connections on this battery which is provided with spring washers and lock nuts, this prevents loose connections. A meter is attached to the switchboard so that the battery condition can be ascertained at any time by simply pushing a button. In plants wired for electricity a bell ringing transformer is used in place of a battery.

The gongs are mounted, one on switchboard and one in the driveway. These gongs are powerful, loud toned, wired independent of each other. Should one fail to work it would not affect the operation of the other. By an ingenious combination of running the wires the system will not fail to operate if any wire is cut or broken.

This alarm was invented and is being manufactured by the Cronan Bros. Co., which will give Journal readers additional information on application.

Insurance Notes.

THE UNDERWRITERS Grain Ass'n closed its business year Aug. 20 with a full percentage of members signed up for the coming year. The Ass'n's largest loss during the past year was on the Armour Elevator at South Chicago, Ill.

INSURANCE COMPANIES have warned the Halliday Elevator Co. at Cairo, Ill., that the floating equipment of the Barrett Co. endangers its elevator as a fire hazard. The Barrett Co. operates barges on the Ohio River and has been docking close to the Halliday Elevator. To settle the dispute the M. & O. R. R. has given orders to the city of Cairo not to permit the barge owners to dock within certain areas without its permission.

TO PREVENT industrial accidents as far as possible the Michigan Department of Labor and Industry has asked that various insurance companies writing compensation insurance in Michigan co-operate with the department. The department wishes to be given the benefit of reports turned in by insurance inspectors as to conditions in industrial plants. The department will then do what it can to secure the correction of various hazards.

MANY INSURANCE COMPANIES have long contended that insurance written by its agents did not become effective until actually accepted by the company. If an agent accepted a risk, took the signature of the applicant for insurance on an application and collected a membership fee and installment, the insurance was not effective until after the agent got ready and sent the information to the company. In the meantime the applicant was unprotected. Insurance Commissioner Travis of Kansas has ruled insurance cannot so be handled in Kansas.

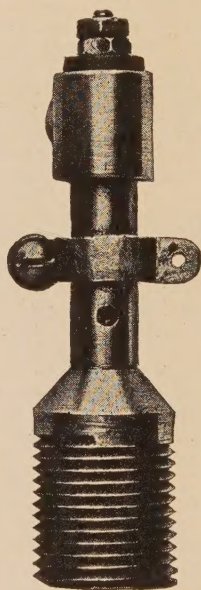
Wheat Pool in Canada Falls Thru.

In Canada many farmers were of the belief that the war time prices of wheat could be guaranteed by the establishment of some kind of a wheat pooling scheme. Such a scheme to handle the 1922 crop was advocated by the Council of Agriculture. The plan provided that the Canadian farmer enter into a contract for five years.

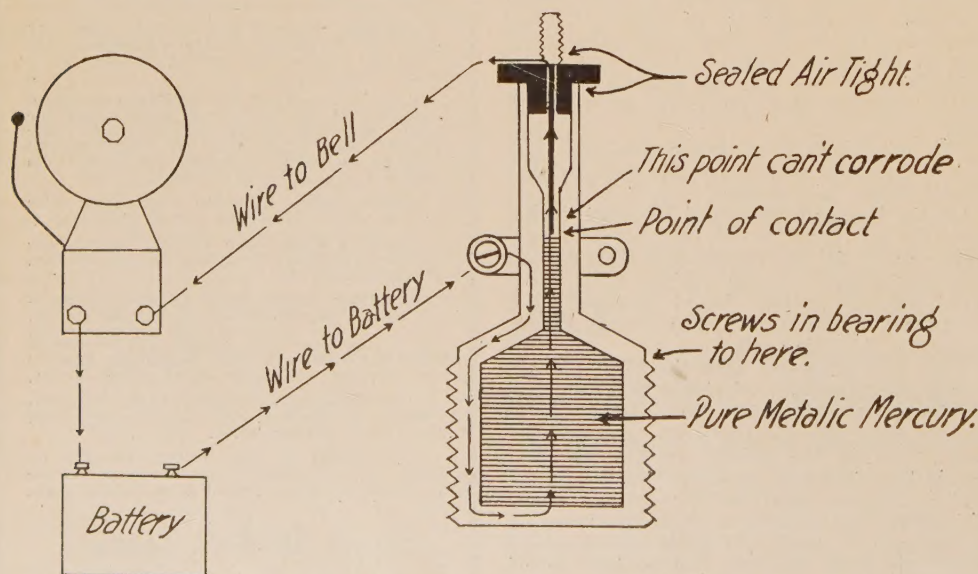
It seems that some farmers in Canada have tired of holding their grain. It is reported that 50,000,000 bushels were being held when wheat took its slump to \$1 per bushel.

Evidently the Canadian farmer who has had a taste of pooling, feels shy at signing any contract for five years, for a press report from Montreal says:

"The plan to form a wheat pool to handle the Canadian crop in 1922 has been practically abandoned owing to the disinclination of the farmers to sign a five-year contract, without which the plan would be unworkable."



Thermostat of Journal Alarm.



Cross Section of Thermostat and Diagram of Wiring to Gong and Battery of Hot Journal Alarm System.

GRAIN INSURANCE

The "Grain Dealers Mutual" Way of handling grain insurance is the result of twenty years experience in looking after the needs of the elevator man. There is no better way.

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C. R. McCOTTE,
Western Manager
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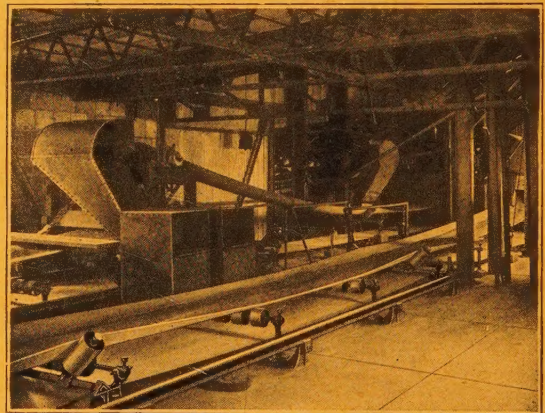
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